



Agenda
Edmonds City Council
COUNCIL CHAMBERS
250 5TH AVE NORTH, EDMONDS, WA 98020

AUGUST 15, 2017, 7:00 PM

- 1. CALL TO ORDER/FLAG SALUTE**
- 2. ROLL CALL**
- 3. APPROVAL OF THE AGENDA**
- 4. PRESENTATIONS**
 1. 2017 Legislative Session Wrap-Up (25 min)
 2. June Quarterly Financial Report (10 min.)
- 5. AUDIENCE COMMENTS (3-MINUTE LIMIT PER PERSON) - REGARDING MATTERS NOT LISTED ON THE AGENDA AS CLOSED RECORD REVIEW OR AS PUBLIC HEARINGS**
- 6. APPROVAL OF THE CONSENT AGENDA**
 1. Approval of Council Special Meeting Minutes of August 8, 2017
 2. Approval of Council Meeting Minutes of August 8, 2017
 3. Approval of claim checks.
 4. Amendments to Downtown Business Improvement District By-Laws
- 7. PUBLIC HEARING**
 1. Resolution to Designate Highway 99 Subarea as Residential Targeted Area (10 min)
- 8. ACTION ITEMS**
 1. Proposed Resolution supporting merger of SNOCOM and SNOPAC (30 min.)
 2. Adoption of Subarea Plan Zone Map Amendments, Development Regulations, and Planned Action for Highway 99 area (30 min.)
 3. Amendments to Multi-Family Tax Exemption Code Provisions (15 min)
 4. Amendments to Chapter 3.75, Business Improvement District (10 min)
 5. Pine Street Lighting Discussion (20 min.)
 6. Sunset Ave Parking (20 min.)
- 9. COUNCIL COMMITTEE REPORTS**
 1. Council Committee Reports (10 min.)
- 10. MAYOR'S COMMENTS**
- 11. COUNCIL COMMENTS**
- 12. CONVENE IN EXECUTIVE SESSION REGARDING PENDING OR POTENTIAL LITIGATION PER RCW 42.30.110(1)(I).**

13. RECONVENE IN OPEN SESSION. POTENTIAL ACTION AS A RESULT OF MEETING IN EXECUTIVE SESSION.

ADJOURN

City Council Agenda Item

Meeting Date: 08/15/2017

2017 Legislative Session Wrap-Up (25 min)

Staff Lead: Patrick Doherty

Department: Community Services

Preparer: Patrick Doherty

Background/History

Jennifer Ziegler is the City's contracted lobbyist/government affairs specialist in Olympia who provides a wrap-up report to City Council about each year's completed Legislative Session.

Staff Recommendation

No action required.

Narrative

Jennifer Ziegler will provide a report to City Council summarizing the key legislation passed, legislation attempted but not passed, key budget-related developments, and the status of work yet to be done by the Legislature, especially regarding the capita budget.

City Council Agenda Item

Meeting Date: 08/15/2017

June Quarterly Financial Report (10 min.)

Staff Lead: Dave Turley

Department: Administrative Services

Preparer: Sarah Mager

Background/History

N/A

Staff Recommendation

No action needed; informational only.

Narrative

June Quarterly Financial Report

Attachments:

Complete June Quarterly Financial Report



AT A GLANCE:

Overall our local economy remains strong. The unemployment rate for Seattle/Bellevue/Everett was at 3.4% in June. Home sales continue at a high rate. According to NWMLS "We are at 0.9 months of home supply in Snohomish County — less than 6 is considered a Seller's Market."

INSIDE THIS ISSUE:

Revenues By Fund Summary	1
Expenditures By Fund Summary	2
General Fund Revenues	3
Expenditures By Fund Details	6
General Fund Expenditure Detail	13
Investment Portfolio	32
Fund Balance Overview	34

Below: Summer is time for flower baskets

Financial Management Report

As of June 30, 2017

Summary of All Operating Funds: *Revenue*

The Financial Management Report is a summary report of the City's 2nd Quarter 2017 financial results with a comparison to 2nd Quarter 2016.

Notable Revenue Highlights:

- General Fund revenues are lagging slightly behind the first six months of 2016, by **\$54,319** (or 0.27%)
- 2017 tax revenues are **\$215,160** higher than 2016. This increase is led by:
 - ♦ Real/Personal Property taxes **\$142,492**
 - ♦ Sales taxes **\$229,651**

Specific Revenue Highlights by Fund:

- General Fund revenues for the first half of 2017 are higher than the same period in 2016 in the categories of Taxes, Licenses and Permits, Charges for Goods and Services, and in other Miscellaneous revenues. General Fund revenues are falling behind 2016 for the same period in Intergovernmental revenues (primarily grant revenues) and in Fines and Forfeitures. A more detailed breakdown of General Fund revenues can be found beginning on page 3, with sales tax performance displayed on page 18.
- Special Revenue Funds 2017 year-to-date revenues

are **\$3,849,719** or **49%** lower than last year. The 2017 revenue decrease is primarily due to a **\$2.9 million** decrease in revenues in the Combined Street Construction/Improvement Fund and a **\$813,637** decrease in the Parks Construction Fund. These decreases are largely due to the timing of grant billings for various projects. The decrease in revenue in this fund was offset somewhat by:

- ♦ The Street Fund revenues increased by **\$92,440** compared to 2016.
- ♦ REET revenues for 2017 have increased by **\$132,275** compared to 2016.
- but Enterprise Fund (Water, Storm, Sewer) revenues for 2017 have decreased by **\$11,876** compared to 2016.
- *Did you Know:* The Snohomish PUD is working with the City to upgrade nearly 1,600 streetlights with energy efficient LED lighting. LED lighting cuts energy use by up to 60% and lasts three to four times longer than traditional lights, reducing maintenance work for replacing the lights. The LED upgrades will begin this summer and the project is expected to be completed by early 2019.



Summary of All Operating Funds: *Expenditures*



Above: The Edmonds Police Dept. participated in the Law Enforcement Torch Run for Special Olympics on June 2

Below: New Band shell at the Frances Anderson Center

- General Fund expenditures ended the quarter with 49% of the budget expended, at a nearly identical spending pace compared to last year.

General Fund expense by categories are on page 6 and departmental expenses are on pages 13-16.

More General Fund expenditures information in graphical form can be found on pages 23-31.

- Special Revenue Fund expenditures decreased **\$1.36 million** in the first half of 2017 compared to the first half of 2016. The majority of this decrease comes from a reduction in construction outlays, with Combined Street Construction/Improvement fund expenses being **\$588,639** lower than last year. Additional Special Revenue Fund expenditure information can be found on pages 6-8.
- Through June 30 the Enterprise Fund (the Water fund, Storm fund, and Sewer fund) expenses increased by **\$1,014,915** over last year, primarily due to increased capital expenses in the sewer fund. Additional Utility Fund expense information can be found on pages 9-10.
- The City's overall ending fund balance remained stable, with an increase of **\$1.7 million** since December 31, representing a 2% increase.



Investment Portfolio

- The City's investment earnings continue to increase over 2016 earnings due to further diversification of the City's investment portfolio. More investment information can be found on pages 32-33.

Other Notable Highlights

- During the second quarter of 2017 the City invested an additional \$3 million in Federally backed bonds, bringing total investments to \$31.1 million.
- During the second quarter of 2017 the City substantially completed the construction of a new Veteran's Plaza next to the Public Safety Complex.

Veteran's Plaza



**CITY OF EDMONDS
REVENUES BY FUND - SUMMARY**

Fund No.	Title	2017 Amended Budget	6/30/2016 Revenues	6/30/2017 Revenues	Amount Remaining	% Received
001	GENERAL FUND	\$ 37,753,480	\$ 20,334,355	\$ 20,280,036	\$ 17,473,444	54%
009	LEOFF-MEDICAL INS. RESERVE	278,630	139,584	142,324	136,306	51%
011	RISK MANAGEMENT FUND	6,800	4,200	9,467	(2,667)	139%
012	CONTINGENCY RESERVE FUND	44,650	7,283	41,154	3,496	92%
013	MULTIMODAL TRANSPORTATION FUND	-	233	-	-	0%
014	HISTORIC PRESERVATION GIFT FUND	5,030	2,526	79	4,951	2%
016	BUILDING MAINTENANCE ¹	452,000	591,195	275,452	176,548	61%
104	DRUG ENFORCEMENT FUND	130,000	68,661	27,074	102,926	21%
111	STREET FUND	1,784,200	989,658	1,082,098	702,102	61%
112	COMBINED STREET CONST/IMPROVE ²	6,397,932	3,779,444	866,481	5,531,451	14%
117	MUNICIPAL ARTS ACQUIS. FUND	137,200	29,531	29,049	108,151	21%
118	MEMORIAL STREET TREE	110	74	188	(78)	171%
120	HOTEL/MOTEL TAX REVENUE FUND	91,200	34,503	34,388	56,812	38%
121	EMPLOYEE PARKING PERMIT FUND	24,600	8,119	11,519	13,081	47%
122	YOUTH SCHOLARSHIP FUND	1,800	71	530	1,270	29%
123	TOURISM PROMOTIONAL FUND/ARTS	30,450	11,649	12,002	18,448	39%
125	REAL ESTATE EXCISE TAX 2	1,311,900	625,801	691,522	620,378	53%
126	REAL ESTATE EXCISE TAX 1, PARKS ACQ FUND	1,314,400	626,412	692,966	621,434	53%
127	GIFTS CATALOG FUND	50,390	28,386	27,756	22,634	55%
129	SPECIAL PROJECTS FUND	280	158	402	(122)	144%
130	CEMETERY MAINTENANCE/IMPROVEMT	177,820	94,494	128,475	49,345	72%
132	PARKS CONSTRUCTION FUND ³	2,616,940	1,079,454	265,817	2,351,123	10%
136	PARKS TRUST FUND	1,070	628	1,594	(524)	149%
137	CEMETERY MAINTENANCE TRUST FD	22,880	12,086	21,426	1,454	94%
138	SISTER CITY COMMISSION	10,010	5,772	2,179	7,831	22%
139	TRANSPORTATION BENEFIT DISTRICT	-	350,193	-	-	0%
140	BUSINESS IMPROVEMENT DISTRICT	-	51,256	51,165	(51,165)	0%
211	L.I.D. FUND CONTROL	14,400	773	-	14,400	0%
231	2012 LT GO DEBT SERVICE FUND	695,830	70,301	64,938	630,892	9%
232	2014 DEBT SERVICE FUND	-	936,429	-	-	0%
411	COMBINED UTILITY OPERATION	-	83,251	78,885	(78,885)	0%
421	WATER UTILITY FUND	8,469,570	4,041,301	3,769,220	4,700,350	45%
422	STORM UTILITY FUND	4,789,173	1,946,923	2,084,829	2,704,344	44%
423	SEWER/WWTP UTILITY FUND ⁴	12,179,879	5,321,028	5,447,693	6,732,186	45%
424	BOND RESERVE FUND	1,988,720	667,446	654,345	1,334,375	33%
511	EQUIPMENT RENTAL FUND	1,628,910	831,427	976,796	652,114	60%
512	TECHNOLOGY RENTAL FUND	946,040	-	498,396	447,644	53%
617	FIREMEN'S PENSION FUND	66,000	57,418	59,782	6,218	91%
		\$ 83,422,294	\$ 42,832,027	\$ 38,330,028	\$ 45,092,266	46%

¹ Difference between 2016 and 2017 is due to grant invoicing for Fishing Pier Rehab.

² Difference between 2016 and 2017 is due to grant invoicing for various projects.

³ Differences between 2016 and 2017 are due to parks donations and park impact fees.

⁴ Difference between 2016 and 2017 is primarily due to invoicing timing difference.

**CITY OF EDMONDS
EXPENDITURES BY FUND - SUMMARY**

Fund No.	Title	2017 Amended Budget	6/30/2016 Expenditures	6/30/2017 Expenditures	Amount Remaining	% Spent
001	GENERAL FUND	\$ 40,069,055	\$ 19,669,418	\$ 19,674,034	\$ 20,395,021	49%
009	LEOFF-MEDICAL INS. RESERVE	293,460	126,411	143,466	149,994	49%
011	RISK MANAGEMENT RESERVE FUND	100,000	-	80,601	19,399	81%
014	HISTORIC PRESERVATION GIFT FUND	5,400	-	-	5,400	0%
016	BUILDING MAINTENANCE ⁵	545,500	757,714	133,383	412,117	24%
104	DRUG ENFORCEMENT FUND	76,030	27,780	16,531	59,499	22%
111	STREET FUND	1,935,260	947,462	852,831	1,082,429	44%
112	COMBINED STREET CONST/IMPROVE ⁶	6,427,332	2,394,938	1,806,299	4,621,033	28%
117	MUNICIPAL ARTS ACQUIS. FUND	181,880	25,274	17,345	164,535	10%
120	HOTEL/MOTEL TAX REVENUE FUND	99,600	26,145	37,257	62,343	37%
121	EMPLOYEE PARKING PERMIT FUND	26,880	-	-	26,880	0%
122	YOUTH SCHOLARSHIP FUND	2,000	835	587	1,413	29%
123	TOURISM PROMOTIONAL FUND/ARTS	61,700	9,543	9,047	52,653	15%
125	REAL ESTATE EXCISE TAX 2	2,962,566	258,888	475,890	2,486,676	16%
126	REAL ESTATE EXCISE TAX 1, PARKS ACQ FUND ⁷	2,395,021	498,915	18,348	2,376,673	1%
127	GIFTS CATALOG FUND	51,220	33,140	28,297	22,923	55%
130	CEMETERY MAINTENANCE/IMPROVEMT	217,020	77,712	81,563	135,457	38%
132	PARKS CONSTRUCTION FUND ⁷	2,955,000	584,177	515,732	2,439,268	17%
138	SISTER CITY COMMISSION	10,500	2,636	137	10,363	1%
139	TRANSPORTATION BENEFIT DISTRICT	-	350,193	-	-	0%
140	BUSINESS IMPROVEMENT DISTRICT	-	14,219	31,008	(31,008)	0%
211	L.I.D. FUND CONTROL	16,450	-	16,450	-	100%
231	2012 LT GO DEBT SERVICE FUND	695,830	70,301	64,938	630,892	9%
232	2014 DEBT SERVICE FUND	-	936,429	-	-	0%
421	WATER UTILITY FUND	13,257,452	3,656,305	3,377,086	9,880,366	25%
422	STORM UTILITY FUND	8,351,547	1,557,839	1,769,781	6,581,766	21%
423	SEWER/WWTP UTILITY FUND ⁸	16,714,828	4,202,354	5,284,546	11,430,282	32%
424	BOND RESERVE FUND	1,989,720	667,442	654,342	1,335,378	33%
511	EQUIPMENT RENTAL FUND	1,886,000	928,227	1,107,367	778,633	59%
512	TECHNOLOGY RENTAL FUND	890,430	-	373,016	517,414	42%
617	FIREMEN'S PENSION FUND	72,070	21,351	33,336	38,734	46%
		\$ 102,289,751	\$ 37,845,648	\$ 36,603,218	\$ 65,686,533	36%

⁵ Differences between 2016 and 2017 are primarily due to Fishing Pier Rehabilitation Project.

⁶ Differences between 2016 and 2017 are due to construction projects expenses in 2016.

⁷ Differences between 2016 and 2017 are due to purchase of Civic Field in 2016.

⁸ Differences between 2016 and 2017 are due to construction projects in 2017.

**CITY OF EDMONDS
REVENUES - GENERAL FUND**

Title	2017 Amended Budget	6/30/2016 Revenues	6/30/2017 Revenues	Amount Remaining	% Received
TAXES:					
REAL PERSONAL / PROPERTY TAX ⁹	\$ 10,221,670	\$ 5,343,422	\$ 5,485,914	\$ 4,735,756	54%
EMS PROPERTY TAX ¹⁰	3,911,080	1,955,463	2,120,943	1,790,137	54%
VOTED PROPERTY TAX	10,000	504,076	13,395	(3,395)	134%
LOCAL RETAIL SALES/USE TAX ¹¹	6,875,000	3,295,015	3,524,666	3,350,334	51%
NATURAL GAS USE TAX	7,040	3,365	6,030	1,010	86%
1/10 SALES TAX LOCAL CRIM JUST	687,500	327,165	338,872	348,628	49%
ELECTRIC UTILITY TAX	1,604,000	913,790	991,566	612,434	62%
GAS UTILITY TAX	608,000	385,669	475,324	132,676	78%
SOLID WASTE UTILITY TAX	313,600	162,220	150,403	163,197	48%
WATER UTILITY TAX	1,235,300	540,263	529,472	705,828	43%
SEWER UTILITY TAX	632,300	311,566	342,031	290,269	54%
STORMWATER UTILITY TAX	331,300	164,329	181,546	149,754	55%
T.V. CABLE UTILITY TAX	857,600	425,407	433,642	423,958	51%
TELEPHONE UTILITY TAX	1,263,200	633,125	582,703	680,497	46%
PULLTABS TAX	52,000	25,643	27,488	24,512	53%
AMUSEMENT GAMES	40	141	-	40	0%
LEASEHOLD EXCISE TAX	262,300	126,667	128,490	133,810	49%
	28,871,930	15,117,325	15,332,485	13,539,446	53%
LICENSES AND PERMITS:					
FIRE PERMITS-SPECIAL USE	250	190	90	160	36%
POLICE - FINGERPRINTING	300	200	408	(108)	136%
PROF AND OCC LICENSE-TAXI	330	-	-	330	0%
AMUSEMENTS	4,740	-	6,325	(1,585)	133%
VENDING MACHINE/CONCESSION	50,000	2,769	13,731	36,269	27%
FRANCHISE AGREEMENT-COMCAST	716,800	358,427	364,826	351,974	51%
FRANCHISE FEE-EDUCATION/GOVERNMENT	42,100	21,148	21,155	20,945	50%
FRANCHISE AGREEMENT-VERIZON/FRONTIER	102,300	50,705	54,268	48,032	53%
FRANCHISE AGREEMENT-BLACKROCK	16,600	8,172	7,511	9,089	45%
OLYMPIC VIEW WATER DISTRICT FRANCHISE	298,200	138,558	148,736	149,464	50%
GENERAL BUSINESS LICENSE	116,600	76,241	61,106	55,494	52%
DEV SERV PERMIT SURCHARGE	44,940	26,515	37,627	7,314	84%
NON-RESIDENT BUS LICENSE	67,800	42,900	43,950	23,850	65%
RIGHT OF WAY FRANCHISE FEE	10,700	44,623	11,880	(1,180)	111%
BUILDING STRUCTURE PERMITS	620,600	402,155	419,255	201,345	68%
ANIMAL LICENSES	35,090	9,792	11,465	23,625	33%
STREET AND CURB PERMIT	48,150	21,432	19,921	28,229	41%
OTR NON-BUS LIC/PERMITS	13,200	8,987	8,641	4,559	65%
	2,188,700	1,212,813	1,230,893	957,807	56%
INTERGOVERNMENTAL:					
DOJ 15-0404-0-1-754 - BULLET PROOF VEST	7,930	6,920	5,949	1,981	75%
WAASSOC OF SHERIFFS TRAFFIC GRANT	-	995	-	-	0%
TARGET ZERO TEAMS GRANT	4,000	2,273	1,030	2,970	26%
HIGH VISIBILITY ENFORCEMENT	7,100	2,663	4,767	2,333	67%
DOCKSIDE DRILLS GRANT REIMBURSE	-	614	1,399	(1,399)	0%
WATERFRONT ANALYSIS GRANT	-	251,859	10,000	(10,000)	0%
PUD PRIVILEGE TAX	195,500	-	-	195,500	0%
MVET/SPECIAL DISTRIBUTION	12,680	5,441	5,657	7,023	45%
JUDICIAL SALARY CONTRIBUTION-STATE	18,000	9,150	4,179	13,821	23%
CRIMINAL JUSTICE-SPECIAL PROGRAMS	42,500	19,893	20,581	21,919	48%
MARIJUANA ENFORCEMENT	23,700	-	-	23,700	0%
DUI - CITIES	3,000	3,166	3,119	(119)	104%
LIQUOR EXCISE TAX	191,000	96,625	100,033	90,967	52%
LIQUOR BOARD PROFITS	345,600	174,541	172,712	172,888	50%
MISCELLANEOUS INTERLOCAL REVENUE	-	75,757	-	-	0%
INTERLOCAL GRANTS	-	-	35,000	(35,000)	0%
VERDANT INTERLOCAL GRANTS	2,000	2,000	2,000	-	100%
POLICE TRAINING CLASSES	-	-	300	(300)	0%
	853,010	651,896	366,725	486,285	43%

⁹ 2017 Real Personal/Property Tax revenues are \$142,492 higher than 2016 revenues.

¹⁰ 2017 EMS Property Tax revenues are \$165,480 higher than 2016 revenues.

¹¹ 2017 Local Retail Sales/Use Tax revenues are \$229,651 higher than 2016 revenues. Please also see pages 18 & 19.

**CITY OF EDMONDS
REVENUES - GENERAL FUND**

Title	2017 Amended Budget	6/30/2016 Revenues	6/30/2017 Revenues	Amount Remaining	% Received
CHARGES FOR GOODS AND SERVICES:					
RECORD/LEGAL INSTRUMENTS	2,000	1,251	2,169	(169)	108%
ATM SURCHARGE FEES	300	86	145	155	48%
CREDIT CARD FEES	15,000	-	5,230	9,770	35%
D/M COURT REC SER	10	7	195	(185)	1955%
MUNIC.-DIST. COURT CURR EXPEN	600	284	147	453	25%
SALE MAPS & BOOKS	100	-	23	77	23%
CLERKS TIME FOR SALE OF PARKING PERMITS	25,100	-	-	25,100	0%
BID SUPPLIES REIMBURSEMENT	600	-	-	600	0%
PHOTOCOPIES	2,000	540	343	1,657	17%
POLICE DISCLOSURE REQUESTS	4,000	1,772	1,674	2,326	42%
ENGINEERING FEES AND CHARGES ¹²	200,000	139,289	259,226	(59,226)	130%
SNO-ISLE	58,000	33,996	43,937	14,063	76%
PASSPORTS AND NATURALIZATION FEES	17,000	11,100	13,400	3,600	79%
POLICE SERVICES SPECIAL EVENTS	30,000	8,757	8,495	21,505	28%
CAMPUS SAFETY-EDM. SCH. DIST.	66,280	2,873	3,161	63,119	5%
WOODWAY-LAW PROTECTION	42,000	20,907	34,067	7,933	81%
MISCELLANEOUS POLICE SERVICES	1,500	-	-	1,500	0%
DUI EMERGENCY FIRE SERVICES	300	-	-	300	0%
FIRE PROTECTION & EMS FOR DUI	-	31	32	(32)	0%
FIRE DISTRICT #1 STATION BILLINGS	45,000	28,234	30,505	14,495	68%
LEGAL SERVICES	-	159	-	-	0%
ADULT PROBATION SERVICE CHARGE	56,000	36,610	24,291	31,709	43%
BOOKING FEES	4,200	2,081	1,477	2,723	35%
FIRE CONSTRUCTION INSPECTION FEES	8,560	14,119	9,012	(452)	105%
EMERGENCY SERVICE FEES	8,000	4,460	2,222	5,778	28%
EMS TRANSPORT USER FEE	824,200	487,513	423,461	400,739	51%
FIBER SERVICES	-	6,095	-	-	0%
INTERGOVERNMENTAL FIBER SERVICES	-	3,600	-	-	0%
FLEX FUEL PAYMENTS FROM STATIONS	1,800	876	1,198	602	67%
ANIMAL CONTROL SHELTER	250	184	50	200	20%
ZONING/SUBDIVISION FEE	80,250	47,321	44,193	36,057	55%
PLAN CHECKING FEES	428,000	231,123	202,618	225,382	47%
FIRE PLAN CHECK FEES	3,210	6,255	4,540	(1,330)	141%
PLANNING 1% INSPECTION FEE	1,600	1,740	633	967	40%
S.E.P.A. REVIEW	2,140	1,220	5,360	(3,220)	250%
CRITICAL AREA STUDY	18,190	10,590	8,190	10,000	45%
DV COORDINATOR SERVICES	11,460	5,560	5,729	5,731	50%
GYM AND WEIGHTROOM FEES	12,000	6,578	7,549	4,451	63%
LOCKER FEES	-	-	15	(15)	0%
PROGRAM FEES	910,100	596,167	528,729	381,371	58%
TAXABLE RECREATION ACTIVITIES	1,300	881	907	393	70%
WINTER MARKET REGISTRATION FEES	5,000	25	150	4,850	3%
BIRD FEST REGISTRATION FEES	750	190	-	750	0%
INTERFUND REIMBURSEMENT-CONTRACT SVCS	2,044,540	1,107,179	1,157,729	886,811	57%
	4,931,340	2,819,651	2,830,801	2,100,539	57%

Attachment: Complete June Quarterly Financial Report (June Quarterly Financial Report)

¹² 2017 Engineering Fees and Charges are \$119,937 higher than 2016 revenues.

**CITY OF EDMONDS
REVENUES - GENERAL FUND**

Title	2017 Amended Budget	6/30/2016 Revenues	6/30/2017 Revenues	Amount Remaining	% Received
FINES AND FORFEITURES:					
PROOF OF VEHICLE INS PENALTY	7,200	3,632	2,689	4,511	37%
TRAFFIC INFRACTION PENALTIES	32,800	99,153	107,673	(74,873)	328%
NC TRAFFIC INFRACTION	232,500	31,109	17,079	215,421	7%
CRT COST FEE CODE LEG ASSESSMENT (LGA)	20,700	10,353	10,093	10,607	49%
NON-TRAFFIC INFRACTION PENALTIES	-	38,628	400	(400)	0%
OTHER INFRACTIONS '04	2,100	1,068	1,764	336	84%
PARKING INFRACTION PENALTIES	25,300	12,645	24,014	1,286	95%
PARK/INDDISZONE	3,600	1,820	1,339	2,261	37%
DWI PENALTIES	8,000	14,581	3,078	4,922	38%
DUI - DP ACCT	2,800	1,420	1,049	1,751	37%
CRIM CNV FEE DUI	500	274	96	404	19%
OTHER CRIMINAL TRAF MISDEM PEN	-	-	135	(135)	0%
CRIMINAL TRAFFIC MISDEMEANOR 8/03	33,600	16,805	18,457	15,143	55%
CRIMINAL CONVICTION FEE CT	2,000	1,396	1,388	612	69%
CRIM CONV FEE CT	2,000	962	851	1,149	43%
OTHER NON-TRAF MISDEMEANOR PEN	-	-	32	(32)	0%
OTHER NON TRAFFIC MISD. 8/03	9,300	4,659	4,961	4,339	53%
COURT DV PENALTY ASSESSMENT	600	283	401	199	67%
CRIMINAL CONVICTION FEE CN	1,900	939	582	1,318	31%
CRIM CONV FEE CN	500	267	204	296	41%
CRIMINAL COSTS-RECOUPMENTS	30,900	15,430	5,193	25,707	17%
PUBLIC DEFENSE RECOUPMENT	25,600	12,823	9,229	16,371	36%
BANK CHARGE FOR CONV. DEFENDANT	6,000	2,935	4,452	1,548	74%
COURT INTERPRETER COSTS	100	65	188	(88)	188%
BUS. LICENSE PERMIT PENALTY	11,000	10,050	8,480	2,520	77%
MISC FINES AND PENALTIES	1,400	240	1,440	(40)	103%
	460,400	281,536	225,267	235,133	49%
MISCELLANEOUS:					
INVESTMENT INTEREST	40,400	24,758	65,522	(25,122)	162%
INTEREST ON COUNTY TAXES	5,700	3,332	5,419	281	95%
INTEREST - COURT COLLECTIONS	5,400	2,697	2,874	2,526	53%
PARKING	15,600	7,899	9,027	6,573	58%
SPACE/FACILITIES RENTALS	147,000	65,334	59,332	87,668	40%
BRACKET ROOM RENTAL	5,000	3,160	2,760	2,240	55%
LEASES LONG-TERM	175,000	90,087	91,852	83,148	52%
OTHER RENTS & USE CHARGES	2,400	1,800	1,500	900	63%
PARKS DONATIONS	4,350	4,250	3,650	700	84%
BIRD FEST CONTRIBUTIONS	1,500	700	890	610	59%
VOLUNTEER PICNIC CONTRIBUTIONS	1,000	1,201	-	1,000	0%
POLICE CONTRIBUTIONS FROM PRIV SOURCES	1,000	-	-	1,000	0%
SALE OF JUNK/SALVAGE	300	324	5,303	(5,003)	1768%
SALES OF UNCLAIM PROPERTY	3,000	4,770	1,595	1,405	53%
CONFISCATED AND FORFEITED PROPERTY	2,000	-	-	2,000	0%
OTHER JUDGEMENT/SETTLEMENT	2,000	3,801	7	1,993	0%
POLICE JUDGMENTS/RESTITUTION	200	276	86	114	43%
CASHIER'S OVERAGES/SHORTAGES	-	161	136	(136)	0%
OTHER MISC REVENUES	2,000	6,439	5,477	(3,477)	274%
SMALL OVERPAYMENT	30	16	18	12	60%
NSF FEES - PARKS & REC	120	-	-	120	0%
NSF FEES - MUNICIPAL COURT	300	140	254	46	85%
NSF FEES - POLICE	-	-	60	(60)	0%
US BANK REBATE	7,500	4,153	4,659	2,841	62%
	421,800	225,299	260,422	161,378	62%
TRANSFERS-IN:					
INSURANCE RECOVERIES	-	-	7,143	(7,143)	0%
TRANSFER FROM FUND 127	26,300	25,835	26,300	-	100%
	26,300	25,835	33,443	(7,143)	127%
TOTAL GENERAL FUND REVENUE	\$ 37,753,480	\$ 20,334,355	\$ 20,280,036	\$ 17,473,444	54%

Attachment: Complete June Quarterly Financial Report (June Quarterly Financial Report)

**CITY OF EDMONDS
EXPENDITURES BY FUND - DETAIL**

Title	2017 Amended Budget	6/30/2016 Expenditures	6/30/2017 Expenditures	Amount Remaining	% Spent
GENERAL FUND EXPENDITURES (001)					
SALARIES AND WAGES	\$ 14,718,310	\$ 6,703,094	\$ 6,913,119	\$ 7,805,191	47%
OVERTIME	485,880	232,475	240,712	245,168	50%
HOLIDAY BUY BACK	226,420	2,349	1,324	225,096	1%
BENEFITS	5,496,060	2,543,636	2,669,977	2,826,083	49%
UNIFORMS	85,110	43,398	31,458	53,652	37%
SUPPLIES	376,509	161,232	160,497	216,012	43%
SMALL EQUIPMENT	50,040	57,399	33,288	16,752	67%
PROFESSIONAL SERVICES	3,677,883	1,294,297	1,197,869	2,480,014	33%
COMMUNICATIONS	182,180	74,771	67,244	114,936	37%
TRAVEL	50,430	15,499	24,562	25,868	49%
EXCISE TAXES	6,500	-	1,956	4,544	30%
ADVERTISING	-	1,699	-	-	0%
RENTAL/LEASE	1,707,550	456,537	845,894	861,656	50%
INSURANCE	609,550	538,732	616,495	(6,945)	101%
UTILITIES	457,800	201,160	216,253	241,547	47%
REPAIRS & MAINTENANCE	223,800	123,090	156,534	67,266	70%
MISCELLANEOUS	395,500	210,608	159,077	236,423	40%
INTERGOVERNMENTAL SERVICES	9,823,140	5,246,967	5,831,961	3,991,179	59%
INTERGOVERNMENTAL PAYMENTS	135,000	-	-	135,000	0%
INTERFUND SUBSIDIES	1,080,200	1,739,021	482,130	598,070	45%
BUILDINGS	18,323	-	-	18,323	0%
MACHINERY/EQUIPMENT	58,000	-	9,694	48,306	17%
GENERAL OBLIGATION BOND PRINCIPAL	164,790	-	-	164,790	0%
OTHER DEBT	500	-	-	500	0%
INTEREST ON LONG-TERM EXTERNAL DEBT	39,580	23,454	13,990	25,591	35%
	<u>\$ 40,069,055</u>	<u>\$ 19,669,418</u>	<u>\$ 19,674,034</u>	<u>\$ 20,395,021</u>	<u>49%</u>
LEOFF-MEDICAL INS. RESERVE (009)					
BENEFITS	\$ 183,190	\$ 80,462	\$ 87,704	\$ 95,486	48%
IN HOME LTC CLAIMS	102,990	43,491	55,362	47,628	54%
PROFESSIONAL SERVICES	7,000	1,286	-	7,000	0%
TRAVEL	-	447	-	-	0%
MISCELLANEOUS	280	725	400	(120)	143%
	<u>\$ 293,460</u>	<u>\$ 126,411</u>	<u>\$ 143,466</u>	<u>\$ 149,994</u>	<u>49%</u>
RISK MANAGEMENT RESERVE FUND (011)					
MISCELLANEOUS	\$ 100,000	\$ -	\$ 80,601	\$ 19,399	81%
	<u>\$ 100,000</u>	<u>\$ -</u>	<u>\$ 80,601</u>	<u>\$ 19,399</u>	<u>81%</u>
HISTORIC PRESERVATION GIFT FUND (014)					
SUPPLIES	\$ 100	\$ -	\$ -	\$ 100	0%
PROFESSIONAL SERVICES	200	-	-	200	0%
MISCELLANEOUS	5,100	-	-	5,100	0%
	<u>\$ 5,400</u>	<u>\$ -</u>	<u>\$ -</u>	<u>\$ 5,400</u>	<u>0%</u>
BUILDING MAINTENANCE SUBFUND (016)					
SUPPLIES	\$ -	\$ 1,756	\$ -	\$ -	0%
PROFESSIONAL SERVICES	2,000	161,272	23,979	(21,979)	1199%
REPAIRS & MAINTENANCE	193,500	35,390	50,764	142,736	26%
MISCELLANEOUS	-	546	-	-	0%
MACHINERY/EQUIPMENT	-	169,691	-	-	0%
CONSTRUCTION PROJECTS	350,000	389,059	58,640	291,360	17%
	<u>\$ 545,500</u>	<u>\$ 757,714</u>	<u>\$ 133,383</u>	<u>\$ 412,117</u>	<u>24%</u>
DRUG ENFORCEMENT FUND (104)					
FUEL CONSUMED	\$ 3,000	\$ 647	\$ 817	\$ 2,183	27%
SMALL EQUIPMENT	5,000	-	-	5,000	0%
COMMUNICATIONS	2,230	877	675	1,555	30%
REPAIR/MAINT	800	-	-	800	0%
MISCELLANEOUS	20,000	5,000	5,000	15,000	25%
INTERGOVERNMENTAL SERVICES	45,000	21,257	10,038	34,962	22%
	<u>\$ 76,030</u>	<u>\$ 27,780</u>	<u>\$ 16,531</u>	<u>\$ 59,499</u>	<u>22%</u>

Attachment: Complete June Quarterly Financial Report (June Quarterly Financial Report)

**CITY OF EDMONDS
EXPENDITURES BY FUND - DETAIL**

Title	2017 Amended Budget	6/30/2016 Expenditures	6/30/2017 Expenditures	Amount Remaining	% Spent
STREET FUND (111)					
SALARIES AND WAGES	\$ 555,070	\$ 291,347	\$ 269,870	\$ 285,200	49%
OVERTIME	18,400	10,757	15,315	3,085	83%
BENEFITS	318,720	149,949	135,556	183,164	43%
UNIFORMS	6,000	3,745	3,711	2,289	62%
SUPPLIES	350,000	167,167	85,545	264,455	24%
SMALL EQUIPMENT	24,000	545	1,223	22,777	5%
PROFESSIONAL SERVICES	15,050	4,146	8,699	6,351	58%
COMMUNICATIONS	3,500	1,850	3,820	(320)	109%
TRAVEL	1,000	-	-	1,000	0%
RENTAL/LEASE	181,020	94,581	89,669	91,351	50%
INSURANCE	113,230	97,912	110,508	2,722	98%
UTILITIES	270,170	113,801	114,054	156,116	42%
REPAIRS & MAINTENANCE	44,000	8,470	11,974	32,026	27%
MISCELLANEOUS	25,000	1,430	1,007	23,993	4%
INTERGOVERNMENTAL SERVICES	5,000	896	1,527	3,474	31%
GENERAL OBLIGATION BOND PRINCIPAL	3,490	-	-	3,490	0%
INTEREST ON LONG-TERM EXTERNAL DEBT	1,610	865	354	1,256	22%
	<u>\$ 1,935,260</u>	<u>\$ 947,462</u>	<u>\$ 852,831</u>	<u>\$ 1,082,429</u>	<u>44%</u>
COMBINED STREET CONST/IMPROVE (112)					
PROFESSIONAL SERVICES	\$ 2,380,040	\$ 654,883	\$ 586,925	\$ 1,793,115	25%
REPAIRS & MAINTENANCE	-	-	34,602	(34,602)	0%
INTERFUND SUBSIDIES	328,100	40,908	40,711	287,389	12%
LAND	-	132,896	-	-	0%
MACHINERY/EQUIPMENT	-	-	8,166	(8,166)	0%
CONSTRUCTION PROJECTS	3,643,922	1,490,654	1,060,658	2,583,264	29%
INTERGOVERNMENTAL LOANS	72,220	72,201	72,201	19	100%
INTEREST	3,050	3,396	3,035	15	100%
	<u>\$ 6,427,332</u>	<u>\$ 2,394,938</u>	<u>\$ 1,806,299</u>	<u>\$ 4,621,033</u>	<u>28%</u>
MUNICIPAL ARTS ACQUIS. FUND (117)					
SUPPLIES	\$ 4,700	\$ 132	\$ 617	\$ 4,083	13%
SMALL EQUIPMENT	1,700	150	762	938	45%
PROFESSIONAL SERVICES	166,500	22,232	12,867	153,633	8%
TRAVEL	80	11	11	70	13%
RENTAL/LEASE	2,000	-	-	2,000	0%
REPAIRS & MAINTENANCE	300	-	-	300	0%
MISCELLANEOUS	6,600	2,750	3,088	3,512	47%
	<u>\$ 181,880</u>	<u>\$ 25,274</u>	<u>\$ 17,345</u>	<u>\$ 164,535</u>	<u>10%</u>
HOTEL/MOTEL TAX REVENUE FUND (120)					
PROFESSIONAL SERVICES	\$ 83,400	\$ 24,080	\$ 35,257	\$ 48,143	42%
MISCELLANEOUS	12,200	65	-	12,200	0%
INTERFUND SUBSIDIES	4,000	2,000	2,000	2,000	50%
	<u>\$ 99,600</u>	<u>\$ 26,145</u>	<u>\$ 37,257</u>	<u>\$ 62,343</u>	<u>37%</u>
EMPLOYEE PARKING PERMIT FUND (121)					
SUPPLIES	\$ 1,790	\$ -	\$ -	\$ 1,790	0%
PROFESSIONAL SERVICES	25,090	-	-	25,090	0%
	<u>\$ 26,880</u>	<u>\$ -</u>	<u>\$ -</u>	<u>\$ 26,880</u>	<u>0%</u>
YOUTH SCHOLARSHIP FUND (122)					
MISCELLANEOUS	\$ 2,000	\$ 835	\$ 587	\$ 1,413	29%
	<u>\$ 2,000</u>	<u>\$ 835</u>	<u>\$ 587</u>	<u>\$ 1,413</u>	<u>29%</u>
TOURISM PROMOTIONAL FUND/ARTS (123)					
PROFESSIONAL SERVICES	\$ 57,200	\$ 8,282	\$ 7,674	\$ 49,526	13%
MISCELLANEOUS	4,500	1,260	1,374	3,126	31%
	<u>\$ 61,700</u>	<u>\$ 9,543</u>	<u>\$ 9,047</u>	<u>\$ 52,653</u>	<u>15%</u>

Attachment: Complete June Quarterly Financial Report (June Quarterly Financial Report)

**CITY OF EDMONDS
EXPENDITURES BY FUND - DETAIL**

Title	2017 Amended Budget	6/30/2016 Expenditures	6/30/2017 Expenditures	Amount Remaining	% Spent
REAL ESTATE EXCISE TAX 2 (125)					
SUPPLIES	\$ 21,000	\$ 42,366	\$ 48,050	\$ (27,050)	229%
PROFESSIONAL SERVICES	150,000	47,575	71,241	78,760	47%
RENTAL/LEASE	-	2,790	-	-	0%
UTILITIES	-	-	1,649	(1,649)	0%
REPAIRS & MAINTENANCE	140,000	12,605	-	140,000	0%
MISCELLANEOUS	-	181	-	-	0%
CONSTRUCTION PROJECTS	2,651,566	153,371	354,951	2,296,615	13%
	<u>\$ 2,962,566</u>	<u>\$ 258,888</u>	<u>\$ 475,890</u>	<u>\$ 2,486,676</u>	<u>16%</u>
REAL ESTATE EXCISE TAX 1, PARKS ACQ (126)					
PROFESSIONAL SERVICES	\$ 101,855	\$ -	\$ 123	\$ 101,732	0%
INTERFUND SUBSIDIES	142,330	17,213	16,163	126,168	11%
LAND	200,000	476,669	-	200,000	0%
CONSTRUCTION PROJECTS	1,921,206	-	-	1,921,206	0%
GENERAL OBLIGATION BONDS	20,280	-	-	20,280	0%
INTEREST	9,350	5,033	2,062	7,288	22%
	<u>\$ 2,395,021</u>	<u>\$ 498,915</u>	<u>\$ 18,348</u>	<u>\$ 2,376,673</u>	<u>1%</u>
GIFTS CATALOG FUND (127)					
SUPPLIES	\$ 17,820	\$ 1,005	\$ 1,997	\$ 15,823	11%
PROFESSIONAL SERVICES	6,500	6,300	-	6,500	0%
MISCELLANEOUS	600	-	-	600	0%
INTERFUND SUBSIDIES	26,300	25,835	26,300	-	100%
	<u>\$ 51,220</u>	<u>\$ 33,140</u>	<u>\$ 28,297</u>	<u>\$ 22,923</u>	<u>55%</u>
CEMETERY MAINTENANCE/IMPROVEMENT (130)					
SALARIES AND WAGES	\$ 124,410	\$ 40,451	\$ 37,443	\$ 86,967	30%
OVERTIME	3,500	963	2,747	753	78%
BENEFITS	35,910	17,524	17,635	18,275	49%
UNIFORMS	1,000	-	-	1,000	0%
SUPPLIES	7,000	745	1,044	5,956	15%
SUPPLIES PURCHASED FOR INVENTORY/RESALE	20,000	9,096	6,621	13,379	33%
PROFESSIONAL SERVICES	4,200	594	5,287	(1,087)	126%
COMMUNICATIONS	1,410	499	668	742	47%
TRAVEL	500	-	-	500	0%
RENTAL/LEASE	10,790	5,775	5,395	5,395	50%
UTILITIES	3,800	300	2,175	1,625	57%
REPAIRS & MAINTENANCE	500	-	-	500	0%
MISCELLANEOUS	4,000	1,765	2,548	1,452	64%
	<u>\$ 217,020</u>	<u>\$ 77,712</u>	<u>\$ 81,563</u>	<u>\$ 135,457</u>	<u>38%</u>
PARKS CONSTRUCTION FUND (132)					
SUPPLIES	\$ 20,000	\$ -	\$ -	\$ 20,000	0%
PROFESSIONAL SERVICES	600,000	30,809	91,977	508,023	15%
LAND	-	475,710	-	-	0%
CONSTRUCTION PROJECTS	2,335,000	77,659	423,754	1,911,246	18%
	<u>\$ 2,955,000</u>	<u>\$ 584,177</u>	<u>\$ 515,732</u>	<u>\$ 2,439,268</u>	<u>17%</u>
SISTER CITY COMMISSION (138)					
SUPPLIES	\$ 1,500	\$ 98	\$ 137	\$ 1,363	9%
TRAVEL	4,500	2,515	-	4,500	0%
MISCELLANEOUS	4,500	23	-	4,500	0%
	<u>\$ 10,500</u>	<u>\$ 2,636</u>	<u>\$ 137</u>	<u>\$ 10,363</u>	<u>1%</u>
TRANSPORTATION BENEFIT DISTRICT (139)					
INSURANCE	\$ -	\$ 2,500	\$ -	\$ -	0%
INTERGOVERNMENTAL SERVICES	-	347,693	-	-	0%
	<u>\$ -</u>	<u>\$ 350,193</u>	<u>\$ -</u>	<u>\$ -</u>	<u>0%</u>

Attachment: Complete June Quarterly Financial Report (June Quarterly Financial Report)

**CITY OF EDMONDS
EXPENDITURES BY FUND - DETAIL**

Title	2017 Amended Budget	6/30/2016 Expenditures	6/30/2017 Expenditures	Amount Remaining	% Spent
BUSINESS IMPROVEMENT DISTRICT FUND (140)					
SUPPLIES	\$ -	\$ 789	\$ 842	\$ (842)	0%
PROFESSIONAL SERVICES	-	12,536	29,031	(29,031)	0%
MISCELLANEOUS	-	894	1,136	(1,136)	0%
	<u>\$ -</u>	<u>\$ 14,219</u>	<u>\$ 31,008</u>	<u>\$ (31,008)</u>	<u>0%</u>
LID FUND CONTROL (211)					
INTERFUND SUBSIDIES	\$ 16,450	\$ -	\$ 16,450	\$ -	100%
	<u>\$ 16,450</u>	<u>\$ -</u>	<u>\$ 16,450</u>	<u>\$ -</u>	<u>100%</u>
2012 LTGO DEBT SERVIC FUND (231)					
GENERAL OBLIGATION BOND	\$ 565,450	\$ -	\$ -	\$ 565,450	0%
INTEREST	129,880	70,301	64,938	64,942	50%
OTHER INTEREST & DEBT SERVICE COSTS	500	-	-	500	0%
	<u>\$ 695,830</u>	<u>\$ 70,301</u>	<u>\$ 64,938</u>	<u>\$ 630,892</u>	<u>9%</u>
2014 DEBT SERVICE FUND (232)					
2014 PRINCIPAL PAYMENT	\$ -	\$ 933,208	\$ -	\$ -	0%
INTEREST	-	3,221	-	-	0%
	<u>\$ -</u>	<u>\$ 936,429</u>	<u>\$ -</u>	<u>\$ -</u>	<u>0%</u>
WATER FUND (421)					
SALARIES AND WAGES	\$ 844,510	\$ 387,645	\$ 377,846	\$ 466,664	45%
OVERTIME	24,000	11,046	10,630	13,370	44%
BENEFITS	405,720	181,285	170,910	234,810	42%
UNIFORMS	4,000	1,517	4,727	(727)	118%
SUPPLIES	150,000	52,790	78,257	71,743	52%
WATER PURCHASED FOR RESALE	1,667,500	695,668	648,352	1,019,148	39%
SUPPLIES PURCHASED FOR INVENTORY/RESALE	143,000	48,285	105,442	37,558	74%
SMALL EQUIPMENT	19,730	2,087	2,074	17,656	11%
PROFESSIONAL SERVICES	2,181,281	532,814	665,840	1,515,441	31%
COMMUNICATIONS	30,000	14,523	15,778	14,222	53%
TRAVEL	200	-	-	200	0%
EXCISE TAXES	340,000	189,240	183,062	156,938	54%
RENTAL/LEASE	146,130	49,367	71,704	74,426	49%
INSURANCE	56,050	52,335	58,214	(2,164)	104%
UTILITIES	35,000	11,502	13,096	21,904	37%
REPAIRS & MAINTENANCE	17,500	5,328	20,691	(3,191)	118%
MISCELLANEOUS	83,500	34,332	43,211	40,289	52%
INTERGOVERNMENTAL SERVICES	30,000	18,423	12,948	17,052	43%
INTERFUND TAXES	1,229,110	540,263	529,472	699,638	43%
INTERFUND SUBSIDIES	725,820	219,326	215,315	510,505	30%
MACHINERY/EQUIPMENT	25,000	-	-	25,000	0%
CONSTRUCTION PROJECTS	4,488,771	453,681	-	4,488,771	0%
GENERAL OBLIGATION BONDS	2,340	-	-	2,340	0%
REVENUE BONDS	335,510	-	-	335,510	0%
INTERGOVERNMENTAL LOANS	25,840	25,839	25,839	1	100%
INTEREST	246,940	129,008	123,678	123,262	50%
	<u>\$ 13,257,452</u>	<u>\$ 3,656,305</u>	<u>\$ 3,377,086</u>	<u>\$ 9,880,366</u>	<u>25%</u>

Attachment: Complete June Quarterly Financial Report (June Quarterly Financial Report)

**CITY OF EDMONDS
EXPENDITURES BY FUND - DETAIL**

Title	2017 Amended Budget	6/30/2016 Expenditures	6/30/2017 Expenditures	Amount Remaining	% Spent
STORM FUND (422)					
SALARIES AND WAGES	\$ 523,320	\$ 263,505	\$ 312,555	\$ 210,765	60%
OVERTIME	6,000	6,750	6,549	(549)	109%
BENEFITS	262,680	130,472	150,438	112,242	57%
UNIFORMS	6,500	5,210	4,238	2,262	65%
SUPPLIES	46,000	10,382	21,753	24,247	47%
SMALL EQUIPMENT	5,730	23	5,880	(150)	103%
PROFESSIONAL SERVICES	2,185,624	414,875	509,541	1,676,083	23%
COMMUNICATIONS	3,200	1,276	2,879	321	90%
TRAVEL	4,300	300	-	4,300	0%
EXCISE TAXES	55,000	27,043	29,598	25,402	54%
RENTAL/LEASE	264,970	124,000	130,031	134,939	49%
INSURANCE	71,540	63,187	72,028	(488)	101%
UTILITIES	10,500	4,691	5,228	5,272	50%
REPAIR & MAINTENANCE	13,000	2,266	13,729	(729)	106%
MISCELLANEOUS	101,000	25,800	45,328	55,672	45%
INTERGOVERNMENTAL SERVICES	150,000	83,369	56,131	93,869	37%
INTERFUND TAXES AND OPERATING ASSESSMENT	330,430	164,329	181,546	148,884	55%
INTERFUND SUBSIDIES	411,950	91,782	84,737	327,213	21%
CONSTRUCTION PROJECTS	3,459,813	22,583	36,649	3,423,164	1%
GENERAL OBLIGATION BONDS	85,440	-	-	85,440	0%
REVENUE BONDS	164,000	-	-	164,000	0%
INTERGOVERNMENTAL LOANS	32,070	32,063	32,063	8	100%
INTEREST	158,480	83,932	68,881	89,599	43%
	<u>\$ 8,351,547</u>	<u>\$ 1,557,839</u>	<u>\$ 1,769,781</u>	<u>\$ 6,581,766</u>	<u>21%</u>
SEWER FUND (423)					
SALARIES AND WAGES	\$ 1,783,120	\$ 857,530	\$ 841,907	\$ 941,213	47%
OVERTIME	95,000	39,997	51,628	43,372	54%
BENEFITS	784,430	385,485	384,194	400,236	49%
UNIFORMS	9,500	4,717	7,313	2,187	77%
SUPPLIES	417,200	125,809	89,340	327,860	21%
FUEL CONSUMED	80,000	21,000	39,535	40,465	49%
SUPPLIES PURCHASED FOR INV OR RESALE	4,000	-	-	4,000	0%
SMALL EQUIPMENT	62,730	19,596	14,180	48,550	23%
PROFESSIONAL SERVICES	1,837,472	735,582	931,379	906,093	51%
COMMUNICATIONS	43,000	19,231	18,740	24,260	44%
TRAVEL	5,000	1,097	-	5,000	0%
EXCISE TAXES	170,000	96,745	105,124	64,876	62%
RENTAL/LEASE	306,650	89,518	152,080	154,570	50%
INSURANCE	109,270	105,637	115,442	(6,172)	106%
UTILITIES	1,217,860	344,354	363,566	854,294	30%
REPAIR & MAINTENANCE	340,000	139,778	107,476	232,524	32%
MISCELLANEOUS	117,950	28,195	61,965	55,985	53%
INTERGOVERNMENTAL SERVICES	178,000	82,285	44,725	133,275	25%
INTERFUND TAXES AND OPERATING ASSESSMENT	629,140	311,566	342,031	287,109	54%
INTERFUND SUBSIDIES	2,065,570	518,760	354,290	1,711,280	17%
MACHINERY/EQUIPMENT	110,000	-	76,604	33,396	70%
CONSTRUCTION PROJECTS	5,854,736	54,182	978,980	4,875,756	17%
GENERAL OBLIGATION BONDS	132,950	-	-	132,950	0%
REVENUE BONDS	75,510	-	-	75,510	0%
INTERGOVERNMENTAL LOANS	171,240	157,034	157,346	13,894	92%
INTEREST	114,500	61,299	43,880	70,620	38%
OTHER INTEREST & DEBT SERVICE COSTS	-	2,955	2,821	(2,821)	0%
	<u>\$ 16,714,828</u>	<u>\$ 4,202,354</u>	<u>\$ 5,284,546</u>	<u>\$ 11,430,282</u>	<u>32%</u>
BOND RESERVE FUND (424)					
REVENUE BONDS	\$ 680,020	\$ -	\$ -	\$ 680,020	0%
INTEREST	1,308,700	667,442	654,342	654,358	50%
OTHER INTEREST & DEBT SERVICE COSTS	1,000	-	-	1,000	0%
	<u>\$ 1,989,720</u>	<u>\$ 667,442</u>	<u>\$ 654,342</u>	<u>\$ 1,335,378</u>	<u>33%</u>

Attachment: Complete June Quarterly Financial Report (June Quarterly Financial Report)

**CITY OF EDMONDS
EXPENDITURES BY FUND - DETAIL**

Title	2017 Amended Budget	6/30/2016 Expenditures	6/30/2017 Expenditures	Amount Remaining	% Spent
EQUIPMENT RENTAL FUND (511)					
SALARIES AND WAGES	\$ 238,360	\$ 110,075	\$ 118,888	\$ 119,472	50%
OVERTIME	2,000	-	53	1,947	3%
BENEFITS	103,580	48,651	52,275	51,305	50%
UNIFORMS	1,000	600	544	456	54%
SUPPLIES	110,000	46,214	35,649	74,351	32%
FUEL CONSUMED	1,000	-	-	1,000	0%
SUPPLIES PURCHASED FOR INVENTORY/RESALE	294,200	55,716	89,397	204,803	30%
SMALL EQUIPMENT	58,000	4,462	52,788	5,212	91%
PROFESSIONAL SERVICES	44,000	696	1,313	42,687	3%
COMMUNICATIONS	3,000	1,194	1,097	1,903	37%
TRAVEL	1,000	1,000	-	1,000	0%
RENTAL/LEASE	15,350	9,786	7,473	7,877	49%
INSURANCE	29,010	29,967	26,351	2,659	91%
UTILITIES	14,000	6,175	7,051	6,949	50%
REPAIRS & MAINTENANCE	60,000	31,687	28,248	31,752	47%
MISCELLANEOUS	12,000	6,711	4,717	7,283	39%
INTERGOVERNMENTAL SERVICES	2,500	249	13	2,487	1%
MACHINERY/EQUIPMENT	897,000	575,043	681,511	215,489	76%
	<u>\$ 1,886,000</u>	<u>\$ 928,227</u>	<u>\$ 1,107,367</u>	<u>\$ 778,633</u>	<u>59%</u>
TECHNOLOGY RENTAL FUND (512)					
SALARIES AND WAGES	\$ 277,270	\$ -	\$ 138,561	\$ 138,709	50%
OVERTIME	2,000	-	30	1,970	2%
BENEFITS	92,150	-	45,601	46,549	49%
SUPPLIES	5,000	-	11,582	(6,582)	232%
SMALL EQUIPMENT	23,000	-	26,308	(3,308)	114%
PROFESSIONAL SERVICES	114,000	-	16,422	97,578	14%
COMMUNICATIONS	65,270	-	22,090	43,180	34%
TRAVEL	1,500	-	983	517	66%
RENTAL/LEASE	7,850	-	2,743	5,107	35%
REPAIRS & MAINTENANCE	241,390	-	100,470	140,920	42%
MISCELLANEOUS	5,000	-	8,226	(3,226)	165%
MACHINERY/EQUIPMENT	56,000	-	-	56,000	0%
	<u>\$ 890,430</u>	<u>\$ -</u>	<u>\$ 373,016</u>	<u>\$ 517,414</u>	<u>42%</u>
FIREMEN'S PENSION FUND (617)					
BENEFITS	\$ 22,550	\$ 9,024	\$ 10,041	\$ 12,509	45%
PENSION AND DISABILITY PAYMENTS	48,320	12,112	23,296	25,024	48%
PROFESSIONAL SERVICES	1,200	214	-	1,200	0%
	<u>\$ 72,070</u>	<u>\$ 21,351</u>	<u>\$ 33,336</u>	<u>\$ 38,734</u>	<u>46%</u>
TOTAL EXPENDITURE ALL FUNDS	<u>\$ 102,289,751</u>	<u>\$ 37,845,648</u>	<u>\$ 36,603,218</u>	<u>\$ 65,686,533</u>	<u>36%</u>

Attachment: Complete June Quarterly Financial Report (June Quarterly Financial Report)

CITY OF EDMONDS
EXPENDITURES - GENERAL FUND - BY DEPARTMENT IN SUMMARY

Title	2017 Amended Budget	6/30/2016 Expenditures	6/30/2017 Expenditures	Amount Remaining	% Spent
CITY COUNCIL	\$ 376,120	\$ 98,095	\$ 147,770	\$ 228,350	39%
OFFICE OF MAYOR	276,700	127,277	135,664	141,036	49%
HUMAN RESOURCES	456,490	175,479	177,689	278,801	39%
MUNICIPAL COURT	994,140	435,326	488,007	506,133	49%
CITY CLERK	639,670	281,026	328,184	311,486	51%
FINANCE	1,049,560	717,364	543,908	505,652	52%
CITY ATTORNEY	818,780	347,078	397,194	421,586	49%
NON-DEPARTMENTAL	12,713,920	7,828,525	7,243,467	5,470,453	57%
POLICE SERVICES	10,527,420	4,596,670	4,792,427	5,734,993	46%
COMMUNITY SERVICES/ECONOMIC DEV.	568,230	232,729	252,311	315,919	44%
DEVELOPMENT SERVICES	3,056,387	1,183,128	1,291,724	1,764,663	42%
PARKS & RECREATION	4,116,740	1,609,632	1,763,105	2,353,635	43%
PUBLIC WORKS	2,879,870	1,339,534	1,319,723	1,560,147	46%
FACILITIES MAINTENANCE	1,595,028	697,556	792,861	802,167	50%
	<u>\$ 40,069,055</u>	<u>\$ 19,669,418</u>	<u>\$ 19,674,034</u>	<u>\$ 20,395,021</u>	<u>49%</u>

CITY OF EDMONDS
EXPENDITURES - UTILITY- BY FUND IN SUMMARY

Title	2017 Amended Budget	6/30/2016 Expenditures	6/30/2017 Expenditures	Amount Remaining	% Spent
WATER UTILITY FUND	\$ 13,257,452	\$ 3,656,305	\$ 3,377,086	\$ 9,880,366	25%
STORM UTILITY FUND	8,351,547	1,557,839	1,769,781	6,581,766	21%
SEWER/WWTP UTILITY FUND	16,714,828	4,202,354	5,284,546	11,430,282	32%
BOND RESERVE FUND	1,989,720	667,442	654,342	1,335,378	33%
	<u>\$ 40,313,547</u>	<u>\$ 10,083,940</u>	<u>\$ 11,085,755</u>	<u>\$ 29,227,792</u>	<u>27%</u>

Attachment: Complete June Quarterly Financial Report (June Quarterly Financial Report)

CITY OF EDMONDS
EXPENDITURES - GENERAL FUND - BY DEPARTMENT IN DETAIL

Title	2017 Amended Budget	6/30/2016 Expenditures	6/30/2017 Expenditures	Amount Remaining	% Spent
CITY COUNCIL					
SALARIES	\$ 158,170	\$ 51,510	\$ 84,642	\$ 73,528	54%
OVERTIME	1,000	-	-	1,000	0%
BENEFITS	108,520	40,257	51,591	56,929	48%
SUPPLIES	2,000	885	615	1,385	31%
SMALL EQUIPMENT	-	-	285	(285)	0%
PROFESSIONAL SERVICES	62,160	1,432	1,644	60,516	3%
COMMUNICATIONS	3,000	1,501	1,801	1,199	60%
TRAVEL	6,700	1,277	87	6,613	1%
RENTAL/LEASE	9,070	219	4,290	4,780	47%
REPAIRS/MAINTENANCE	500	15	-	500	0%
MISCELLANEOUS	25,000	1,000	2,815	22,185	11%
	<u>\$ 376,120</u>	<u>\$ 98,095</u>	<u>\$ 147,770</u>	<u>\$ 228,350</u>	<u>39%</u>
OFFICE OF MAYOR					
SALARIES	\$ 202,230	\$ 98,741	\$ 101,159	\$ 101,071	50%
BENEFITS	49,670	24,042	24,575	25,095	49%
SUPPLIES	1,500	635	270	1,230	18%
PROFESSIONAL SERVICES	2,000	1,191	1,582	418	79%
COMMUNICATION	1,400	599	697	703	50%
TRAVEL	4,000	538	529	3,471	13%
RENTAL/LEASE	11,450	720	5,572	5,878	49%
MISCELLANEOUS	4,450	813	1,279	3,171	29%
	<u>\$ 276,700</u>	<u>\$ 127,277</u>	<u>\$ 135,664</u>	<u>\$ 141,036</u>	<u>49%</u>
HUMAN RESOURCES					
SALARIES	\$ 249,980	\$ 107,092	\$ 107,186	\$ 142,794	43%
OVERTIME	-	43	369	(369)	0%
BENEFITS	79,390	36,467	36,485	42,905	46%
SUPPLIES	2,300	2,306	393	1,907	17%
SMALL EQUIPMENT	300	-	110	190	37%
PROFESSIONAL SERVICES	78,000	24,266	11,129	66,871	14%
COMMUNICATIONS	700	485	605	95	86%
TRAVEL	1,000	416	305	695	31%
RENTAL/LEASE	19,790	646	11,011	8,779	56%
REPAIR/MAINTENANCE	7,850	-	7,056	794	90%
MISCELLANEOUS	17,180	3,759	3,040	14,140	18%
	<u>\$ 456,490</u>	<u>\$ 175,479</u>	<u>\$ 177,689</u>	<u>\$ 278,801</u>	<u>39%</u>
MUNICIPAL COURT					
SALARIES	\$ 609,690	\$ 282,245	\$ 296,262	\$ 313,428	49%
OVERTIME	600	419	434	166	72%
BENEFITS	222,520	101,792	113,302	109,218	51%
SUPPLIES	6,700	4,263	4,051	2,649	60%
SMALL EQUIPMENT	900	262	232	668	26%
PROFESSIONAL SERVICES	73,000	35,884	40,962	32,038	56%
COMMUNICATIONS	2,100	1,272	1,297	803	62%
TRAVEL	4,500	1,040	1,473	3,027	33%
RENTAL/LEASE	39,280	467	19,752	19,528	50%
REPAIR/MAINTENANCE	1,250	794	537	713	43%
MISCELLANEOUS	33,600	6,886	9,705	23,895	29%
	<u>\$ 994,140</u>	<u>\$ 435,326</u>	<u>\$ 488,007</u>	<u>\$ 506,133</u>	<u>49%</u>

Attachment: Complete June Quarterly Financial Report (June Quarterly Financial Report)

CITY OF EDMONDS
EXPENDITURES - GENERAL FUND - BY DEPARTMENT IN DETAIL

Title	2017 Amended Budget	6/30/2016 Expenditures	6/30/2017 Expenditures	Amount Remaining	% Spent
CITY CLERK					
SALARIES AND WAGES	\$ 326,390	\$ 172,246	\$ 163,275	\$ 163,115	50%
OVERTIME	1,000	1,509	-	1,000	0%
BENEFITS	140,440	63,634	73,321	67,119	52%
SUPPLIES	10,240	2,626	2,475	7,765	24%
SMALL EQUIPMENT	-	610	-	-	0%
PROFESSIONAL SERVICES	28,310	11,331	15,057	13,253	53%
COMMUNICATIONS	50,000	7,093	12,951	37,049	26%
TRAVEL	1,000	-	315	685	31%
RENTAL/LEASE	50,810	8,337	26,155	24,655	51%
REPAIRS & MAINTENANCE	27,480	10,448	30,422	(2,942)	111%
MISCELLANEOUS	4,000	3,193	4,214	(214)	105%
	<u>\$ 639,670</u>	<u>\$ 281,026</u>	<u>\$ 328,184</u>	<u>\$ 311,486</u>	<u>51%</u>
FINANCE					
SALARIES	\$ 696,560	\$ 410,852	\$ 338,586	\$ 357,974	49%
OVERTIME	4,500	1,308	-	4,500	0%
BENEFITS	223,910	139,652	111,689	112,221	50%
SUPPLIES	7,350	2,538	2,608	4,742	35%
SMALL EQUIPMENT	2,650	28,760	906	1,744	34%
PROFESSIONAL SERVICES	32,800	17,762	30,270	2,530	92%
COMMUNICATIONS	2,000	26,068	940	1,060	47%
TRAVEL	2,600	833	940	1,660	36%
RENTAL/LEASE	30,340	3,555	15,832	14,508	52%
REPAIR/MAINTENANCE	38,500	34,479	40,372	(1,872)	105%
MISCELLANEOUS	8,350	51,555	1,765	6,585	21%
	<u>\$ 1,049,560</u>	<u>\$ 717,364</u>	<u>\$ 543,908</u>	<u>\$ 505,652</u>	<u>52%</u>
CITY ATTORNEY					
PROFESSIONAL SERVICES	\$ 818,780	\$ 344,942	\$ 397,194	\$ 421,586	49%
MISCELLANEOUS	-	2,137	-	-	0%
	<u>\$ 818,780</u>	<u>\$ 347,078</u>	<u>\$ 397,194</u>	<u>\$ 421,586</u>	<u>49%</u>
NON-DEPARTMENTAL					
SALARIES	\$ 100,000	\$ -	\$ -	\$ 100,000	0%
BENEFITS - UNEMPLOYMENT	25,000	1,037	811	24,189	3%
SUPPLIES	5,000	2,089	804	4,196	16%
PROFESSIONAL SERVICES	726,430	233,976	247,152	479,278	34%
EXCISE TAXES	6,500	1,699	1,956	4,544	30%
RENTAL/LEASE	10,480	3,600	7,040	3,440	67%
INSURANCE	609,550	538,732	616,495	(6,945)	101%
MISCELLANEOUS	69,000	48,223	49,206	19,794	71%
INTERGOVT SERVICES ¹³	9,741,890	5,236,693	5,823,883	3,918,007	60%
ECA LOAN PAYMENT	135,000	-	-	135,000	0%
INTERFUND SUBSIDIES ¹⁴	1,080,200	1,739,021	482,130	598,070	45%
GENERAL OBLIGATION BOND	164,790	-	-	164,790	0%
INTEREST ON LONG-TERM DEBT	39,580	23,454	13,990	25,591	35%
FISCAL AGENT FEES	500	-	-	500	0%
	<u>\$ 12,713,920</u>	<u>\$ 7,828,525</u>	<u>\$ 7,243,467</u>	<u>\$ 5,470,453</u>	<u>57%</u>

¹³ Difference between 2016 and 2017 is due to completion of Fire District One Retro Payments in 2016.

¹⁴ Differences between 2016 and 2017 are primarily due to final payment of 2014 debt.

CITY OF EDMONDS
EXPENDITURES - GENERAL FUND - BY DEPARTMENT IN DETAIL

Title	2017 Amended Budget	6/30/2016 Expenditures	6/30/2017 Expenditures	Amount Remaining	% Spent
POLICE SERVICES					
SALARIES	\$ 6,209,630	\$ 2,812,687	\$ 2,846,270	\$ 3,363,360	46%
OVERTIME	454,780	211,860	219,956	234,824	48%
HOLIDAY BUYBACK	226,420	2,349	1,324	225,096	1%
BENEFITS	2,332,980	1,090,825	1,113,870	1,219,110	48%
UNIFORMS	75,450	39,223	25,763	49,687	34%
SUPPLIES	88,500	33,652	30,698	57,802	35%
SMALL EQUIPMENT	28,210	16,103	12,724	15,486	45%
PROFESSIONAL SERVICES	117,670	45,981	45,896	71,774	39%
COMMUNICATIONS	50,820	12,897	18,810	32,010	37%
TRAVEL	17,310	6,674	12,874	4,436	74%
RENTAL/LEASE	842,630	294,955	419,497	423,133	50%
REPAIR/MAINTENANCE	15,120	6,065	3,090	12,030	20%
MISCELLANEOUS	46,350	18,124	23,884	22,466	52%
INTERGOVTL SERVICES	10,550	5,274	8,078	2,473	77%
MACHINERY/EQUIPMENT	11,000	-	9,694	1,306	88%
	<u>\$ 10,527,420</u>	<u>\$ 4,596,670</u>	<u>\$ 4,792,427</u>	<u>\$ 5,734,993</u>	<u>46%</u>
COMMUNITY SERVICES/ECON DEV.					
SALARIES	\$ 230,850	\$ 112,969	\$ 115,564	\$ 115,286	50%
BENEFITS	69,230	34,424	35,422	33,808	51%
SUPPLIES	7,000	280	7,064	(64)	101%
SMALL EQUIPMENT	800	125	-	800	0%
PROFESSIONAL SERVICES	238,400	82,038	86,497	151,903	36%
COMMUNICATIONS	1,490	486	594	896	40%
TRAVEL	2,000	-	529	1,472	26%
RENTAL/LEASE	10,960	955	4,820	6,140	44%
REPAIR/MAINTENANCE	500	-	-	500	0%
MISCELLANEOUS	7,000	1,453	1,821	5,179	26%
	<u>\$ 568,230</u>	<u>\$ 232,729</u>	<u>\$ 252,311</u>	<u>\$ 315,919</u>	<u>44%</u>
DEVELOPMENT SERVICES/PLANNING					
SALARIES	\$ 1,547,970	\$ 718,769	\$ 723,279	\$ 824,691	47%
OVERTIME	1,300	6,052	5,850	(4,550)	450%
BENEFITS	568,060	261,658	263,108	304,952	46%
UNIFORMS	500	-	-	500	0%
SUPPLIES	16,100	6,513	6,178	9,922	38%
SMALL EQUIPMENT	6,000	2,745	4,268	1,732	71%
PROFESSIONAL SERVICES	694,567	145,991	193,538	501,029	28%
COMMUNICATIONS	8,700	2,523	4,086	4,614	47%
TRAVEL	4,750	3,945	3,921	829	83%
RENTAL/LEASE	147,580	10,699	74,605	72,975	51%
REPAIRS & MAINTENANCE	6,800	1,177	-	6,800	0%
MISCELLANEOUS	54,060	23,056	12,891	41,169	24%
	<u>\$ 3,056,387</u>	<u>\$ 1,183,128</u>	<u>\$ 1,291,724</u>	<u>\$ 1,764,663</u>	<u>42%</u>
ENGINEERING					
SALARIES	\$ 1,462,570	\$ 624,922	\$ 715,572	\$ 746,998	49%
OVERTIME	5,000	2,890	104	4,896	2%
BENEFITS	572,030	249,286	291,067	280,963	51%
UNIFORMS	360	-	-	360	0%
SUPPLIES	-	79	-	-	0%
SMALL EQUIPMENT	3,930	1,952	2,016	1,914	51%
PROFESSIONAL SERVICES	231,080	252,650	17,453	213,627	8%
COMMUNICATIONS	14,700	4,691	6,380	8,320	43%
TRAVEL	600	296	-	600	0%
RENTAL/LEASE	93,250	11,717	45,177	48,073	48%
REPAIR/MAINTENANCE	2,600	-	587	2,013	23%
MISCELLANEOUS	23,000	12,630	9,349	13,651	41%
	<u>\$ 2,409,120</u>	<u>\$ 1,161,113</u>	<u>\$ 1,087,704</u>	<u>\$ 1,321,416</u>	<u>45%</u>

Attachment: Complete June Quarterly Financial Report (June Quarterly Financial Report)

CITY OF EDMONDS
EXPENDITURES - GENERAL FUND - BY DEPARTMENT IN DETAIL

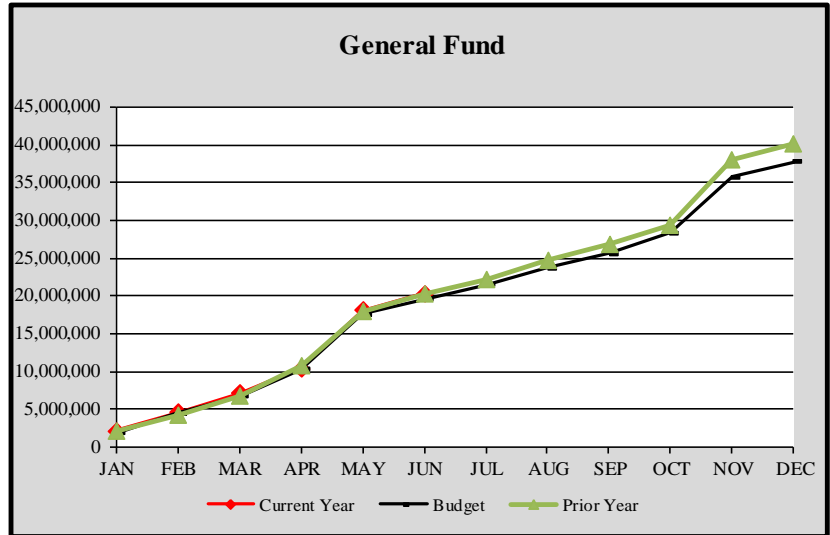
Title	2017 Amended Budget	6/30/2016 Expenditures	6/30/2017 Expenditures	Amount Remaining	% Spent
PARKS & RECREATION					
SALARIES	\$ 1,958,290	\$ 861,110	\$ 940,762	\$ 1,017,528	48%
OVERTIME	10,000	4,928	9,982	18	100%
BENEFITS	724,810	328,840	362,783	362,027	50%
UNIFORMS	5,800	2,643	2,394	3,406	41%
SUPPLIES	117,390	71,171	62,416	54,974	53%
SMALL EQUIPMENT	3,250	6,645	12,330	(9,080)	379%
PROFESSIONAL SERVICES	556,240	96,739	96,252	459,988	17%
COMMUNICATIONS	29,920	9,492	10,223	19,697	34%
TRAVEL	4,470	468	3,589	881	80%
RENTAL/LEASE	283,560	90,716	136,122	147,438	48%
PUBLIC UTILITY	175,000	67,472	66,381	108,619	38%
REPAIR/MAINTENANCE	37,700	31,337	23,374	14,326	62%
MISCELLANEOUS	92,610	33,069	36,498	56,112	39%
MACHINERY/EQUIPMENT	47,000	-	-	47,000	0%
INTERGOVTL SERVICES	70,700	5,000	-	70,700	0%
	<u>\$ 4,116,740</u>	<u>\$ 1,609,632</u>	<u>\$ 1,763,105</u>	<u>\$ 2,353,635</u>	<u>43%</u>
PUBLIC WORKS					
SALARIES	\$ 270,330	\$ 130,896	\$ 137,870	\$ 132,460	51%
OVERTIME	200	-	-	200	0%
BENEFITS	83,880	41,455	43,412	40,468	52%
SUPPLIES	9,600	1,410	2,595	7,005	27%
SMALL EQUIPMENT	1,000	-	-	1,000	0%
PROFESSIONAL SERVICES	200	39	48	152	24%
COMMUNICATIONS	1,350	315	360	990	27%
TRAVEL	500	-	-	500	0%
RENTAL/LEASE	93,990	3,072	46,341	47,649	49%
PUBLIC UTILITY	2,800	1,235	1,335	1,465	48%
REPAIR/MAINTENANCE	1,000	-	-	1,000	0%
MISCELLANEOUS	5,900	-	60	5,840	1%
	<u>\$ 470,750</u>	<u>\$ 178,420</u>	<u>\$ 232,019</u>	<u>\$ 238,731</u>	<u>49%</u>
FACILITIES MAINTENANCE					
SALARIES	695,650	319,055	342,692	352,958	49%
OVERTIME	7,500	3,465	4,017	3,483	54%
BENEFITS	295,620	130,267	148,542	147,078	50%
UNIFORMS	3,000	1,531	3,302	(302)	110%
SUPPLIES	102,829	32,786	40,330	62,499	39%
SMALL EQUIPMENT	3,000	197	417	2,583	14%
PROFESSIONAL SERVICES	18,246	76	13,195	5,051	72%
COMMUNICATIONS	16,000	7,350	8,502	7,498	53%
TRAVEL	1,000	12	-	1,000	0%
RENTAL/LEASE	64,360	26,878	29,680	34,680	46%
PUBLIC UTILITY	280,000	132,454	148,537	131,463	53%
REPAIR/MAINTENANCE	84,500	38,775	51,098	33,402	60%
MISCELLANEOUS	5,000	4,711	2,549	2,451	51%
BUILDINGS	18,323	-	-	18,323	0%
	<u>\$ 1,595,028</u>	<u>\$ 697,556</u>	<u>\$ 792,861</u>	<u>\$ 802,167</u>	<u>50%</u>
TOTAL GENERAL FUND EXPENDITURES	<u>\$ 40,069,055</u>	<u>\$ 19,669,418</u>	<u>\$ 19,674,034</u>	<u>\$ 20,395,021</u>	<u>49%</u>

Attachment: Complete June Quarterly Financial Report (June Quarterly Financial Report)

City of Edmonds, WA
Monthly Revenue Summary-General Fund
2017

General Fund

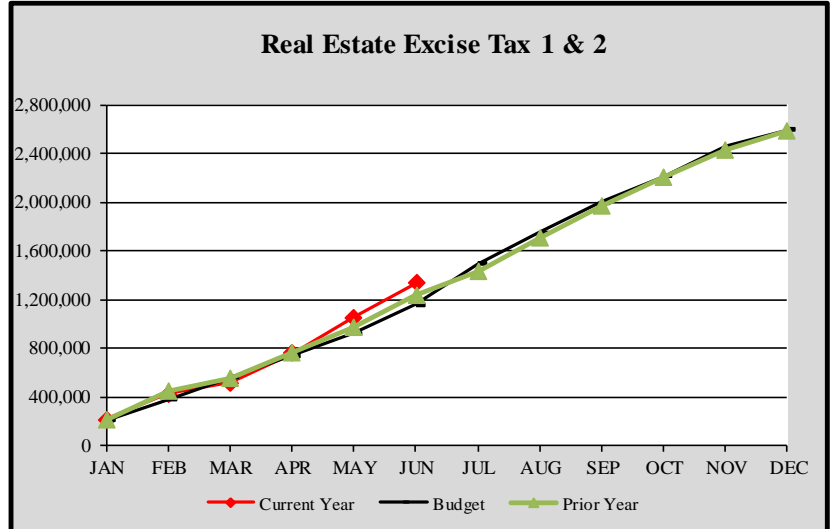
	Cumulative	Monthly	YTD	Variance
	Budget Forecast	Budget Forecast	Actuals	%
January	\$ 1,842,486	\$ 1,842,486	\$ 2,222,449	20.62%
February	4,475,711	2,633,225	4,590,134	2.56%
March	6,692,908	2,217,197	7,111,840	6.26%
April	10,325,016	3,632,108	10,413,847	0.86%
May	17,601,140	7,276,124	18,087,809	2.76%
June	19,548,720	1,947,581	20,280,036	3.74%
July	21,505,534	1,956,814		
August	23,701,818	2,196,284		
September	25,619,471	1,917,652		
October	28,368,775	2,749,305		
November	35,742,399	7,373,624		
December	37,753,480	2,011,081		



City of Edmonds, WA
Monthly Revenue Summary-Real Estate Excise Tax
2017

Real Estate Excise Tax 1 & 2

	Cumulative	Monthly	YTD	Variance
	Budget Forecast	Budget Forecast	Actuals	%
January	\$ 207,208	\$ 207,208	\$ 214,967	3.74%
February	377,258	170,050	425,156	12.70%
March	561,933	184,674	517,163	-7.97%
April	738,924	176,991	759,746	2.82%
May	921,191	182,267	1,057,416	14.79%
June	1,156,829	235,638	1,336,234	15.51%
July	1,491,329	334,500		
August	1,747,148	255,819		
September	1,998,654	251,505		
October	2,216,068	217,414		
November	2,456,999	240,931		
December	2,600,000	143,001		



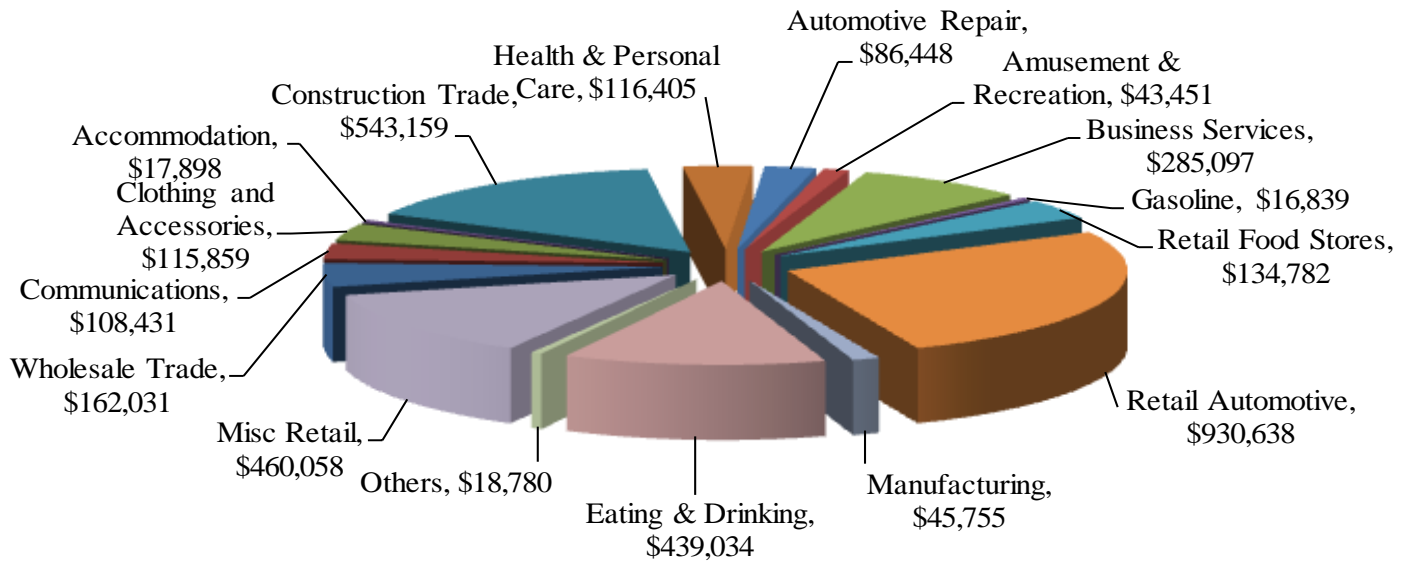
**The monthly budget forecast columns are based on a five-year average.*

Sales Tax Analysis By Category

Current Period: June 2017

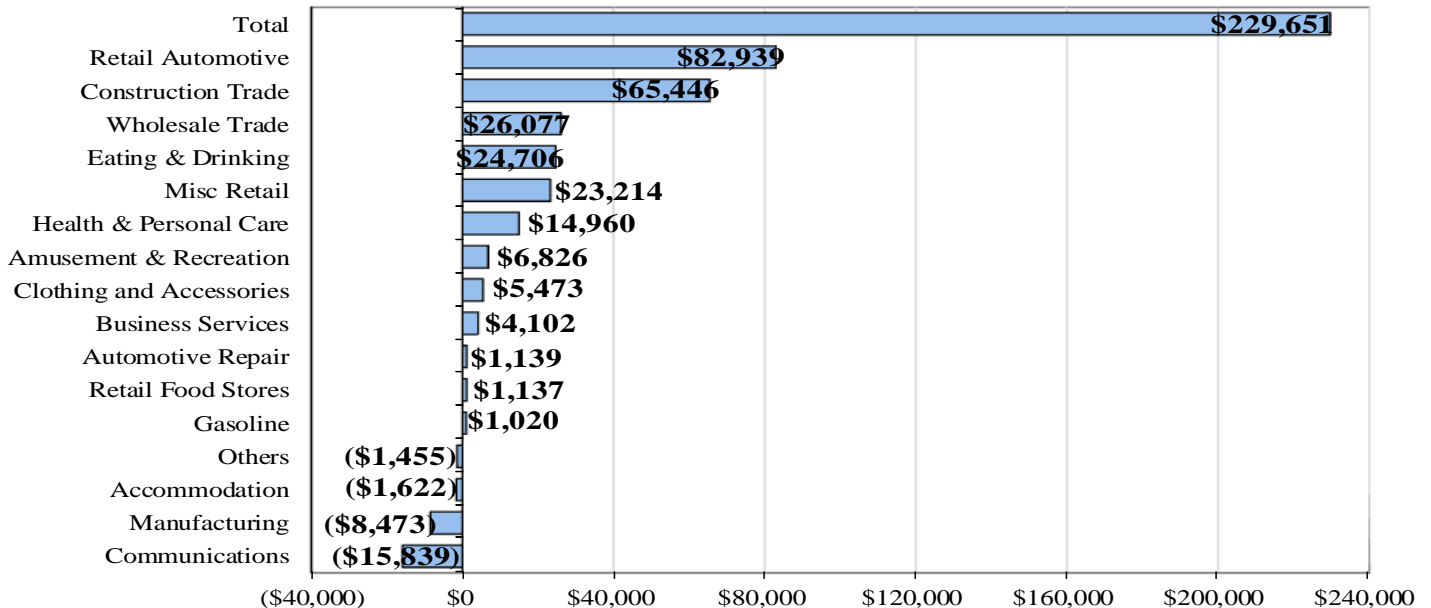
Year-to-Date

Total \$3,524,666



Change in Sales Tax Revenue:

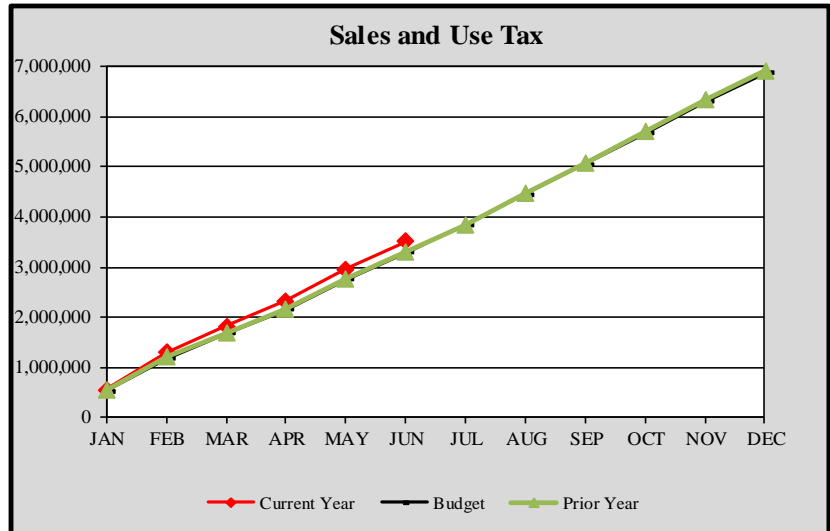
June 2017 compared to June 2016



City of Edmonds, WA
Monthly Revenue Summary-Sales and Use Tax
2017

Sales and Use Tax

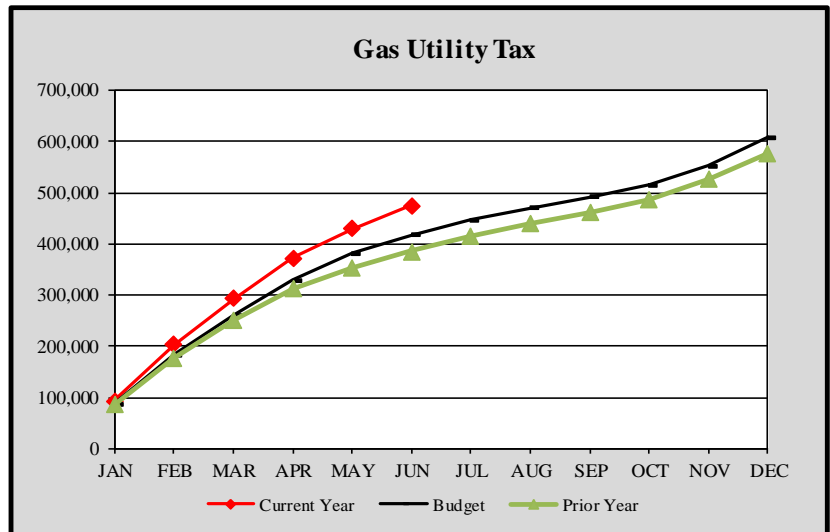
	Cumulative	Monthly	YTD	Variance
	Budget Forecast	Budget Forecast	Actuals	%
January	\$ 528,142	\$ 528,142	\$ 554,719	5.03%
February	1,179,855	651,712	1,307,546	10.82%
March	1,679,874	500,020	1,829,850	8.93%
April	2,159,477	479,603	2,327,446	7.78%
May	2,755,486	596,009	2,970,761	7.81%
June	3,293,571	538,085	3,524,666	7.02%
July	3,847,001	553,430		
August	4,458,173	611,171		
September	5,050,960	592,787		
October	5,666,829	615,869		
November	6,303,607	636,779		
December	6,875,000	571,393		



City of Edmonds, WA
Monthly Revenue Summary-Gas Utility Tax
2017

Gas Utility Tax

	Cumulative	Monthly	YTD	Variance
	Budget Forecast	Budget Forecast	Actuals	%
January	\$ 86,887	\$ 86,887	\$ 93,550	7.67%
February	183,529	96,642	203,652	10.96%
March	260,361	76,832	294,027	12.93%
April	328,823	68,462	372,490	13.28%
May	381,147	52,324	429,203	12.61%
June	416,950	35,802	475,324	14.00%
July	446,516	29,566		
August	469,952	23,437		
September	491,313	21,360		
October	515,645	24,332		
November	552,451	36,806		
December	608,000	55,549		

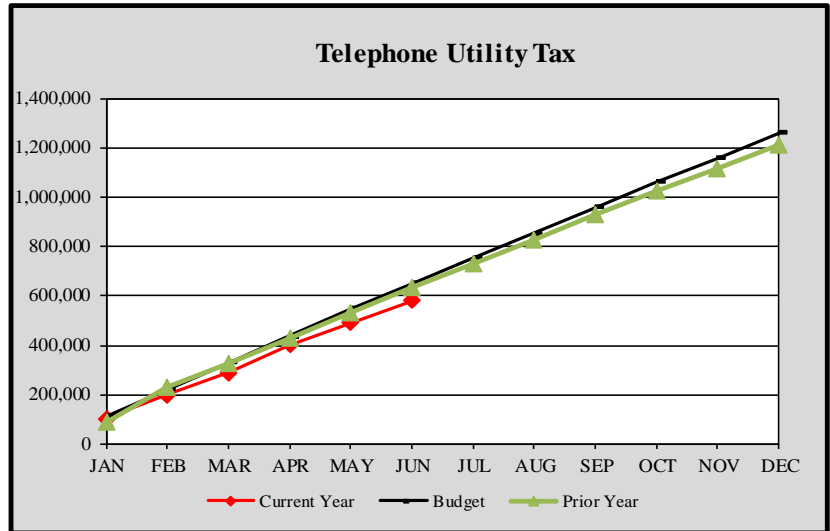


**The monthly budget forecast columns are based on a five-year average.*

City of Edmonds, WA
Monthly Revenue Summary-Telephone Utility Tax
2017

Telephone Utility Tax

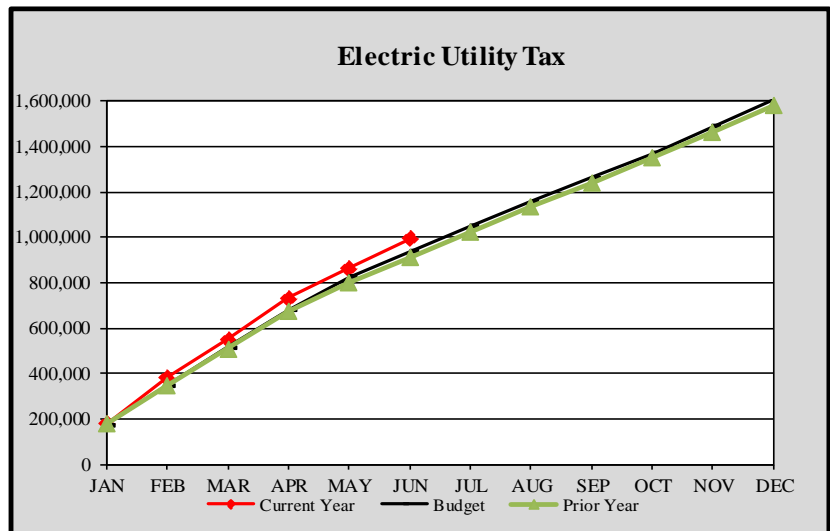
	Cumulative		Monthly		YTD	Variance
	Budget Forecast		Budget Forecast		Actuals	%
January	\$ 114,426	\$	114,426	\$	105,424	-7.87%
February	223,803		109,377		199,068	-11.05%
March	330,274		106,471		290,145	-12.15%
April	436,772		106,497		403,189	-7.69%
May	549,330		112,558		492,059	-10.43%
June	651,204		101,874		582,703	-10.52%
July	754,077		102,873			
August	855,646		101,570			
September	961,054		105,407			
October	1,062,454		101,400			
November	1,159,179		96,725			
December	1,263,200		104,021			



City of Edmonds, WA
Monthly Revenue Summary-Electric Utility Tax
2017

Electric Utility Tax

	Cumulative		Monthly		YTD	Variance
	Budget Forecast		Budget Forecast		Actuals	%
January	\$ 171,830	\$	171,830	\$	175,773	2.29%
February	347,448		175,618		382,822	10.18%
March	515,770		168,322		549,954	6.63%
April	678,100		162,330		729,784	7.62%
May	824,207		146,107		863,230	4.73%
June	937,922		113,715		991,566	5.72%
July	1,049,475		111,553			
August	1,155,197		105,722			
September	1,260,458		105,261			
October	1,368,262		107,804			
November	1,486,788		118,526			
December	1,604,000		117,212			



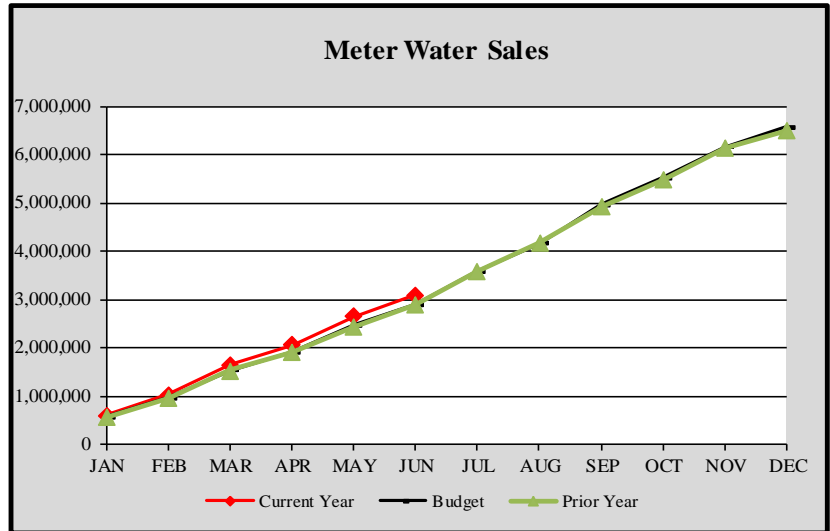
**The monthly budget forecast columns are based on a five-year average.*

Attachment: Complete June Quarterly Financial Report (June Quarterly Financial Report)

City of Edmonds, WA
Monthly Revenue Summary-Meter Water Sales
2017

Meter Water Sales

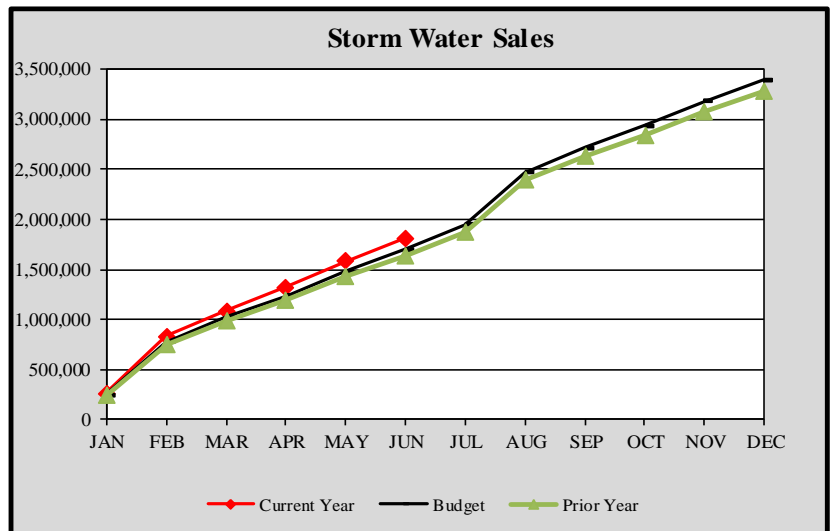
	Cumulative		Monthly		YTD	Variance
	Budget Forecast		Budget Forecast		Actuals	%
January	\$ 573,159	\$	573,159	\$	599,648	4.62%
February	966,145		392,986		1,027,975	6.40%
March	1,529,375		563,229		1,658,808	8.46%
April	1,906,725		377,350		2,057,951	7.93%
May	2,464,794		558,070		2,660,519	7.94%
June	2,900,426		435,631		3,101,835	6.94%
July	3,585,922		685,496			
August	4,167,341		581,419			
September	4,962,652		795,311			
October	5,517,073		554,421			
November	6,167,530		650,457			
December	6,572,750		405,220			



City of Edmonds, WA
Monthly Revenue Summary-Storm Water Sales
2017

Storm Water Sales

	Cumulative		Monthly		YTD	Variance
	Budget Forecast		Budget Forecast		Actuals	%
January	\$ 245,291	\$	245,291	\$	261,646	2.29%
February	773,850		528,559		828,655	10.18%
March	1,018,952		245,102		1,089,708	6.63%
April	1,236,230		217,279		1,322,203	7.62%
May	1,481,659		245,428		1,583,130	4.73%
June	1,699,683		218,024		1,815,097	5.72%
July	1,946,434		246,751			
August	2,474,151		527,717			
September	2,718,865		244,714			
October	2,936,691		217,826			
November	3,181,934		245,242			
December	3,400,000		218,066			

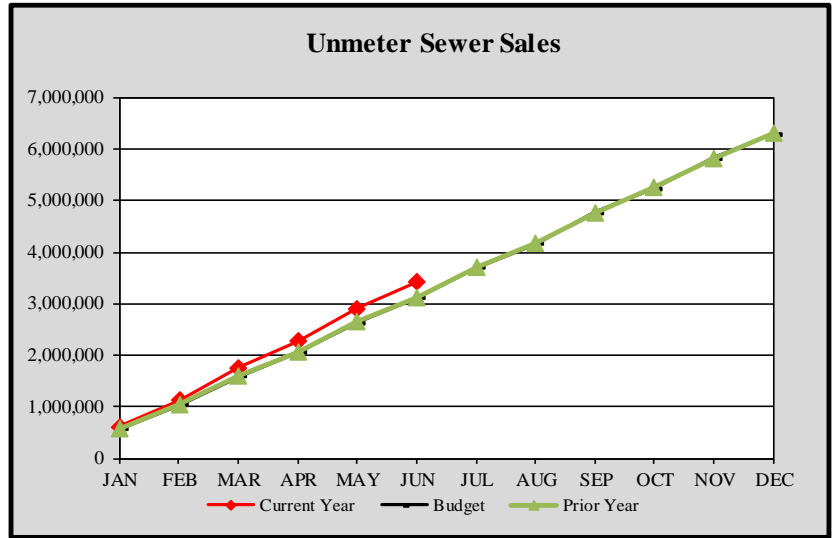


**The monthly budget forecast columns are based on a five-year average.*

City of Edmonds, WA
Monthly Revenue Summary-Unmeter Sewer Sales
2017

Unmeter Sewer Sales

	Cumulative	Monthly	YTD	Variance
	Budget Forecast	Budget Forecast	Actuals	%
January	\$ 572,309	\$ 572,309	\$ 623,336	8.92%
February	1,034,805	462,496	1,135,356	9.72%
March	1,603,135	568,330	1,767,521	10.25%
April	2,067,863	464,728	2,277,537	10.14%
May	2,638,929	571,065	2,905,981	10.12%
June	3,109,116	470,187	3,422,308	10.07%
July	3,696,739	587,623		
August	4,167,756	471,017		
September	4,763,468	595,712		
October	5,237,994	474,526		
November	5,818,870	580,876		
December	6,291,410	472,540		



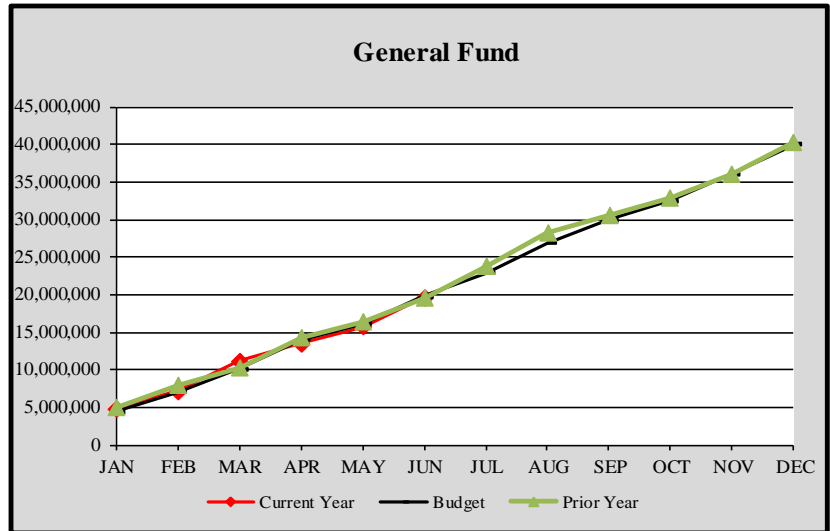
Attachment: Complete June Quarterly Financial Report (June Quarterly Financial Report)

**The monthly budget forecast columns are based on a five-year average.*

City of Edmonds, WA
Monthly Expenditure Report-General Fund
2017

General Fund

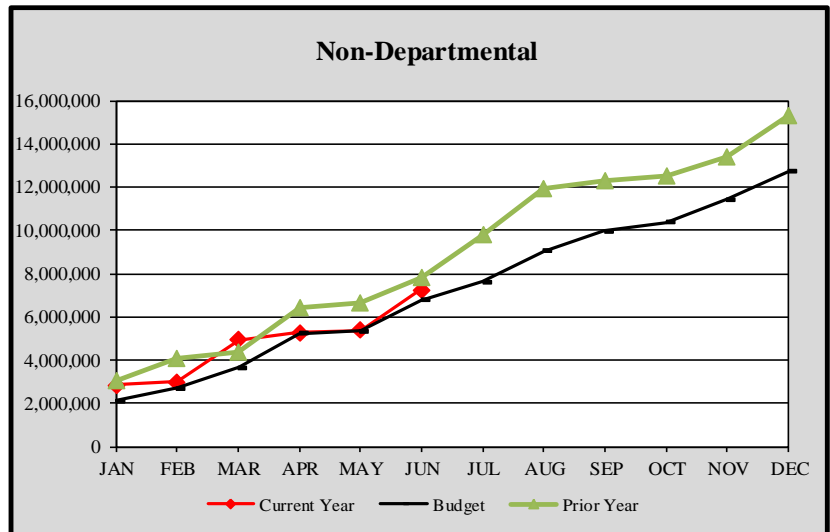
	Cumulative	Monthly	YTD	Variance
	Budget Forecast	Budget Forecast	Actuals	%
January	\$ 4,409,595	\$ 4,409,595	\$ 4,856,792	10.14%
February	7,042,288	2,632,692	7,118,110	1.08%
March	10,151,670	3,109,383	11,124,815	9.59%
April	14,001,703	3,850,033	13,496,549	-3.61%
May	16,128,455	2,126,752	15,723,883	-2.51%
June	19,812,535	3,684,080	19,674,034	-0.70%
July	22,947,235	3,134,700		
August	26,870,574	3,923,339		
September	29,983,441	3,112,867		
October	32,535,825	2,552,384		
November	36,144,968	3,609,144		
December	40,069,055	3,924,087		



City of Edmonds, WA
Monthly Expenditure Report-Non-Departmental
2017

Non-Departmental

	Cumulative	Monthly	YTD	Variance
	Budget Forecast	Budget Forecast	Actuals	%
January	\$ 2,146,531	\$ 2,146,531	\$ 2,867,736	33.60%
February	2,697,696	551,165	2,992,431	10.93%
March	3,672,424	974,728	4,967,917	35.28%
April	5,229,897	1,557,473	5,263,262	0.64%
May	5,372,743	142,846	5,385,911	0.25%
June	6,788,429	1,415,687	7,243,467	6.70%
July	7,638,067	849,638		
August	9,076,194	1,438,127		
September	9,979,289	903,095		
October	10,369,720	390,431		
November	11,451,431	1,081,711		
December	12,713,920	1,262,489		

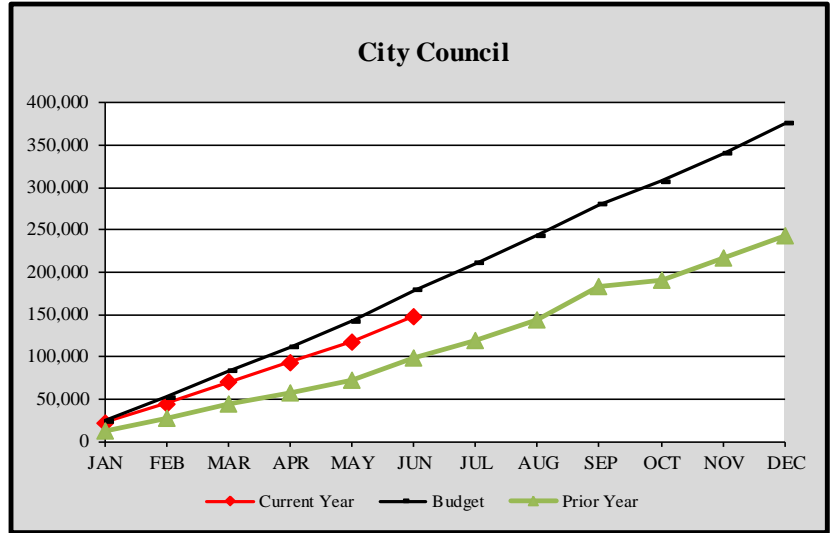


**The monthly budget forecast columns are based on a five-year average.*

City of Edmonds, WA
Monthly Expenditure Report-City Council
2017

City Council

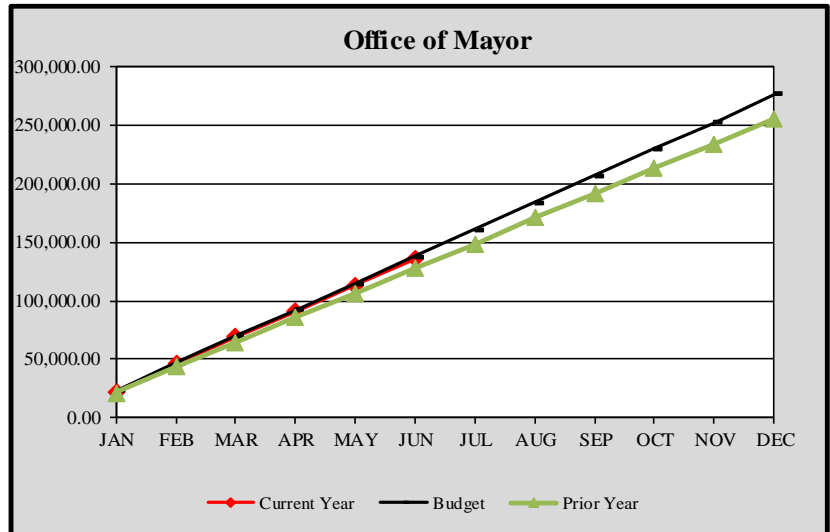
	Cumulative		Monthly		YTD	Variance
	Budget Forecast		Budget Forecast		Actuals	%
January	\$ 24,382	\$	24,382	\$	22,742	-6.73%
February	52,318		27,936		44,954	-14.07%
March	83,798		31,480		70,026	-16.43%
April	111,661		27,864		93,544	-16.23%
May	142,110		30,449		118,441	-16.66%
June	179,407		37,297		147,770	-17.63%
July	211,248		31,841			
August	243,636		32,388			
September	280,710		37,075			
October	307,316		26,606			
November	340,142		32,826			
December	376,120		35,978			



City of Edmonds, WA
Monthly Expenditure Report-Office of Mayor
2017

Office of Mayor

	Cumulative		Monthly		YTD	Variance
	Budget Forecast		Budget Forecast		Actuals	%
January	\$ 23,100	\$	23,100	\$	22,135	-4.18%
February	46,663		23,563		46,116	-1.17%
March	69,540		22,877		68,939	-0.87%
April	92,124		22,584		91,090	-1.12%
May	114,857		22,733		113,581	-1.11%
June	137,580		22,723		135,664	-1.39%
July	160,958		23,378			
August	184,229		23,271			
September	207,064		22,834			
October	230,074		23,010			
November	252,731		22,657			
December	276,700		23,969			

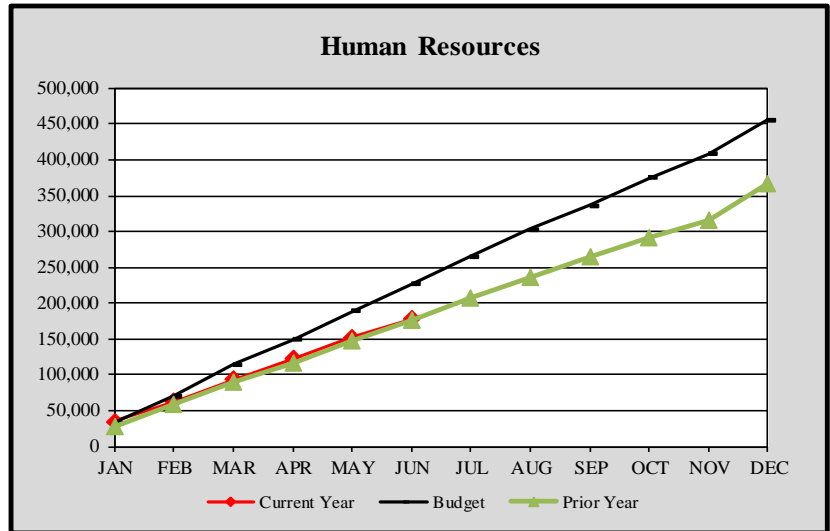


**The monthly budget forecast columns are based on a five-year average.*

City of Edmonds, WA
Monthly Expenditure Report-Human Resources
2017

Human Resources

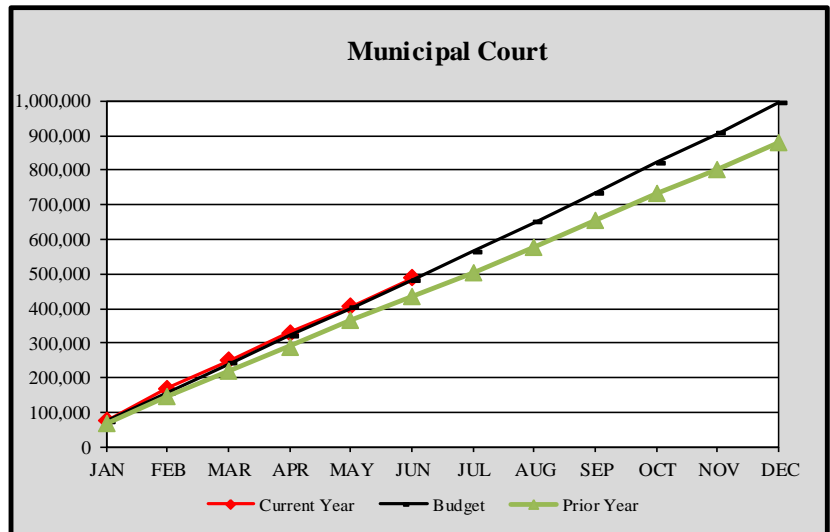
	Cumulative		Monthly		YTD	Variance
	Budget Forecast		Budget Forecast		Actuals	%
January	\$ 33,336	\$	33,336	\$	35,109	5.32%
February	71,046		37,709		63,054	-11.25%
March	115,092		44,047		93,670	-18.61%
April	148,861		33,769		122,632	-17.62%
May	188,582		39,721		152,171	-19.31%
June	226,982		38,400		177,689	-21.72%
July	266,054		39,073			
August	302,670		36,616			
September	336,201		33,531			
October	375,053		38,852			
November	408,108		33,055			
December	456,490		48,382			



City of Edmonds, WA
Monthly Expenditure Report-Municipal Court
2017

Municipal Court

	Cumulative		Monthly		YTD	Variance
	Budget Forecast		Budget Forecast		Actuals	%
January	\$ 74,527	\$	74,527	\$	76,438	2.56%
February	157,040		82,513		170,699	8.70%
March	242,535		85,495		249,970	3.07%
April	321,521		78,987		330,859	2.90%
May	402,116		80,595		407,638	1.37%
June	482,636		80,519		488,007	1.11%
July	564,050		81,414			
August	649,652		85,602			
September	733,470		83,818			
October	821,328		87,857			
November	906,841		85,513			
December	994,140		87,299			

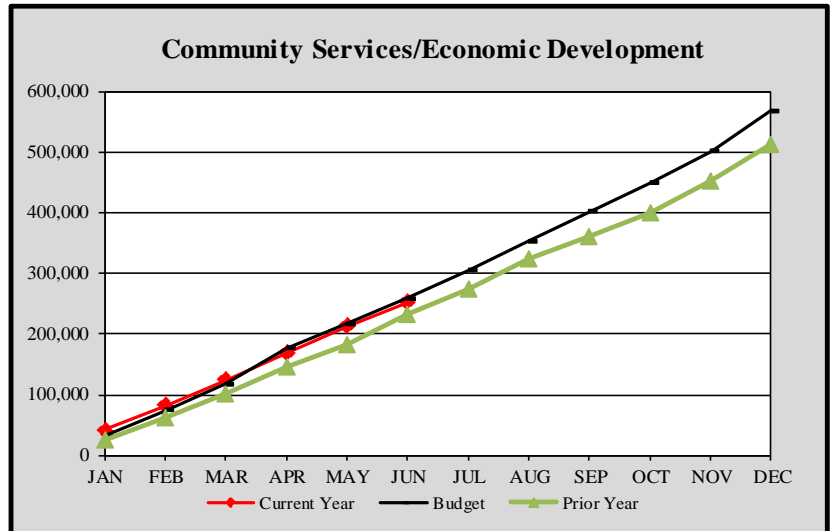


**The monthly budget forecast columns are based on a five-year average.*

City of Edmonds, WA
Monthly Expenditure Report-Community Services/Economic Development
2017

Community Services/Economic Development

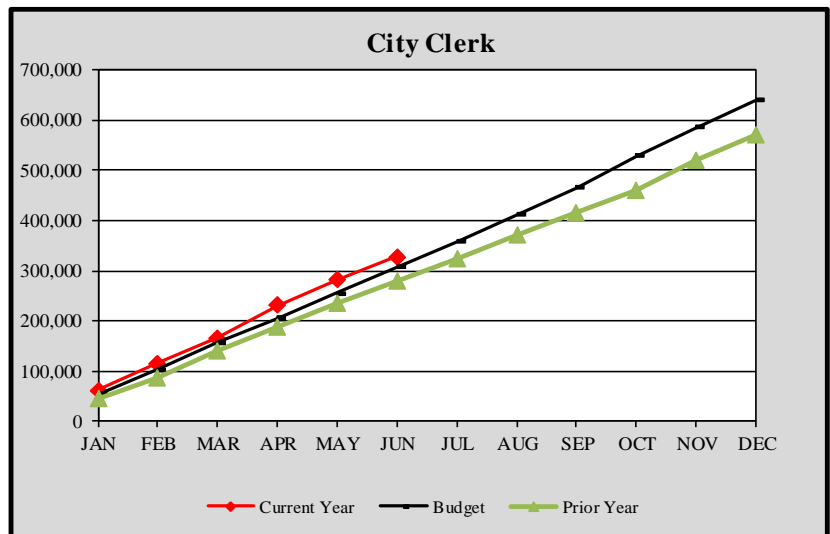
	Cumulative	Monthly	YTD	Variance
	Budget Forecast	Budget Forecast	Actuals	%
January	\$ 31,803	\$ 31,803	\$ 41,399	30.17%
February	74,567	42,764	82,958	11.25%
March	117,487	42,920	124,227	5.74%
April	176,878	59,391	168,847	-4.54%
May	217,526	40,649	213,705	-1.76%
June	259,704	42,178	252,311	-2.85%
July	305,867	46,163		
August	354,338	48,470		
September	402,092	47,754		
October	449,860	47,768		
November	502,009	52,150		
December	568,230	66,221		



City of Edmonds, WA
Monthly Expenditure Report-City Clerk
2017

City Clerk

	Cumulative	Monthly	YTD	Variance
	Budget Forecast	Budget Forecast	Actuals	%
January	\$ 53,698	\$ 53,698	\$ 63,151	17.60%
February	104,118	50,421	115,829	11.25%
March	156,948	52,830	165,932	5.72%
April	206,989	50,041	232,085	12.12%
May	256,099	49,110	282,468	10.30%
June	307,065	50,966	328,184	6.88%
July	358,855	51,790		
August	412,651	53,796		
September	465,765	53,115		
October	529,589	63,824		
November	585,495	55,906		
December	639,670	54,175		

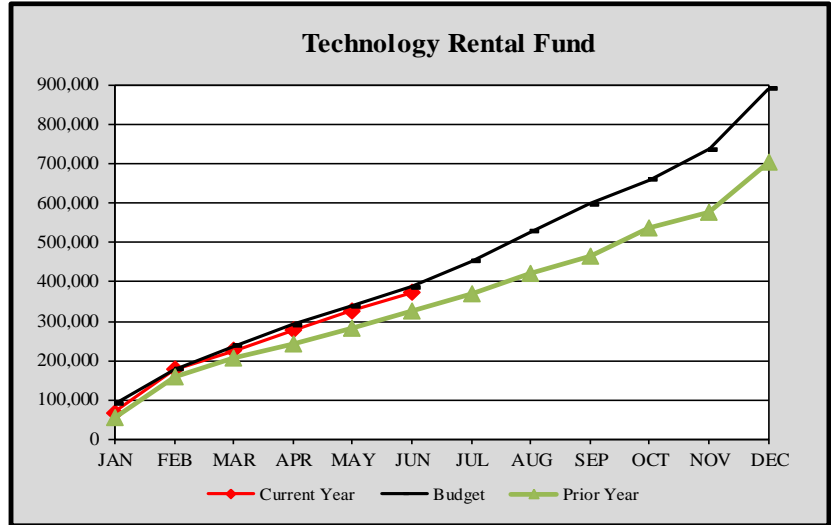


**The monthly budget forecast columns are based on a five-year average.*

City of Edmonds, WA
Monthly Expenditure Report-Technology Rental Fund
2017

Technology Rental Fund

	Cumulative	Monthly	YTD	Variance
	Budget Forecast	Budget Forecast	Actuals	%
January	\$ 89,050	\$ 89,050	\$ 68,612	-22.95%
February	177,771	88,721	177,236	-0.30%
March	237,540	59,769	225,779	-4.95%
April	291,722	54,182	277,784	-4.78%
May	338,118	46,396	327,297	-3.20%
June	387,726	49,608	373,016	-3.79%
July	453,064	65,338		
August	528,249	75,186		
September	598,969	70,720		
October	660,256	61,287		
November	738,213	77,957		
December	890,430	152,217		

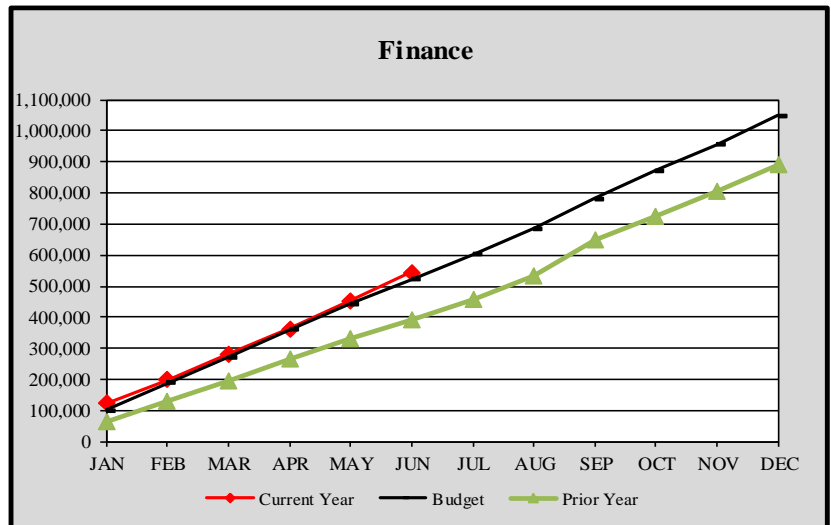


Prior Year amounts are from the Information Services Budget

City of Edmonds, WA
Monthly Expenditure Report-Finance
2017

Finance

	Cumulative	Monthly	YTD	Variance
	Budget Forecast	Budget Forecast	Actuals	%
January	\$ 102,207	\$ 102,207	\$ 122,581	19.93%
February	189,948	87,740	199,380	4.97%
March	271,844	81,896	281,459	3.54%
April	361,256	89,412	364,229	0.82%
May	442,527	81,271	453,448	2.47%
June	522,997	80,470	543,908	4.00%
July	603,839	80,842		
August	687,233	83,394		
September	781,183	93,949		
October	871,230	90,048		
November	956,025	84,794		
December	1,049,560	93,535		



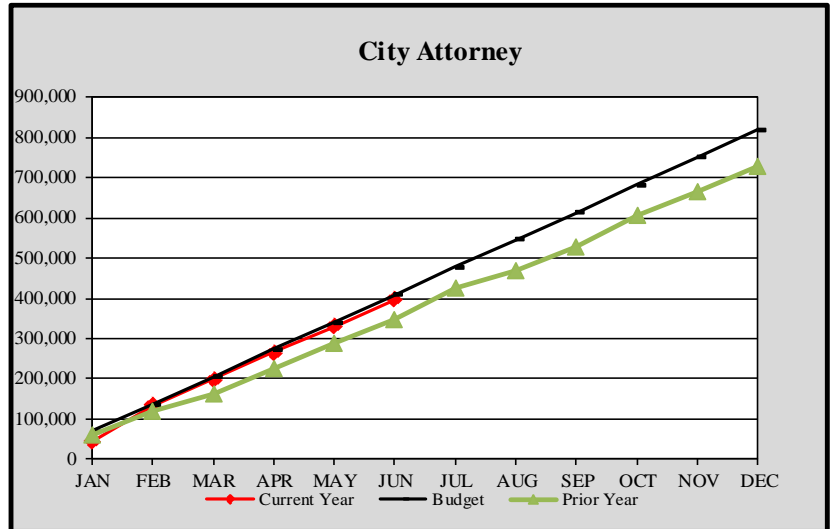
**The monthly budget forecast columns are based on a five-year average.*

Attachment: Complete June Quarterly Financial Report (June Quarterly Financial Report)

City of Edmonds, WA
Monthly Expenditure Report-City Attorney
2017

City Attorney

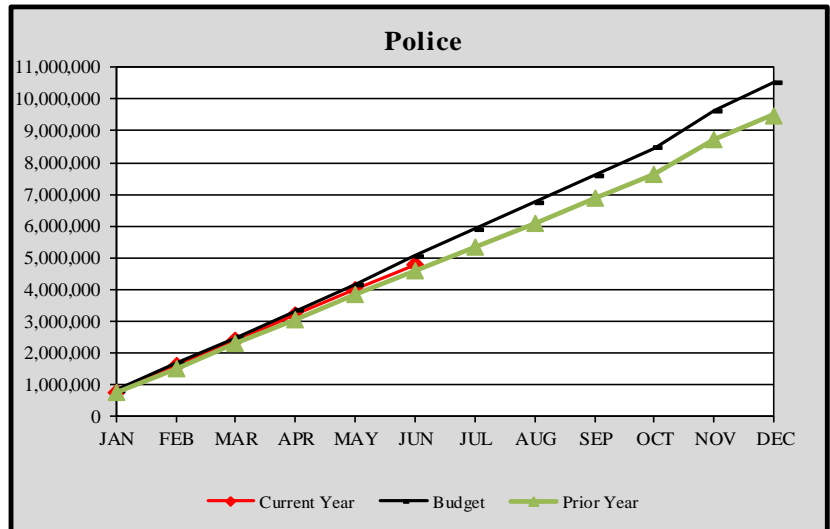
	Cumulative	Monthly	YTD	Variance
	Budget Forecast	Budget Forecast	Actuals	%
January	\$ 68,232	\$ 68,232	\$ 44,346	-35.01%
February	136,463	68,232	133,495	-2.18%
March	204,695	68,232	199,483	-2.55%
April	272,927	68,232	265,386	-2.76%
May	341,158	68,232	331,290	-2.89%
June	409,390	68,232	397,194	-2.98%
July	477,621	68,232		
August	545,853	68,232		
September	614,085	68,232		
October	682,316	68,232		
November	750,548	68,232		
December	818,780	68,232		



City of Edmonds, WA
Monthly Expenditure Report-Police
2017

Police

	Cumulative	Monthly	YTD	Variance
	Budget Forecast	Budget Forecast	Actuals	%
January	\$ 829,943	\$ 829,943	\$ 780,892	-5.91%
February	1,662,470	832,528	1,625,589	-2.22%
March	2,505,809	843,339	2,399,391	-4.25%
April	3,339,581	833,772	3,219,838	-3.59%
May	4,168,368	828,787	4,002,270	-3.98%
June	5,053,459	885,092	4,792,427	-5.17%
July	5,891,147	837,688		
August	6,741,701	850,554		
September	7,584,219	842,517		
October	8,462,646	878,427		
November	9,638,182	1,175,536		
December	10,527,420	889,238		

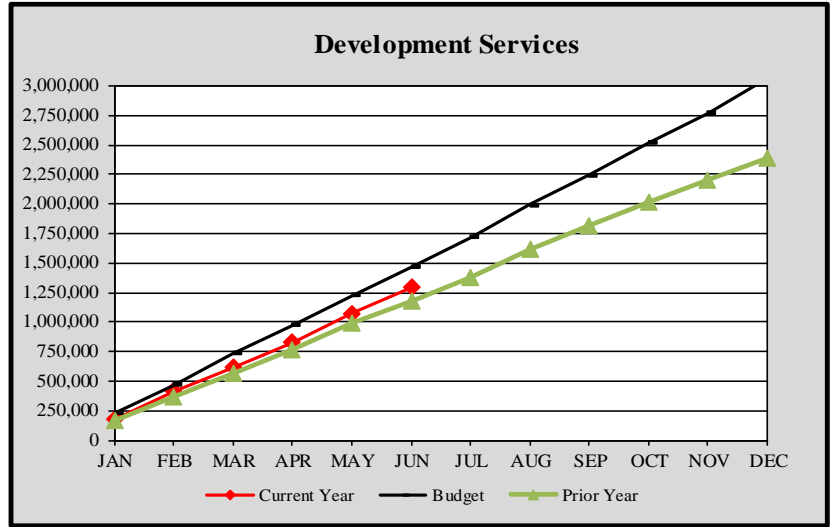


**The monthly budget forecast columns are based on a five-year average.*

City of Edmonds, WA
Monthly Expenditure Report-Development Services
2017

Development Services

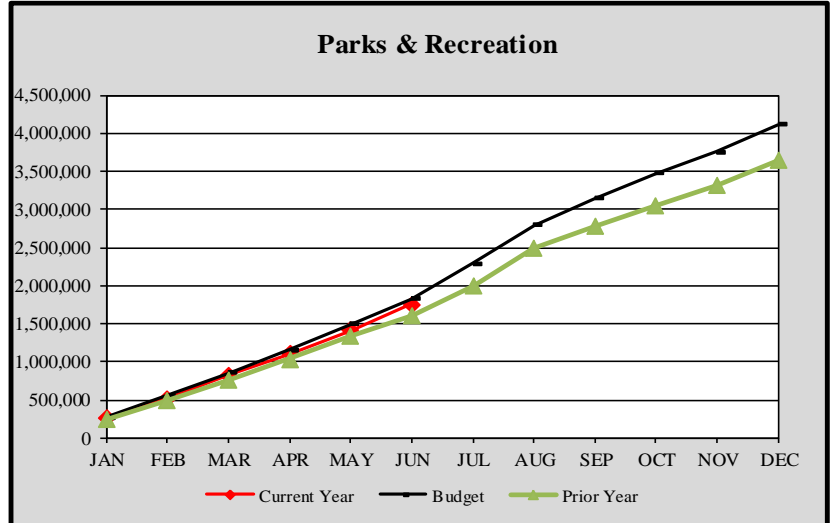
	Cumulative	Monthly	YTD	Variance
	Budget Forecast	Budget Forecast	Actuals	%
January	\$ 230,681	\$ 230,681	\$ 177,820	-22.92%
February	471,547	240,866	416,189	-11.74%
March	736,403	264,856	613,958	-16.63%
April	972,117	235,714	831,603	-14.45%
May	1,226,401	254,284	1,076,107	-12.25%
June	1,468,275	241,874	1,291,724	-12.02%
July	1,721,583	253,308		
August	1,989,604	268,020		
September	2,242,988	253,384		
October	2,519,625	276,636		
November	2,770,925	251,300		
December	3,056,387	285,462		



City of Edmonds, WA
Monthly Expenditure Report-Parks & Recreation
2017

Parks & Recreation

	Cumulative	Monthly	YTD	Variance
	Budget Forecast	Budget Forecast	Actuals	%
January	\$ 274,212	\$ 274,212	\$ 260,128	-5.14%
February	559,410	285,198	525,411	-6.08%
March	859,833	300,423	828,782	-3.61%
April	1,163,949	304,116	1,110,332	-4.61%
May	1,496,601	332,653	1,417,439	-5.29%
June	1,827,076	330,474	1,763,105	-3.50%
July	2,294,141	467,065		
August	2,797,570	503,430		
September	3,153,190	355,619		
October	3,474,134	320,944		
November	3,754,468	280,335		
December	4,116,740	362,272		

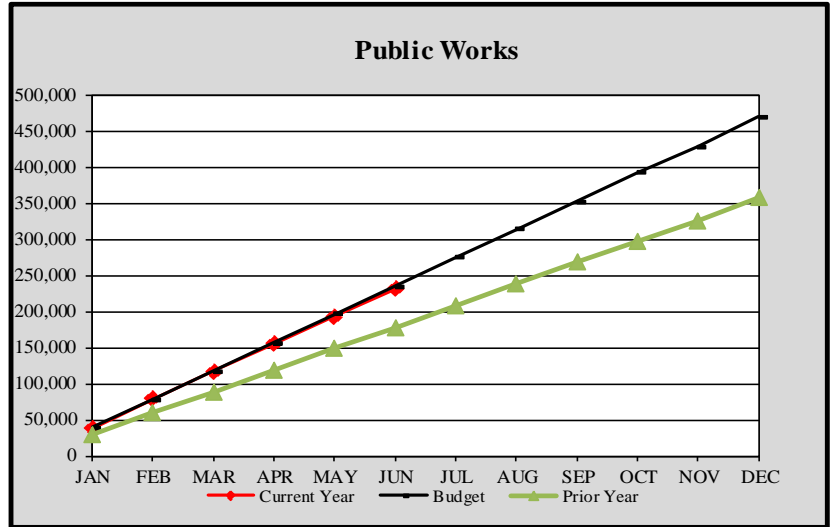


**The monthly budget forecast columns are based on a five-year average.*

City of Edmonds, WA
Monthly Expenditure Report-Public Works
2017

Public Works

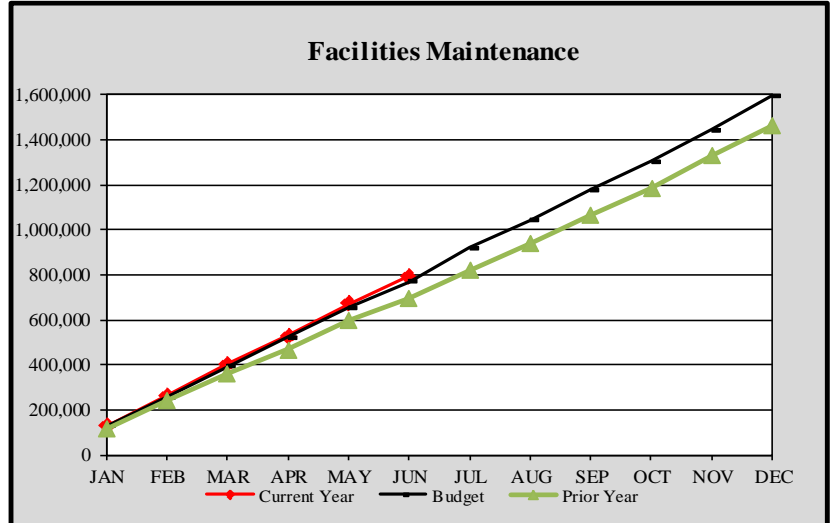
	Cumulative	Monthly	YTD	Variance
	Budget Forecast	Budget Forecast	Actuals	%
January	\$ 38,606	\$ 38,606	\$ 37,599	-2.61%
February	77,835	39,229	79,084	1.61%
March	117,100	39,265	117,357	0.22%
April	156,588	39,488	155,173	-0.90%
May	196,390	39,802	193,212	-1.62%
June	235,491	39,101	232,019	-1.47%
July	275,204	39,712		
August	314,352	39,148		
September	352,464	38,113		
October	393,232	40,768		
November	429,298	36,066		
December	470,750	41,452		



City of Edmonds, WA
Monthly Expenditure Report-Facilities Maintenance
2017

Facilities Maintenance

	Cumulative	Monthly	YTD	Variance
	Budget Forecast	Budget Forecast	Actuals	%
January	\$ 127,054	\$ 127,054	\$ 128,273	0.96%
February	257,275	130,221	264,009	2.62%
March	393,296	136,021	404,318	2.80%
April	523,097	129,801	528,660	1.06%
May	653,776	130,679	672,777	2.91%
June	769,798	116,022	792,861	3.00%
July	920,705	150,908		
August	1,041,207	120,502		
September	1,179,252	138,045		
October	1,302,136	122,884		
November	1,442,204	140,068		
December	1,595,028	152,824		

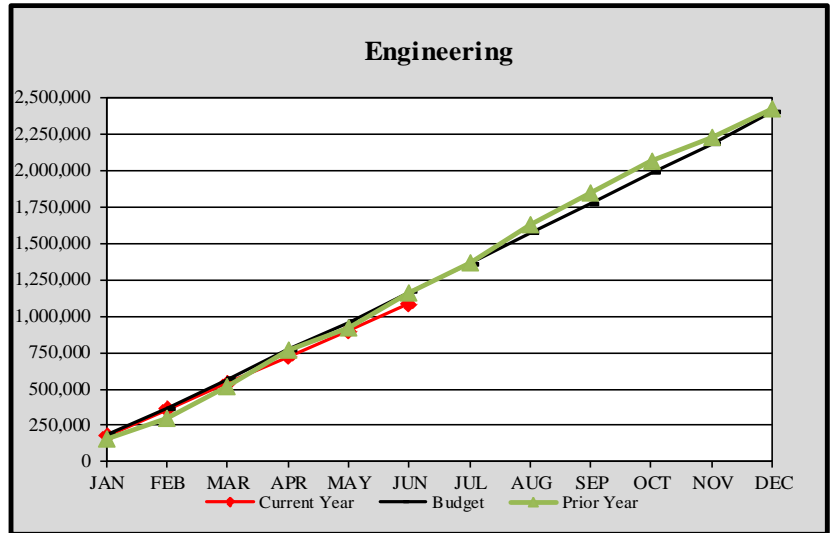


**The monthly budget forecast columns are based on a five-year average.*

City of Edmonds, WA
Monthly Expenditure Report-Engineering
2017

Engineering

	Cumulative	Monthly	YTD	Variance
	Budget Forecast	Budget Forecast	Actuals	%
January	\$ 186,081	\$ 186,081	\$ 176,444	-5.18%
February	368,560	182,479	358,913	-2.62%
March	568,196	199,636	539,386	-5.07%
April	770,455	202,259	719,010	-6.68%
May	962,858	192,403	903,426	-6.17%
June	1,167,015	204,157	1,087,704	-6.80%
July	1,360,802	193,788		
August	1,570,747	209,945		
September	1,772,816	202,069		
October	1,985,415	212,599		
November	2,181,969	196,554		
December	2,409,120	227,151		



Attachment: Complete June Quarterly Financial Report (June Quarterly Financial Report)

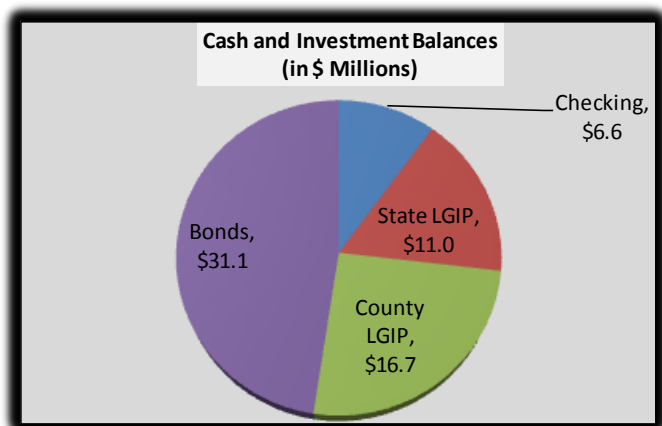
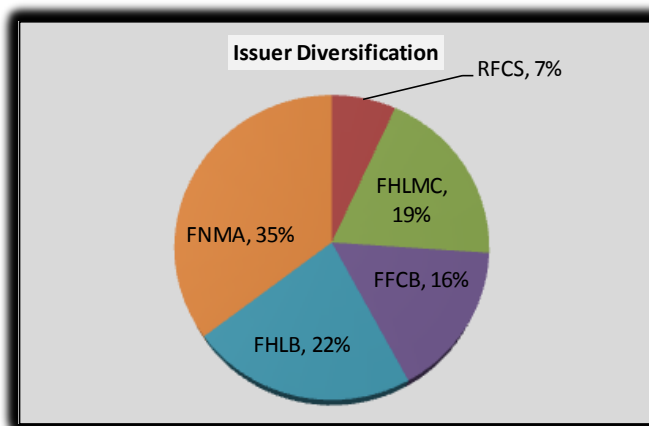
**The monthly budget forecast columns are based on a five-year average.*

INVESTMENT PORTFOLIO SUMMARY

City of Edmonds Investment Portfolio Detail As of June 30, 2017

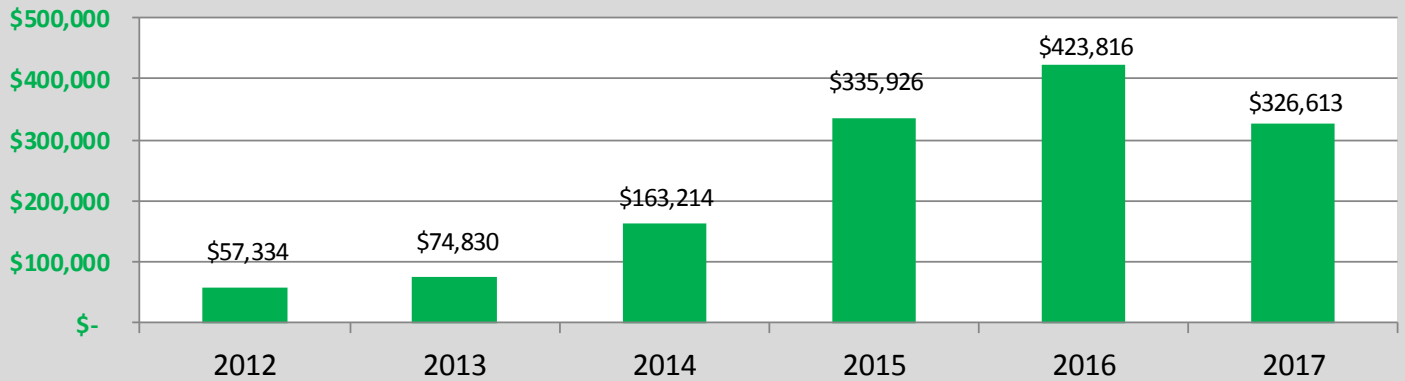
Agency/ Issuer	Investment Type	Purchase Price	Years to Maturity	Par Value	Market Value	Maturity Date	Coupon Rate
FHLMC	Bonds	1,000,000	0.50	1,000,000	999,002	12/28/17	0.90%
FFCB	Bonds	995,820	1.05	1,000,000	994,327	07/18/18	0.75%
FFCB	Bonds	2,010,790	1.50	2,000,000	1,998,222	12/28/18	1.42%
FFCB	Bonds	2,000,000	1.57	2,000,000	1,994,080	01/25/19	1.23%
FHLMC	Bonds	995,970	2.26	1,000,000	994,881	10/02/19	1.25%
FNMA	Bonds	1,994,310	2.33	2,000,000	1,989,264	10/28/19	1.35%
FNMA	Bonds	997,300	2.75	1,000,000	992,378	03/30/20	1.38%
FHLB	Bonds	2,003,780	2.75	2,000,000	1,981,862	03/30/20	1.45%
FNMA	Bonds	2,000,000	2.75	2,000,000	1,994,114	03/30/20	1.65%
FHLMC	Bonds	2,003,868	2.83	2,000,000	1,976,688	04/28/20	1.35%
FNMA	Bonds	1,000,000	3.00	1,000,000	980,869	06/30/20	1.38%
FNMA	Bonds	1,000,000	3.00	1,000,000	980,869	06/30/20	1.38%
FHLB	Bonds	3,000,000	3.04	3,000,000	2,952,897	07/13/20	1.20%
RFCS	Bonds	1,999,698	3.04	2,120,000	2,010,856	07/15/20	1.60%
FHLB	Bonds	2,000,000	3.08	2,000,000	2,000,534	07/30/20	1.75%
FNMA	Bonds	1,000,000	3.16	1,000,000	997,198	08/28/20	1.40%
FNMA	Bonds	1,000,000	3.16	1,000,000	997,198	08/28/20	1.40%
FHLMC	Bonds	999,500	3.50	1,000,000	996,816	12/30/20	1.75%
FNMA	Bonds	2,005,474	3.56	2,000,000	1,943,766	01/19/21	1.50%
FHLMC	Bonds	999,400	4.50	1,000,000	996,110	12/30/21	2.00%
TOTAL SECURITIES		31,005,910	2.7	31,120,000	30,771,931		
Washington State Local Gov't Investment Pool				10,986,941	10,986,941	Demand	0.98%
Snohomish County Local Gov't Investment Pool				16,735,633	16,735,633	Demand	1.11%
TOTAL PORTFOLIO				<u>\$ 58,842,574</u>	<u>\$ 58,494,505</u>		

Attachment: Complete June Quarterly Financial Report (June Quarterly Financial Report)

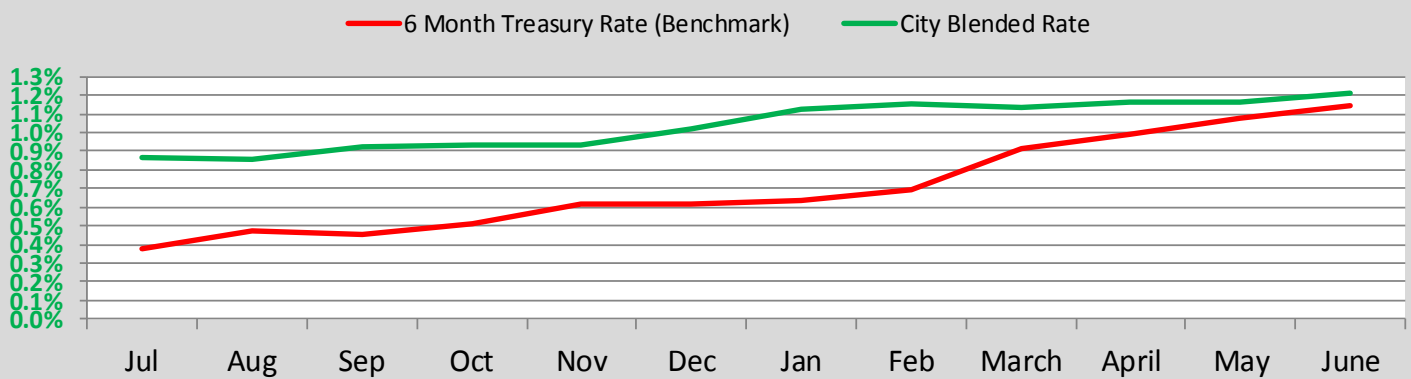


INVESTMENT PORTFOLIO SUMMARY

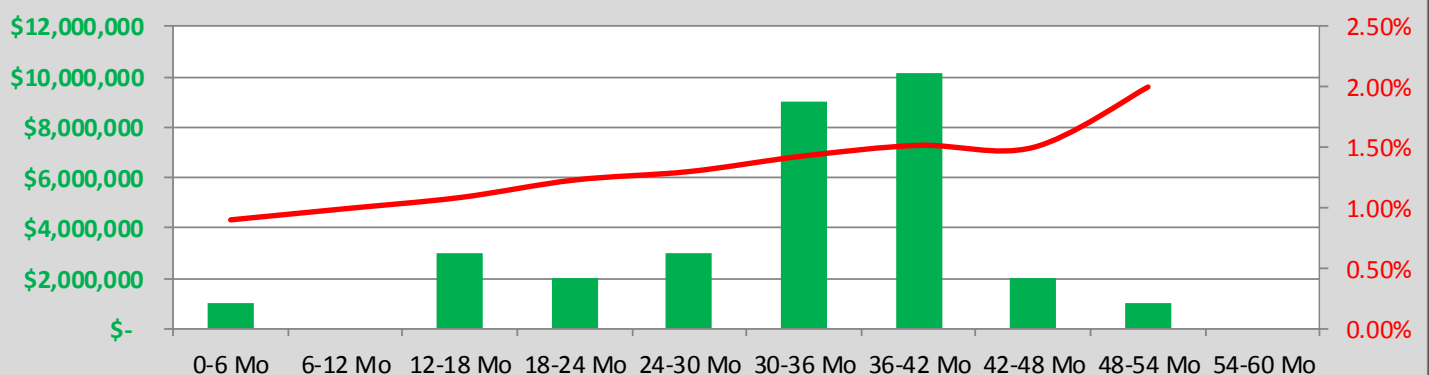
Annual Interest Income



Edmonds Rate of Return Compared to Benchmark (Rolling 12 months)



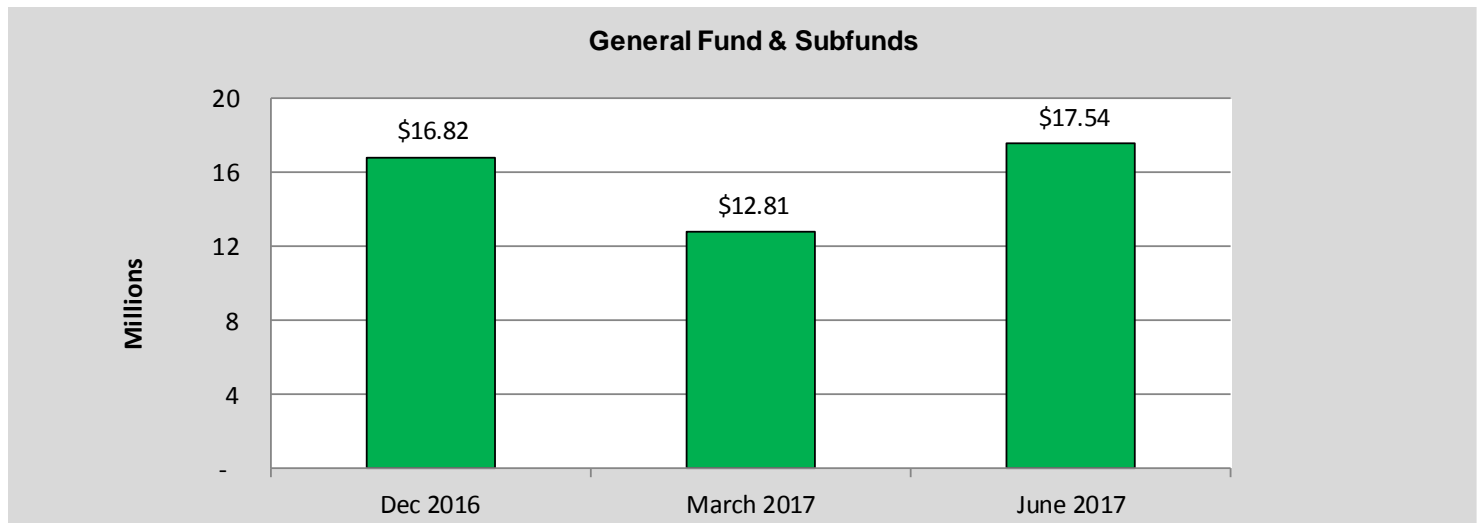
Maturity Distribution and Rate of Return



Attachment: Complete June Quarterly Financial Report (June Quarterly Financial Report)

GENERAL FUND OVERVIEW

GENERAL FUND & SUBFUNDS	FUND BALANCES			CHANGE IN FUND BALANCES	
	---- ACTUAL ----			---- ACTUAL ----	
	<u>12/31/2016</u>	<u>3/31/2017</u>	<u>6/30/2017</u>	<u>Q2</u>	<u>YTD</u>
001-General Fund	\$ 9,841,719	\$ 5,828,744	\$ 10,447,721	\$ 4,618,977	\$ 606,002
009-Leoff-Medical Ins. Reserve	540,255	454,233	539,113	84,881	(1,141)
011-Risk Management Fund	963,025	886,927	891,891	4,964	(71,134)
012-Contingency Reserve Fund	5,367,841	5,379,266	5,408,995	29,729	41,154
014-Historic Preservation Gift Fund	7,646	7,683	7,726	43	79
016-Building Maintenance	98,436	252,144	240,505	(11,639)	142,069
Total General Fund & Subfunds	\$ 16,818,922	\$ 12,808,996	\$ 17,535,951	\$ 4,726,955	\$ 717,029



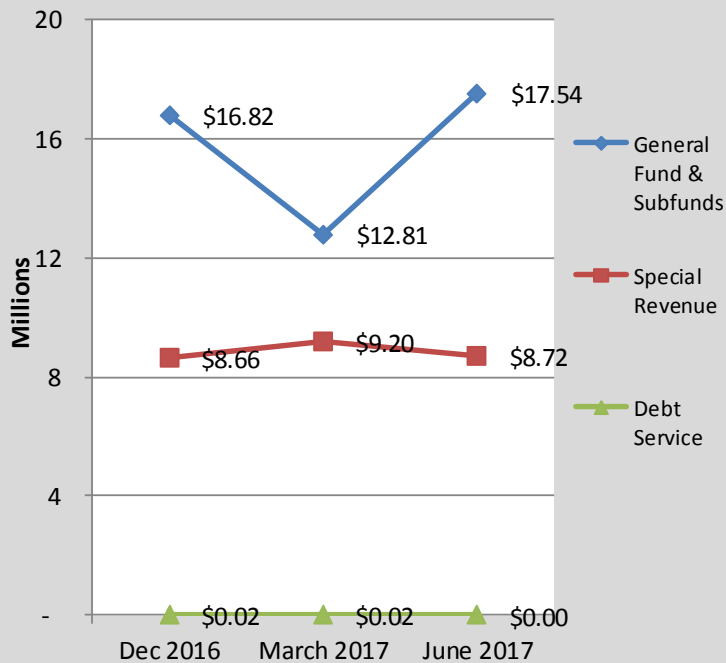
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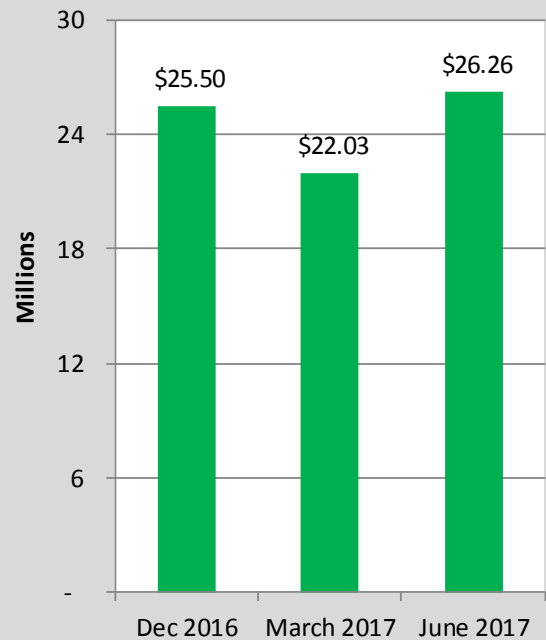
GOVERNMENTAL FUNDS OVERVIEW

GOVERNMENTAL FUNDS	FUND BALANCES			CHANGE IN FUND BALANCES	
	---- ACTUAL ----			---- ACTUAL ----	
	<u>12/31/2016</u>	<u>3/31/2017</u>	<u>6/30/2017</u>	<u>Q2</u>	<u>YTD</u>
General Fund & Subfunds	\$ 16,818,922	\$ 12,808,997	\$ 17,535,951	\$ 4,726,955	\$ 717,02
Special Revenue	8,664,438	9,195,889	8,720,198	(475,692)	55,75
Debt Service	20,262	20,262	3,812	(16,450)	(16,45
Total Governmental Funds	\$ 25,503,622	\$ 22,025,147	\$ 26,259,960	\$ 4,234,813	\$ 756,33

Governmental Fund Balances-By Fund Group



Governmental Fund Balances - Combined

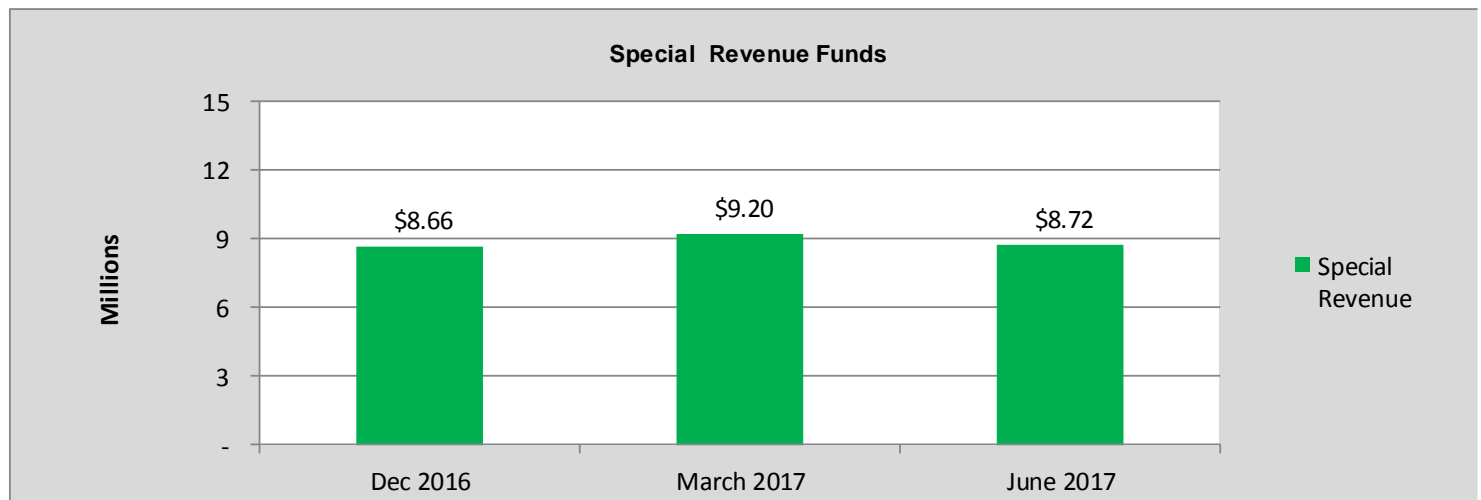


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SPECIAL REVENUE FUNDS OVERVIEW

GOVERNMENTAL SPECIAL REVENUE	FUND BALANCES			CHANGE IN FUND BALANCES	
	---- ACTUAL ----			---- ACTUAL ----	
	12/31/2016	3/31/2017	6/30/2017	Q2	YTD
104 - Drug Enforcement Fund	\$ 34,632	\$ 45,657	\$ 45,175	\$ (481)	\$ 10,544
111 - Street Fund	790,429	752,425	1,019,696	267,271	229,267
112 - Combined Street Const/Improve	91,960	316,710	(847,858)	(1,164,569)	(939,818)
117 - Municipal Arts Acquis. Fund	493,080	500,685	504,784	4,099	11,704
118 - Memorial Street Tree	18,101	18,188	18,288	101	188
120 - Hotel/Motel Tax Revenue Fund	94,468	104,017	91,599	(12,418)	(2,868)
121 - Employee Parking Permit Fund	63,704	73,600	75,223	1,623	11,519
122 - Youth Scholarship Fund	14,931	14,788	14,873	85	(57)
123 - Tourism Promotional Fund/Arts	84,371	86,772	87,326	554	2,955
125 - Real Estate Tax 2	2,259,949	2,192,767	2,475,580	282,813	215,632
126 - Real Estate Excise Tax 1	1,977,485	2,247,796	2,652,104	404,308	674,619
127 - Gifts Catalog Fund	263,544	287,029	263,003	(24,026)	(541)
129 - Special Projects Fund	38,782	38,968	39,184	216	402
130 - Cemetery Maintenance/Improvement	142,339	162,058	189,250	27,192	46,911
132 - Parks Construction Fund	1,185,145	1,196,669	935,231	(261,438)	(249,914)
136 - Parks Trust Fund	153,793	154,530	155,387	857	1,594
137 - Cemetery Maintenance Trust Fund	901,172	912,496	922,598	10,102	21,426
138 - Sister City Commission	6,416	8,176	8,457	281	2,042
140 - Business Improvement District	50,137	82,556	70,294	(12,262)	20,157
Total Special Revenue	\$ 8,664,438	\$ 9,195,889	\$ 8,720,198	\$ (475,692)	\$ 55,759



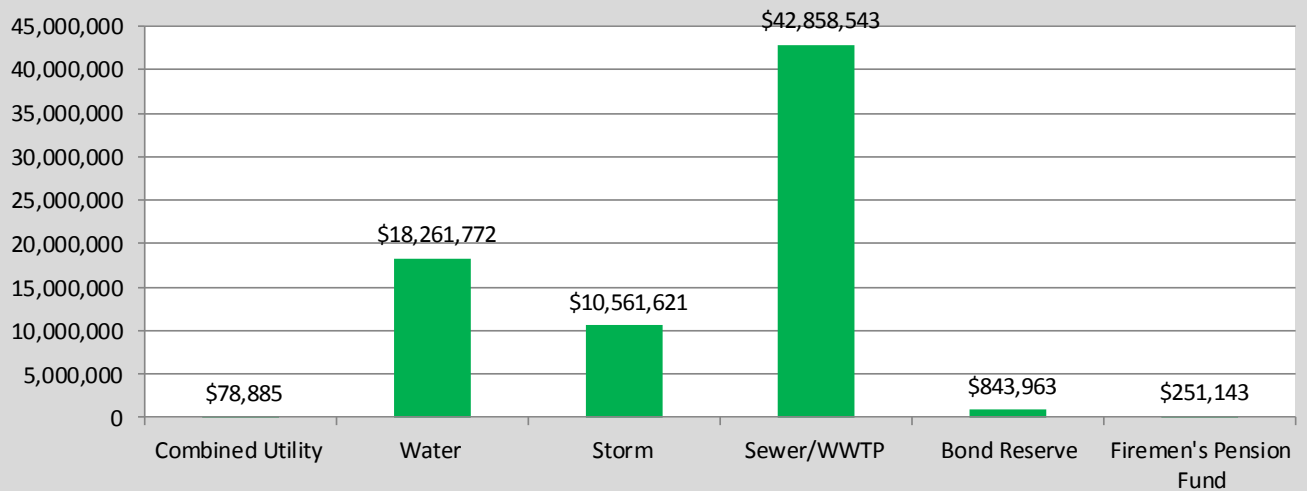
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ENTERPRISE FUNDS OVERVIEW

ENTERPRISE FUNDS	FUND BALANCES			CHANGE IN FUND	
	---- ACTUAL ----			---- ACTUAL ----	
	12/31/2016	3/31/2017	6/30/2017	Q2	YTD
421 - Water Utility Fund	\$ 17,869,638	\$ 18,428,595	\$ 18,261,772	\$ (166,824)	\$ 392,134
422 - Storm Utility Fund	10,246,573	10,707,276	10,561,621	(145,655)	315,048
423 - Sewer/WWTP Utility Fund	42,695,396	43,337,637	42,858,543	(479,094)	163,147
424 - Bond Reserve Fund	843,960	843,961	843,963	2	3
411 - Combined Utility Operation	-	42,151	78,885	36,734	78,885
Total Enterprise Funds	\$ 71,655,566	\$ 73,359,621	\$ 72,604,784	\$ (754,837)	\$ 949,217

Enterprise and Agency Fund Balances as of June 30, 2017



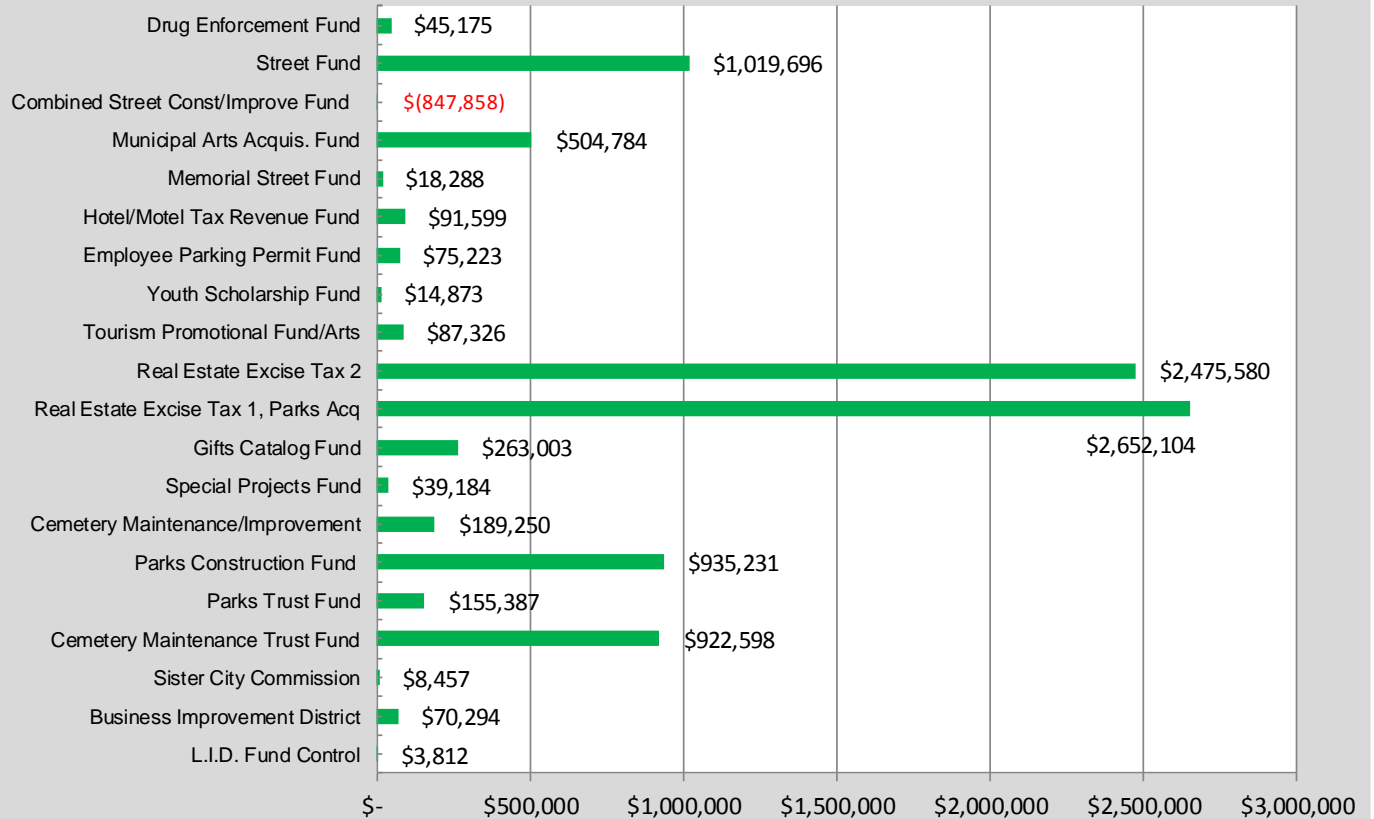
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SUMMARY OVERVIEW

CITY-WIDE	FUND BALANCES			CHANGE IN FUND BALANCES	
	---- ACTUAL ----			---- ACTUAL ----	
	<u>12/31/2016</u>	<u>3/31/2017</u>	<u>6/30/2017</u>	<u>Q2</u>	<u>YTD</u>
Governmental Funds	\$ 25,503,622	\$ 22,025,147	\$ 26,259,960	\$ 4,234,813	\$ 756,338
Enterprise Funds	71,655,566	73,359,621	72,604,784	(754,837)	949,217
Internal Services Fund	8,311,631	8,350,207	8,306,440	(43,767)	(5,191)
Agency Funds	224,697	205,908	251,143	45,235	26,446
Total City-wide Total	\$ 105,695,516	\$ 103,940,884	\$ 107,422,327	\$ 3,481,443	\$ 1,726,811

Governmental Fund Balances (Excluding General Fund) as of June 30, 2017

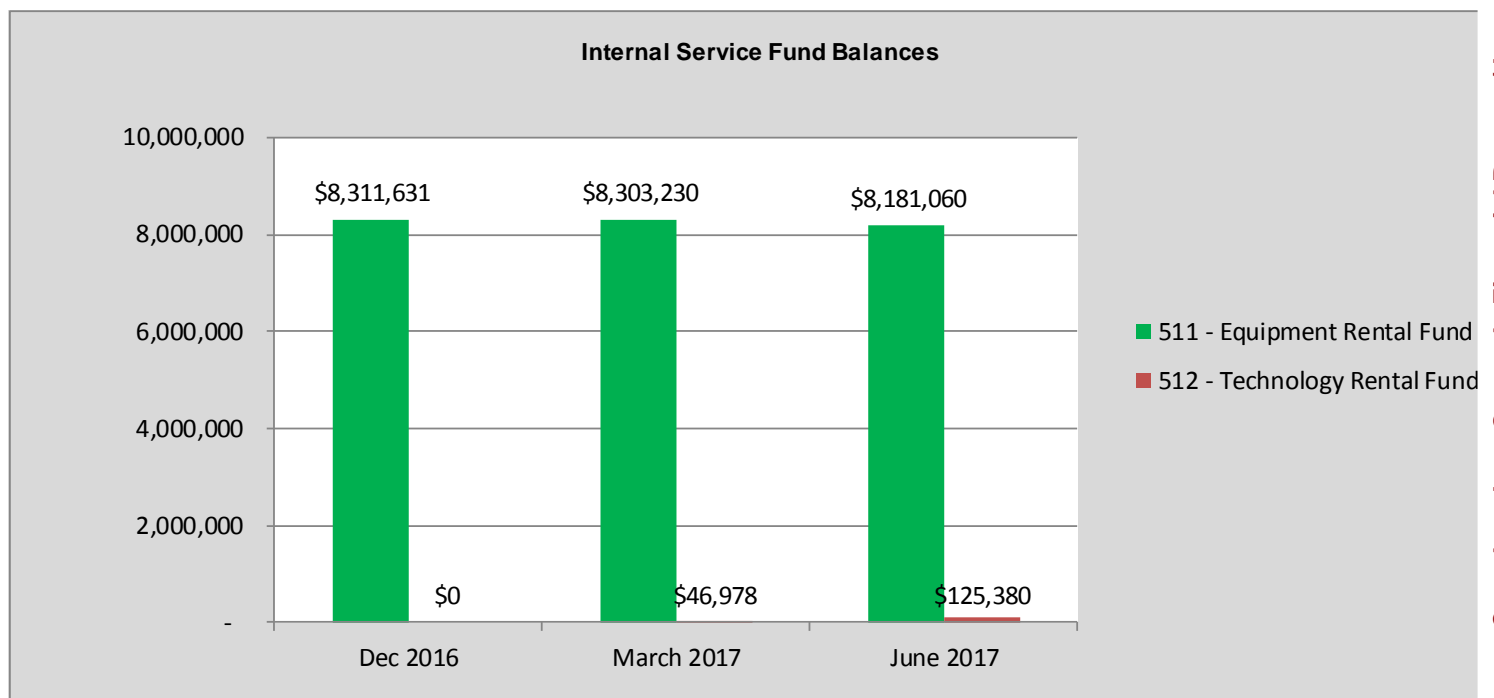


***Please note that these revenues and expenses occur within annual cycles.**

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INTERNAL SERVICE FUNDS OVERVIEW

INTERNAL SERVICE FUNDS	FUND BALANCES			CHANGE IN FUND BALANCES	
	---- ACTUAL ----			---- ACTUAL ----	
	<u>12/31/2016</u>	<u>3/31/2017</u>	<u>6/30/2017</u>	<u>Q2</u>	<u>YTD</u>
511 - Equipment Rental Fund	\$ 8,311,631	\$ 8,303,230	\$ 8,181,060	\$ (122,170)	\$ (130,571)
512 - Technology Rental Fund	-	46,978	125,380	78,402	125,380
Total Internal Service Funds	\$ 8,311,631	\$ 8,350,207	\$ 8,306,440	\$ (43,767)	\$ (5,191)



**Please note that these revenues and expenses occur within annual cycles.*

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City Council Agenda Item

Meeting Date: 08/15/2017

Approval of Council Special Meeting Minutes of August 8, 2017

Staff Lead: Scott Passey

Department: City Clerk's Office

Preparer: Scott Passey

Background/History

N/A

Staff Recommendation

Review and approve the draft meeting minutes on the Consent Agenda.

Narrative

N/A

Attachments:

08-08-2017 Draft Council Special Meeting Minutes

+

EDMONDS CITY COUNCIL SPECIAL MEETING DRAFT MINUTES

AUGUST 8, 2017

Elected Officials Present

Diane Buckshnis, Councilmember
Dave Teitzel, Councilmember
Neil Tibbott, Councilmember
Adrienne Fraley-Monillas, Councilmember
Tom Mesaros, Council President
Kristiana Johnson, Councilmember
Mayor Dave Earling

Staff Present

Mary Ann Hardie, HR Director
Al Compaan, Police Chief
Jeff Taraday, City Attorney

Elected Officials Absent

Mike Nelson, Councilmember

1. CALL TO ORDER/CONVENE IN JURY MEETING ROOM

At 6:40 p.m., the City Council Special Meeting was called to order by Mayor Earling in the Council Chambers, 250 5th Avenue North, Edmonds.

2. EXECUTIVE SESSION TO DISCUSS COLLECTIVE BARGAINING PER RCW 42.30.140(1)(A)

The City Council then adjourned to the Jury Meeting Room in executive session to discuss collective bargaining. The executive session concluded at 7:00 p.m.

ADJOURN

At 7:00 p.m., the meeting was adjourned.

City Council Agenda Item

Meeting Date: 08/15/2017

Approval of Council Meeting Minutes of August 8, 2017

Staff Lead: Scott Passey

Department: City Clerk's Office

Preparer: Scott Passey

Background/History

N/A

Staff Recommendation

Review and approve the draft meeting minutes on the Consent Agenda.

Narrative

N/A

Attachments:

08-08-2017 Draft Council Meeting Minutes

EDMONDS CITY COUNCIL DRAFT MINUTES

August 8, 2017

ELECTED OFFICIALS PRESENT

Dave Earling, Mayor
 Thomas Mesaros, Council President
 Kristiana Johnson, Councilmember
 Michael Nelson, Councilmember
 Adrienne Fraley-Monillas, Councilmember
 Diane Buckshnis, Councilmember
 Dave Teitzel, Councilmember
 Neil Tibbott, Councilmember

STAFF PRESENT

Phil Williams, Public Works Director
 Patrick Doherty, Econ. Dev & Comm. Serv. Dir.
 Scott James, Finance Director
 Dave Turley, Assistant Finance Director
 Jeff Taraday, City Attorney
 Scott Passey, City Clerk
 Andrew Pierce, Legislative/Council Assistant
 Jerrie Bevington, Camera Operator
 Jeannie Dines, Recorder

1. CALL TO ORDER/FLAG SALUTE

The Edmonds City Council meeting was called to order at 7:01 p.m. by Mayor Earling in the Council Chambers, 250 5th Avenue North, Edmonds. The meeting was opened with the flag salute.

2. ROLL CALL

City Clerk Scott Passey called the roll. All elected officials were present.

3. APPROVAL OF AGENDA

COUNCILMEMBER TEITZEL MOVED, SECONDED BY COUNCIL PRESIDENT MESAROS, TO APPROVE THE AGENDA IN CONTENT AND ORDER. MOTION CARRIED UNANIMOUSLY.

4. PRESENTATIONS - None

5. APPROVAL OF CONSENT AGENDA ITEMS

COUNCIL PRESIDENT MESAROS MOVED, SECONDED BY COUNCILMEMBER BUCKSHNIS, TO APPROVE THE CONSENT AGENDA. MOTION CARRIED UNANIMOUSLY.
 The agenda items approved are as follows:

1. **APPROVAL OF COUNCIL MEETING MINUTES OF JULY 31, 2017**
2. **APPROVAL OF CLAIM, PAYROLL AND BENEFIT CHECKS, DIRECT DEPOSIT AND WIRE PAYMENTS**

6. ADJOURN TO COMMITTEE MEETINGS

With no further business, the Council meeting was adjourned to committee meetings at 7:03 p.m. (Parks, Planning & Public Works Committee in the Jury Meeting Room, Finance Committee in Council Chambers and Public Safety & Personnel Committee in the Police Training Room.)

City Council Agenda Item

Meeting Date: 08/15/2017

Approval of claim checks.

Staff Lead: Scott James

Department: Administrative Services

Preparer: Nori Jacobson

Background/History

Approval of claim checks #226832 through #226953 dated August 10, 2017 for \$356,823.80.

Staff Recommendation

Approval of claim checks.

Fiscal Impact

Claims \$356,823.80

Narrative

In accordance with the State statutes, City payments must be approved by the City Council. Ordinance #2896 delegates this approval to the Council President who reviews and recommends either approval or non-approval of expenditures.

Attachments:

claim cks 08-10-17

FrequentlyUsedProjNumbers 08-10-17

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 1

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226832	8/4/2017	071816 CARLSON, JESSICA	5823 ART CAMP		5823 ART CAMP INSTRUCTION 5823 ART CAMP INSTRUCTION 001.000.64.571.22.41.00	875.00
Total :						875.00
226833	8/10/2017	076040 911 SUPPLY INC	52157		INV#52157 - ROUSSEAU - EDMOND 5.11 BIKE PATROL S/S SHIRT 001.000.41.521.70.24.00 BLAUER 8567 PANTS 001.000.41.521.70.24.00 BLAUER L/S SHIRT - CLASS A 001.000.41.521.70.24.00 BLAUER 4670 SOFT SHELL JACKET 001.000.41.521.70.24.00 EMBROIDE NAME ON JACKET 001.000.41.521.70.24.00 EMBROIDER NAME ON S/S SHIRT 001.000.41.521.70.24.00 HEAT STAMP PARKING ENFORCEM 001.000.41.521.70.24.00 10.0% Sales Tax 001.000.41.521.70.24.00 52460 CR FOR INCORRECT PRICE ON #5: SHIRT CHGD \$74.99/S/B \$69.99 001.000.41.521.70.24.00 10.0% Sales Tax 001.000.41.521.70.24.00 52487 INV#52487 - PAULSON - EDMONDS BLAUER 8567 PANTS 001.000.41.521.22.24.00 10.0% Sales Tax 001.000.41.521.22.24.00 52488 INV#52488 - ROUSSEAU - EDMOND 5.11 BIKE PATROL SHIRTS	49.99 269.97 74.99 129.99 8.00 8.00 20.00 56.00 -5.00 -0.50 89.99 9.00

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 1

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 2

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226833	8/10/2017	076040 911 SUPPLY INC	(Continued)		001.000.41.521.70.24.00 EMBROIDER NAME ON SHIRTS 001.000.41.521.70.24.00 10.0% Sales Tax 001.000.41.521.70.24.00 Total :	99.98 16.00 11.60 838.10
226834	8/10/2017	065052 AARD PEST CONTROL	386150		PM & SENIOR CENTER PEST CONT PM & SENIOR CENTER PEST CONT 001.000.64.576.80.41.00 Total :	 137.88 137.88
226835	8/10/2017	061029 ABSOLUTE GRAPHIX	817516		SUMMER ADULT SOFTBALL SHIRTS SUMMER ADULT SOFTBALL SHIRTS 001.000.64.571.25.31.00 10.4% Sales Tax 001.000.64.571.25.31.00 Total :	 535.50 55.68 591.18
226836	8/10/2017	072189 ACCESS INFORMATION MANAGEMENT	2148160		STORAGE OF DOCUMENTS 08/01/2 STORAGE OF DOCUMENTS 08/01/2 001.000.25.514.30.41.00 Total :	 113.51 113.51
226837	8/10/2017	063863 ADVANCED TRAFFIC PRODUCTS	18402		E7AB.TRAFFIC AUDIO CABINET/CC E7AB.TRAFFIC AUDIO CABINET/CC 112.000.68.595.33.65.00 10.3% Sales Tax 112.000.68.595.33.65.00 Total :	 16,192.28 1,667.81 17,860.09
226838	8/10/2017	065568 ALLWATER INC	080117065		WWTP: 8/2/17 DRINKING WATER SI Water services (plus rental/supplies 423.000.76.535.80.31.00 Total :	 48.50 48.50

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 2

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 3

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226839	8/10/2017	069751 ARAMARK UNIFORM SERVICES	1990286466		WWTP: 8/2/17 UNIFORMS, TOWELS	
					wwtp uniforms	
					423.000.76.535.80.24.00	5.10
					wwtp mats & towels	
					423.000.76.535.80.41.00	115.98
					10.3% Sales Tax	
					423.000.76.535.80.24.00	0.52
					10.3% Sales Tax	
					423.000.76.535.80.41.00	11.92
			1990286467		PARKS MAINT UNIFORM SERVICE	
					PARKS MAINT UNIFORM SERVICE	
					001.000.64.576.80.24.00	57.32
					Total :	190.84
226840	8/10/2017	070305 AUTOMATIC FUNDS TRANSFER	95595		OUT SOURCING OF UTILITY BILLS	
					UB Outsourcing area #300 Printing	
					422.000.72.531.90.49.00	149.85
					UB Outsourcing area #300 Printing	
					421.000.74.534.80.49.00	149.85
					UB Outsourcing area #300 Printing	
					423.000.75.535.80.49.00	154.39
					UB Outsourcing area #300 Postage	
					421.000.74.534.80.42.00	533.95
					UB Outsourcing area #300 Postage	
					423.000.75.535.80.42.00	533.92
					10.1 % Sales Tax	
					422.000.72.531.90.49.00	15.13
					10.1 % Sales Tax	
					421.000.74.534.80.49.00	15.13
					10.1 % Sales Tax	
					423.000.75.535.80.49.00	15.60
					Total :	1,567.84
226841	8/10/2017	001801 AUTOMATIC WILBERT VAULT CO	50282		ROUGH BOX - WHARTON	
					ROUGH BOX - WHARTON	
					130.000.64.536.20.34.00	474.00

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 3

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 4

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226841	8/10/2017	001801	001801 AUTOMATIC WILBERT VAULT CO	(Continued)		
					Total :	474.00
226842	8/10/2017	069076	BACKGROUND INVESTIGATIONS INC	COE0717	JULY 2017 BACKGROUND CHECKS PRE-EMPLOYMENT BACKGROUND 001.000.22.518.10.41.00 PARKS OVERAGE 001.000.64.571.25.41.00	420.00 211.00
					Total :	631.00
226843	8/10/2017	061659	BAILEY'S TRADITIONAL TAEKWON	5735 TAEKWON-DO	5735 TAEKWON-DO INSTRUCTION- 5735 TAEKWON-DO INSTRUCTION 001.000.64.571.27.41.00	1,020.24
				5739 TAEKWON-DO	5739 TAEKWON-DO INSTRUCTION 5739 TAEKWON-DO INSTRUCTION 001.000.64.571.22.41.00	38.00
				5743 TAEKWON-DO	5743 TAEKWON-DO INSTRUCTION- 5743 TAEKWON-DO INSTRUCTION- 001.000.64.571.22.41.00	156.00
					Total :	1,214.24
226844	8/10/2017	002170	BARTON, RONALD	57	LEOFF 1 REIMBURSEMENT LEOFF 1 REIMBURSEMENT 009.000.39.517.20.23.00	199.24
					Total :	199.24
226845	8/10/2017	066891	BEACON PUBLISHING INC	27084	CEMETERY AD CEMETERY AD 130.000.64.536.20.41.40	176.00
					Total :	176.00
226846	8/10/2017	002300	BEAVER EQUIPMENT SPECIALITY CO	170651	WWTP: ROTORK ACTUATOR ROTORK ACTUATOR 423.000.76.594.35.64.00 10.3% Sales Tax 423.000.76.594.35.64.00	10,142.00 1,044.60
					Total :	11,186.60

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 4

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 5

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226847	8/10/2017	069226 BHC CONSULTANTS LLC	8825		E6GA.SERVICES THRU 7/21/17 E6GA.SERVICES THRU 7/21/17 423.200.75.594.35.41.00	493.36
			9021		E5JB.SERVICES THRU 7/21/17 E5JB.SERVICES THRU 7/21/17 421.000.74.594.34.41.10	7,789.00
					Total :	8,282.36
226848	8/10/2017	075342 BORUCHOWITZ, ROBERT	073117		MAY - JUL 2017 PUBLIC DEFENSE (
					MAY 2017 COSTS 001.000.22.518.10.41.00	362.50
					JUNE 2017 COSTS 001.000.22.518.10.41.00	891.67
					JULY 2017 COSTS 001.000.22.518.10.41.00	1,641.67
					Total :	2,895.84
226849	8/10/2017	072005 BROCKMANN, KERRY	5749 YOGA		5749 YOGA INSTRUCTION 5749 YOGA INSTRUCTION 001.000.64.571.27.41.00	443.50
					Total :	443.50
226850	8/10/2017	072177 BROOKS PRODUCTS & SERVICE	1318		INV#1318 - CLERKS - EDMONDS PD 3/4 SLEEVED SHIRTS (LG) - DIEHL, 001.000.41.521.11.24.00	65.90
					3/4 SLEEVED SHIRTS- (XL) -COLLIN 001.000.41.521.11.24.00	65.90
					3/4 SLEEVED SHIRTS -(3XL) -BROM 001.000.41.521.11.24.00	139.90
					S/S POLO (LG) - DIEHL, SCHEELE, 001.000.41.521.11.24.00	89.80
					S/S POLO - (3XL) - MANDEVILLE 001.000.41.521.11.24.00	20.90
					ONE TIME SET UP CHARGE 001.000.41.521.11.24.00	20.00
					EMBROIDER NAMES ON 18 SHIRTS	

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 5

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 6

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226850	8/10/2017	072177 BROOKS PRODUCTS & SERVICE	(Continued)		001.000.41.521.11.24.00 EMBROIDER BADGE ON 18 SHIRTS	144.00
					001.000.41.521.11.24.00 10.3% Sales Tax	216.00
					001.000.41.521.11.24.00	78.50
					Total :	841.10
226851	8/10/2017	074776 BUCKSHNIS, DIANE	DB072017		REIMBURSEMENT - TRAVEL WRIA8 Travel ~	
					001.000.11.511.60.43.00 Tolls	29.50
					001.000.11.511.60.43.00	8.20
					Total :	37.80
226852	8/10/2017	075023 CAROLYN DOUGLAS COMMUNICATIONS 68			COMMUNICATIONS/COMMUNITY O Communications/community outreach	
					001.000.61.557.20.41.00	2,500.00
					Total :	2,500.00
226853	8/10/2017	075092 CASCADE BICYCLE CLUB ED FNDN	40234		E3DD.SERVICES THRU JULY 2017 E3DD.SERVICES THRU JULY 2017	
					112.000.68.595.33.41.00	204.70
					Total :	204.70
226854	8/10/2017	069813 CDW GOVERNMENT INC	JPF5075		NIMBLE STORAGE MAINTENANCE Nimble Storage Maintenance Support	
					512.000.31.518.88.48.00 10.3% Sales Tax	9,300.00
					512.000.31.518.88.48.00	957.90
					Total :	10,257.90
226855	8/10/2017	068484 CEMEX LLC	9435987523		STORM DUMP FEES Storm Dump Fees	
					422.000.72.531.10.49.00	1,989.70
					Total :	1,989.70

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 6

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 7

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226856	8/10/2017	003510 CENTRAL WELDING SUPPLY	LY 249830		SPRAY PARK CARBON DIOXIDE SPRAY PARK CARBON DIOXIDE 001.000.64.576.80.31.00 10.3% Sales Tax	93.75
			LY 249933		001.000.64.576.80.31.00 SPRAY PARK CARBON DIOXIDE SPRAY PARK CARBON DIOXIDE 001.000.64.576.80.31.00 10.3% Sales Tax	9.65
			RN07170998		001.000.64.576.80.31.00 SPRAY PARK CARBON DIOXIDE, G' GYMNASTICS HELIUM 001.000.64.571.28.45.00 10.3% Sales Tax	10.75
			RN07170999		001.000.64.571.28.45.00 WWTP: CYLINDER RENTAL + HAZI nitrogen, oxygen, carbon monoxide 423.000.76.535.80.31.00 10.3% Sales Tax 423.000.76.535.80.31.00	1.15 64.50 6.64
					Total :	289.85
226857	8/10/2017	076176 CHRISTIAN, JOHN	8/2-8/4 FIELD ATTEND		8/2-8/4/17 SOFTBALL FIELD ATTENI 8/2-8/4/17 SOFTBALL FIELD ATTENI 001.000.64.571.25.41.00	132.00
					Total :	132.00
226858	8/10/2017	019215 CITY OF LYNNWOOD	13320		E6AB.TRAFFIC SIGNAL MAINTENAN E6AB.TRAFFIC SIGNAL MAINTENAN 112.000.68.595.33.65.00	1,798.85
					Total :	1,798.85
226859	8/10/2017	065519 CITY OF LYNNWOOD	13322		INV#13322 1/2 NARCOTICS SGT- 2N 1/2 NARC SGT SAL & BEN - 2ND QT 104.100.41.521.21.51.00	15,508.48

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 7

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 8

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226859	8/10/2017	065519 CITY OF LYNNWOOD	(Continued)			
					Total :	15,508.48
226860	8/10/2017	003801 CITY WIDE FENCE CO INC	33482		PM: FENCE PARTS PM: FENCE PARTS 001.000.64.576.80.31.00 10.4% Sales Tax 001.000.64.576.80.31.00	261.82 27.23
					Total :	289.05
226861	8/10/2017	073135 COGENT COMMUNICATIONS INC	AUG-17		C/A CITYOFED00001 Aug-17 Fiber Optics Internet Connect 512.000.31.518.87.42.00	406.10 406.10
					Total :	406.10
226862	8/10/2017	075925 CROSSROADS STRATEGIES LLC	1012240		FEDERAL LOBBYIST JULY 2017 Federal lobbyist July 2017 001.000.61.511.70.41.00	5,000.00 5,000.00
					Total :	5,000.00
226863	8/10/2017	006200 DAILY JOURNAL OF COMMERCE	3327931		BUSINESS RECRUITMENT ADS FOI Business recruitment ads for July 201 001.000.61.558.70.41.40	540.00 540.00
					Total :	540.00
226864	8/10/2017	064531 DINES, JEANNIE	17-3783		7/25/17 & 7/31/17 CITY COUNCIL & 7 7/25/17 CITY COUNCIL MEETING MI 001.000.25.514.30.41.00	418.20 418.20
					Total :	418.20
226865	8/10/2017	007675 EDMONDS AUTO PARTS	1-76381		PM: 10W30, TRI-FLOW PM: 10W30, TRI-FLOW 001.000.64.576.80.31.00 10.3% Sales Tax 001.000.64.576.80.31.00	61.95 6.38
					Total :	68.33
226866	8/10/2017	074302 EDMONDS HARDWARE & PAINT LLC	002330		PM: AERO COATING, PAINT	

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 8

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 9

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226866	8/10/2017	074302 EDMONDS HARDWARE & PAINT LLC	(Continued)		PM: AERO COATING, PAINT	
					001.000.64.576.80.31.00	12.98
					10.3% Sales Tax	
					001.000.64.576.80.31.00	1.34
			002332		PM: MULTIMETER	
					PM: MULTIMETER	
					001.000.64.576.80.31.00	23.95
					10.3% Sales Tax	
					001.000.64.576.80.31.00	2.47
					Total :	40.78
226867	8/10/2017	069523 EDMONDS P&R YOUTH SCHOLARSHIP	6078 RAPIPOUR		6078 SURI RAPIPOUR YOUTH SCH	
					6078 SURI RAPIPOUR YOUTH SCH	
					122.000.64.571.20.49.00	75.00
			6079 JACOBS		6079 GIDGET JACOBS YOUTH SCH	
					6079 GIDGET JACOBS YOUTH SCH	
					122.000.64.571.20.49.00	75.00
					Total :	150.00
226868	8/10/2017	008705 EDMONDS WATER DIVISION	8-40000		HICKMAN PARK STORM DRAIN & U	
					HICKMAN PARK STORM DRAIN & U	
					001.000.64.576.80.47.00	1,088.65
					Total :	1,088.65
226869	8/10/2017	065331 EMD MILLIPORE CORPORATION	8125336		WWTP: ZRQSV3US LAB EQUIPME	
					ZRQSV3US LAB EQUIPMENT. The	
					423.000.76.535.80.35.00	2,990.48
					Freight	
					423.000.76.535.80.35.00	45.00
					10.3% Sales Tax	
					423.000.76.535.80.35.00	312.65
					Total :	3,348.13
226870	8/10/2017	074437 EMPLOYERS HEALTH COALITION WA	080417		2017 Q3 EHCW RETIREE MGMT FE	
					3 X FIRE RETIREE MEDICAL PROGI	

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 9

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 10

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226870	8/10/2017	074437 EMPLOYERS HEALTH COALITION WA	(Continued)		617.000.51.517.20.23.10 2017 Q3 EHCW RETIREE MEDICAL 009.000.39.517.20.41.00	270.00 1,800.00
					Total :	2,070.00
226871	8/10/2017	008975 ENTENMANN ROVIN CO	0128545-IN		INV#0128545-IN ACCT#0011847 - EI OFFICER RETIRED BADGES 001.000.41.521.10.31.00 PLAIN BADGE CASES 001.000.41.521.10.31.00 PACKAGE INSURANCE 001.000.41.521.10.31.00 PACKAGING & HANDLING FEES 001.000.41.521.10.31.00 Freight 001.000.41.521.10.31.00	196.00 54.00 6.00 4.50 16.00
					Total :	276.50
226872	8/10/2017	009410 EVERETT STEEL INC	185832		PM: TUBES PM: TUBES 001.000.64.576.80.31.00 9.7% Sales Tax 001.000.64.576.80.31.00	747.40 72.50
					Total :	819.90
226873	8/10/2017	066378 FASTENAL COMPANY	WAMOU45219		WATER SUPPLIES Water Supplies 421.000.74.534.80.31.00 10.3% Sales Tax 421.000.74.534.80.31.00	24.10 2.40
					Total :	26.60
226874	8/10/2017	009815 FERGUSON ENTERPRISES INC	0561039		WATER METER INVENTORY #2027 Water Meter Inventory #2027 421.000.74.534.80.34.30	3,056.00

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 10

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 11

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226874	8/10/2017	009815 FERGUSON ENTERPRISES INC	(Continued)		#0577 W-RADIO-01-010 421.000.74.534.80.34.20 10.3% Sales Tax 421.000.74.534.80.34.30 10.3% Sales Tax 421.000.74.534.80.34.20 Total :	4,209.30 314.77 433.56 8,013.63
226875	8/10/2017	075536 FREGONESE ASSOCIATES	450-019		PROF SERV. HIGHWAY 99 SUBARE Prof Serv. Highway 99 Subarea 001.000.62.524.10.41.00 Total :	3,410.00 3,410.00
226876	8/10/2017	075970 FRIEDMAN, JAMES	8/3 HMP FRIEDMAN		8/3 HMP FRIEDMAN 8/3 HMP FRIEDMAN 117.100.64.573.20.41.00 Total :	450.00 450.00
226877	8/10/2017	011900 FRONTIER	206-188-0247		TELEMETRY MASTER SUMMARY A TELEMETRY MASTER SUMMARY A 421.000.74.534.80.42.00 TELEMETRY MASTER SUMMARY A 423.000.75.535.80.42.00 425-745-5055 MEADOWDALE PRESCHOOL PHON MEADOWDALE PRESCHOOL PHON 001.000.64.571.29.42.00 FAC MAINT IP LINE (10 + TAX) 001.000.66.518.30.42.00 425-774-1031 LIFT STATION #8 VG SPECIAL ACCE LIFT STATION #8 TWO VOICE GRA 423.000.75.535.80.42.00 425-776-1281 SNO-ISLE LIBRARY ELEVATOR PHC SNO-ISLE LIBRARY ELEVATOR PHC 001.000.66.518.30.42.00 425-776-5316 425-776-5316 PARKS MAINT FAX LII	264.14 264.14 126.23 11.03 47.45 54.92

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 11

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 12

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226877	8/10/2017	011900 FRONTIER	(Continued)		425-776-5316 PARKS MAINT FAX LII 001.000.64.576.80.42.00	101.26
					Total :	869.17
226878	8/10/2017	075725 GIRLS ROCK MATH	5653 GIRLS ROCK MATH		5653 GIRLS ROCK MATH INSTRUCT 5653 GIRLS ROCK MATH INSTRUCT 001.000.64.571.22.41.00	2,870.40
					Total :	2,870.40
226879	8/10/2017	070437 HARDIE, MARY ANN	051817		TUITION REIMBURSEMENT TUITION REIMBURSEMENT 001.000.22.518.10.49.00	518.00
					Total :	518.00
226880	8/10/2017	013140 HENDERSON, BRIAN	58		LEOFF 1 REIMBURSEMENT LEOFF 1 REIMBURSEMENT 009.000.39.517.20.23.00	83.39
					Total :	83.39
226881	8/10/2017	072647 HERRERA ENVIRONMENTAL	40627B		PERMITTING SVCS FOR WATERFR PERMITTING SVCS FOR WATERFR 132.000.64.594.76.41.00	15,668.54
			40758		PERMITTING SVCS FOR WATERFR PERMITTING SVCS FOR WATERFR 132.000.64.594.76.41.00	10,945.40
					Total :	26,613.94
226882	8/10/2017	067862 HOME DEPOT CREDIT SERVICES	4054855		WWTP: FANS/SHEETINGS/CLR GAI fans picked up at Shoreline Store 423.000.76.535.80.35.00	37.92
					sheeting, clr gallons picked up at 423.000.76.535.80.31.00	196.91
					10.0% Sales Tax 423.000.76.535.80.35.00	3.79
					10.0% Sales Tax	

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 12

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 13

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226882	8/10/2017	067862 HOME DEPOT CREDIT SERVICES	(Continued)		423.000.76.535.80.31.00	19.69
					Total :	258.31
226883	8/10/2017	075966 HULBERT, CARRIE	BID-005		BID/ED! ADMINISTRATIVE SERVICE BID/Ed! administrative services July 140.000.61.558.70.41.00	1,200.00
					Total :	1,200.00
226884	8/10/2017	060165 HWA GEOSCIENCES INC	27585		E6GA.SERVICES THRU 7/23/17 E6GA.SERVICES THRU 7/23/17 423.200.75.594.35.41.00	5,759.00
					Total :	5,759.00
226885	8/10/2017	072891 HYDRO FLOW PRODUCTS	127344		WATER - HYDRANT METER HOSE Water - Hydrant Meter Hose 421.000.74.534.80.31.00 Freight 421.000.74.534.80.31.00	198.00 18.14
					Total :	216.14
226886	8/10/2017	076159 IMS INFRASTRUCTURE MGMT SVCS	11317-2		E7CA SERVICES THRU JULY 2017 E7CA SERVICES THRU JULY 2017 112.000.68.595.33.41.00	1,545.00
					Total :	1,545.00
226887	8/10/2017	073548 INDOFF INCORPORATED	2983147		COLORED COPY PAPER FOR DSD Colored copy paper for DSD 001.000.62.524.10.31.00	18.42
			2988791		WWTP: TOILET PAPER Toilet paper 423.000.76.535.80.31.00 10.3% Sales Tax	69.58
			2989085		423.000.76.535.80.31.00 DSD COPY PAPPER DSD Copy Papper 001.000.62.524.10.31.00	7.11 138.16

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 13

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 14

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226887	8/10/2017	073548 INDOFF INCORPORATED	(Continued) 2989501		RETURN OF COPY PAPER - WRON Return of copy paper - wrong color 001.000.62.524.10.31.00	-18.42
			2990351		DSD - OFFICE SUPPLIES DSD - Office supplies 001.000.62.524.10.31.00	18.84
					Total :	233.71
226888	8/10/2017	014940 INTERSTATE BATTERY SYSTEMS	300-10027488		UNIT E138SO - PARTS Unit E138SO - Parts 511.100.77.594.48.64.00	41.95
					10.3% Sales Tax 511.100.77.594.48.64.00	4.32
			300-10027511		UNITS E143,144,146RE - PARTS Units E143,144,146RE - Parts 511.100.77.594.48.64.00	64.42
					10.3% Sales Tax 511.100.77.594.48.64.00	6.62
			300-10027702		UNIT E134PO - PARTS Unit E134PO - Parts 511.100.77.594.48.64.00	91.90
					10.3% Sales Tax 511.100.77.594.48.64.00	9.47
			300-10027807		UNIT E143RE - SUPPLIES Unit E143RE - Supplies 511.100.77.594.48.64.00	109.85
					10.3% Sales Tax 511.100.77.594.48.64.00	11.37
					Total :	339.81
226889	8/10/2017	075062 JAMESTOWN NETWORKS	4547		FIBER OPTICS INTERNET CONNEC Aug-17 Fiber Optics Internet Connect 512.000.31.518.87.42.00	500.00
					10.3% Sales Tax 512.000.31.518.87.42.00	51.50

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 14

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 15

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226889	8/10/2017	075062	075062 JAMESTOWN NETWORKS	(Continued)		
					Total :	551.50
226890	8/10/2017	075661	JANET MAPLES DESIGN /IZZY GIRL	BID-07312017	SOCIAL MEDIA MANAGEMENT FOR BID/Ed! social media management fo 140.000.61.558.70.41.00	500.00
					Total :	500.00
226891	8/10/2017	074888	JOYOUS NOISE LLC	5694 KINDERMUSIK	5694 KINDERMUSIK INSTRUCTION 5694 KINDERMUSIK INSTRUCTION 001.000.64.571.22.41.00	190.58
				5698 KINDERMUSIK	5698 KINDERMUSIK INSTRUCTION 5698 KINDERMUSIK INSTRUCTION 001.000.64.571.22.41.00	392.41
					Total :	582.99
226892	8/10/2017	075378	JUMPIN J'S JUMP ROPE CHAMPS	5791 JUMP ROPE CAMP	5791 JUMP ROPE CAMP 5791 JUMP ROPE CAMP 001.000.64.571.22.41.00	2,752.75
					Total :	2,752.75
226893	8/10/2017	075646	K-A GENERAL CONST CONTRACTOR	E6MA.Pmt 5	E6MA.PMT 5 THRU 6/30/17 E6MA.PMT 5 THRU 6/30/17 132.000.64.594.76.65.00 E6MA.RET 5 132.000.223.400	661.80 -30.00
				E6MB.Pmt 7	E6MB.PMT 7 THRU 6/30/17 E6MB.PMT 7 THRU 6/30/17 125.000.64.594.75.65.00 E6MB.RET 7 125.000.223.400	1,020.25 -46.25
					Total :	1,605.84
226894	8/10/2017	072650	KCDA PURCHASING COOPERATIVE	300180428	INV#300180428 ACCT#100828 - EDI 10 CASES MULTI USE COPY PAPER 001.000.41.521.10.31.00 HANDLING FEE 001.000.41.521.10.31.00	229.60 67.00

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 15

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 16

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226894	8/10/2017	072650 KCDA PURCHASING COOPERATIVE	(Continued)		10.3% Sales Tax 001.000.41.521.10.31.00	23.65
					Total :	320.21
226895	8/10/2017	016600 KROESENS INC	45866		INV#45866 ACCT#1320 - EDMONDS CHAMBERS BASKETWEAVE BELT 001.000.41.521.11.24.00	24.00
					10.3% Sales Tax 001.000.41.521.11.24.00	2.47
					Total :	26.47
226896	8/10/2017	066231 LINCOLN AQUATICS	SI322349		YOST WEDGE ASSEMBLY YOST WEDGE ASSEMBLY 001.000.64.576.80.31.00	58.95
					10.3% Sales Tax 001.000.64.576.80.31.00	6.08
					Total :	65.03
226897	8/10/2017	074263 LYNNWOOD WINSUPPLY CO	019216 00		PM: IRRIGATION SUPPLIES PM: IRRIGATION SUPPLIES 001.000.64.576.80.31.00	185.60
					10.3% Sales Tax 001.000.64.576.80.31.00	19.12
					Total :	204.72
226898	8/10/2017	075769 MAILFINANCE INC	N6668925		E-CERTIFIED 08/29/2017 - 11/28/2017 N16073501 E-CERTIFIED 08/29/2017 001.000.25.514.30.45.00	192.00
					10.3% Sales Tax 001.000.25.514.30.45.00	19.76
					Total :	211.76
226899	8/10/2017	019920 MCCANN, MARIAN	55		LEOFF 1 REIMBURSEMENT LEOFF 1 REIMBURSEMENT 009.000.39.517.20.29.00	8,281.50

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 16

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 17

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226899	8/10/2017	019920	019920 MCCANN, MARIAN		(Continued)	Total : 8,281.50
226900	8/10/2017	020039	MCMASTER-CARR SUPPLY CO	41047984	WWTP: WK GLOVES,PUMP,EYES,S WK GLOVES,PUMP,EYES,SAFETY 423.000.76.535.80.31.00 Freight 423.000.76.535.80.31.00	338.71 14.56
					Total :	353.32
226901	8/10/2017	075746	MCMURRAY, LAURA	5858 FELDENKRAIS	5858 FELDENKRAIS INSTRUCTION 5858 FELDENKRAIS INSTRUCTION 001.000.64.571.27.41.00	232.50
				5861 FELDENKRAIS	5861 FELDENKRAIS INSTRUCTION 5861 FELDENKRAIS INSTRUCTION 001.000.64.571.27.41.00	241.50
					Total :	474.00
226902	8/10/2017	076179	MELISSA MORGAN	1-06330	#1012057AL UTILITY REFUND #1012057AL Utility refund due to 411.000.233.000	103.15
					Total :	103.15
226903	8/10/2017	063773	MICROFLEX	00022639	06-17 TAX AUDIT PROGRAM TAX AUDIT PROGRAM 001.000.31.514.23.41.00	10.00
					Total :	10.00
226904	8/10/2017	020900	MILLERS EQUIP & RENT ALL INC	263363	PM: DIAMOND WHEEL, DRIVE SHA PM: DIAMOND WHEEL, DRIVE SHA 001.000.64.576.80.31.00 10.3% Sales Tax	358.35
					001.000.64.576.80.31.00	36.91
				263958	SEWER - REPAIRS AT SENIOR CEN Sewer - Repairs at Senior Center - 423.000.75.535.80.45.00 10.3% Sales Tax 423.000.75.535.80.45.00	112.75 11.61

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 17

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 18

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226904	8/10/2017	020900 MILLERS EQUIP & RENT ALL INC	(Continued) 263991		WATER/SEWER - 2 CYCLE OIL Water/Sewer - 2 cycle Oil 421.000.74.534.80.31.00	33.90
					Water/Sewer - 2 cycle Oil 423.000.75.535.80.31.00	33.90
					10.3% Sales Tax 421.000.74.534.80.31.00	3.45
					10.3% Sales Tax 423.000.75.535.80.31.00	3.45
			264444		PM: CHAIN, BLADE CLEANER PM: CHAIN, BLADE CLEANER 001.000.64.576.80.31.00	76.15
					10.3% Sales Tax 001.000.64.576.80.31.00	7.84
			264710		PM: AUTOCUT SPOOL, BLADES PM: AUTOCUT SPOOL, BLADES 001.000.64.576.80.31.00	38.85
					10.3% Sales Tax 001.000.64.576.80.31.00	4.00
					Total :	721.25
226905	8/10/2017	074556 MOORE, IACOFANO & GOLTSMAN INC	49673		E6DB.SERVICES THRU 6/30/2017 E6DB.SERVICES THRU 6/30/2017 112.000.68.595.33.41.00	2,579.25
					Total :	2,579.25
226906	8/10/2017	072746 MURRAYSMITH	15-1715-20		E5KA.SERVICES THRU 6/30/2017 E5KA.SERVICES THRU 6/30/2017 421.000.74.594.34.41.10	13,967.25
					Total :	13,967.25
226907	8/10/2017	074356 NAVAS-RIVAS, HERNAN	21579		SPANISH INTERPRETER COURT 08 SPANISH INTERPRETER COURT 08 001.000.23.512.50.41.01	103.00

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 18

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 19

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226907	8/10/2017	074356	074356 NAVAS-RIVAS, HERNAN	(Continued)	Total :	103.00
226908	8/10/2017	070855	NAVIA BENEFIT SOLUTIONS	10095543	NAVIA AUGUST 2017 FEES PARTICIPANT FEE AND MONTHLY M 001.000.22.518.10.41.00	100.00 Total : 100.00
226909	8/10/2017	063034	NCL	393262	WWTP: B-12D BOD STANDARD B-12D BOD STANDARD 423.000.76.535.80.31.00 Freight 423.000.76.535.80.31.00	180.00 26.19 Total : 206.19
226910	8/10/2017	074724	NEWMAN-BURROW LLC	141041	FALL 2017 REC DIGITAL GUIDE CR/ FALL 2017 REC DIGITAL GUIDE CR/ 001.000.64.571.22.49.00 10.3% Sales Tax 001.000.64.571.22.49.00	514.00 52.94 Total : 566.94
226911	8/10/2017	025217	NORTH SOUND HOSE & FITTINGS	83585	WWTP: FIRE HOSE, SEALANT, TAPE FIRE HOSE, SEALANT, TAPE, COUF 423.000.76.535.80.31.00 9.7% Sales Tax 423.000.76.535.80.31.00	583.86 56.60 Total : 640.46
226912	8/10/2017	025690	NOYES, KARIN	000 00 755 000 00 758	PLANNING BOARD 7/26/17 AND ~ Planning Board 7/26/17 and ~ 001.000.62.558.60.41.00 ADB MINUTES 8/2/17 ADB Minutes 8/2/17 001.000.62.558.60.41.00	455.00 245.00 Total : 700.00
226913	8/10/2017	065720	OFFICE DEPOT	9464755450001	PW OFFICE SUPPLIES	

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 19

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 20

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226913	8/10/2017	065720 OFFICE DEPOT	(Continued)		PW Office Supplies	
					001.000.65.518.20.31.00	34.17
					10.3% Sales Tax	
					001.000.65.518.20.31.00	3.52
		946876316001			PW OFFICE SUPPLIES	
					PW Office Supplies	
					001.000.65.518.20.31.00	17.57
					10.3% Sales Tax	
					001.000.65.518.20.31.00	1.87
		947809442001			PW OFFICE SUPPLIES	
					PW Office Supplies	
					001.000.65.518.20.31.00	45.08
					10.3% Sales Tax	
					001.000.65.518.20.31.00	4.62
					Total :	106.79
226914	8/10/2017	070166 OFFICE OF THE STATE TREASURER	July, 2017		COURT, BLDG CODE & JIS TRANSM	
					Emergency Medical Services & Traun	
					001.000.237.120	1,315.12
					PSEA 1, 2 & 3 Account	
					001.000.237.130	26,300.43
					Building Code Fee Account	
					001.000.237.150	171.00
					State Patrol Death Investigation	
					001.000.237.330	32.62
					Judicial Information Systems Account	
					001.000.237.180	6,625.70
					School Zone Safety Account	
					001.000.237.200	357.50
					Washington Auto Theft Prevention	
					001.000.237.250	2,603.62
					Traumatic Brain Injury	
					001.000.237.260	516.40
					Hwy Safety Acct	

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 20

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 21

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226914	8/10/2017	070166 OFFICE OF THE STATE TREASURER	(Continued)		001.000.237.320 Crime Lab Blood Breath Analysis 001.000.237.170 WSP Hwy Acct 001.000.237.340	51.76 31.96 185.27
Total :						38,191.40
226915	8/10/2017	026200 OLYMPIC VIEW WATER DISTRICT	0054671		HICKMAN PARK IRRIGATION HICKMAN PARK IRRIGATION 001.000.64.576.80.47.00	3,778.00
			0060860		HICKMAN PARK DRINKING FOUNTA HICKMAN PARK DRINKING FOUNTA 001.000.64.576.80.47.00	107.59
Total :						3,885.62
226916	8/10/2017	063750 ORCA PACIFIC INC	28740		CHEMICALS YOST POOL CHEMICALS YOST POOL 001.000.64.576.80.31.00 10.3% Sales Tax 001.000.64.576.80.31.00	832.58 85.75
Total :						918.33
226917	8/10/2017	027060 PACIFIC TOPSOILS	18-T1018497		PM YARD WASTE DUMP CUST # 51 PM YARD WASTE DUMP 001.000.64.576.80.47.00	96.00
			18-T1018515		PM YARD WASTE DUMP CUST # 51 PM YARD WASTE DUMP 001.000.64.576.80.47.00	96.00
Total :						192.00
226918	8/10/2017	065051 PARAMETRIX INC	21-21654		WWTP:5/28-7/1/17 PLC&SCADA PH 5/28-7/1/17 PLC&SCADA PH4 TSK18 423.100.76.594.39.41.10	59,620.40
Total :						59,620.40
226919	8/10/2017	065787 PATRIOT DIAMOND INC	A07558		WATER - 18" ASPHALT BLADE	

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 21

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 22

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226919	8/10/2017	065787 PATRIOT DIAMOND INC	(Continued)		Water - 18" Asphalt Blade 421.000.74.534.80.31.00 Freight 421.000.74.534.80.31.00 Total :	234.00 14.00 248.00
226920	8/10/2017	069633 PET PROS	0014966-IN		INV#0014966-IN CUST#07-EDMONI SALES TAX ON FREE BAG OF DOG 001.000.41.521.26.31.00	4.95
			0015133-IN		INV#0015133-IN CUST #07-EDMONI NUTRI CHKN/RICE DOG FOOD 001.000.41.521.26.31.00 NUTRI LAMB/RICE DOG FOOD 001.000.41.521.26.31.00 10.4% Sales Tax 001.000.41.521.26.31.00 Total :	45.55 47.45 9.65 107.75
226921	8/10/2017	072384 PLAY-WELL TEKNOLOGIES	5681 PLAY WELL		5681 PLAY WELL INSTRUCTION 5681 PLAY WELL INSTRUCTION 001.000.64.571.22.41.00	1,974.00
			5683 PLAY WELL		5683 PLAY WELL INSTRUCTION 5683 PLAY WELL INSTRUCTION 001.000.64.571.22.41.00 Total :	1,410.00 3,384.00
226922	8/10/2017	070160 POETS & WRITERS INC	2017-18942		WOTS ADVERTISING WOTS ADVERTISING 123.000.64.573.20.41.40 Total :	510.00 510.00
226923	8/10/2017	064088 PROTECTION ONE	31146525		ALARM MONITORING CITY HALL ALARM MONITORING CITY HALL 12 001.000.66.518.30.42.00 Total :	51.11 51.11

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 22

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 23

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226924	8/10/2017	070809 PUGET SOUND EXECUTIVE	17-2011		COURT SECURITY 07/17/2017-07/31 COURT SECURITY 07/17/2017-07/31 001.000.23.512.50.41.00	2,392.50
Total :						2,392.50
226925	8/10/2017	061540 REPUBLIC SERVICES #197	3-0197-0800478		FIRE STATION #20 23009 88TH AVE FIRE STATION #20 23009 88TH AVE 001.000.66.518.30.47.00	155.50
			3-0197-0800897		PUBLIC WORKS OMC 7110 210TH S PUBLIC WORKS OMC 7110 210TH S 001.000.65.518.20.47.00	31.10
					PUBLIC WORKS OMC 7110 210TH S 111.000.68.542.90.47.00	118.30
					PUBLIC WORKS OMC 7110 210TH S 421.000.74.534.80.47.00	118.30
					PUBLIC WORKS OMC 7110 210TH S 423.000.75.535.80.47.10	118.30
					PUBLIC WORKS OMC 7110 210TH S 511.000.77.548.68.47.00	118.30
					PUBLIC WORKS OMC 7110 210TH S 422.000.72.531.90.47.00	118.20
			3-0197-0801132		FIRE STATION #16 8429 196TH ST S FIRE STATION #16 8429 196TH ST S 001.000.66.518.30.47.00	165.60
			3-0197-0829729		CLUBHOUSE 6801 N MEADOWDALE CLUBHOUSE 6801 N MEADOWDALE 001.000.66.518.30.47.00	71.90
Total :						1,015.60
226926	8/10/2017	076002 RODRIGUEZ, ARTURO	8/1 HMP RODRIGUEZ		8/1 HMP RODRIGUEZ 8/1 HMP RODRIGUEZ 117.100.64.573.20.41.00	700.00
Total :						700.00
226927	8/10/2017	071467 S MORRIS COMPANY	JULY 2017		INVOICE DATED 7/31/17 ACCT#700 #23425 1 NPC - 7/13/17	

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 23

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 24

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226927	8/10/2017	071467 S MORRIS COMPANY	(Continued)		001.000.41.521.70.41.00 #23450 3 NPC - 7/27/17 001.000.41.521.70.41.00	12.11 36.30
					Total :	48.41
226928	8/10/2017	066806 SANDERS, BILL	073117		SICK LEAVE BUY BACK SICK LEAVE BUY BACK FOR 7/1/17 111.000.68.544.70.11.00	 111.47
					Total :	111.47
226929	8/10/2017	074431 SEATTLE ASBESTOS TEST LLC	201712847		CIVIC FIELD CIVIC FIELD 001.000.64.576.80.41.00	 1,250.00
					Total :	1,250.00
226930	8/10/2017	066964 SEATTLE AUTOMOTIVE DIST INC	S3-2334608		UNIT 105 - TRANS FLUID Unit 105 - Trans Fluid 511.000.77.548.68.31.10 10.3% Sales Tax 511.000.77.548.68.31.10	 51.48 5.31
					Total :	56.79
226931	8/10/2017	074997 SEITEL SYSTEMS, LLC	41512		REMOTE COMPUTER SUPPORT Remote computer support 7/18/17 512.000.31.518.88.41.00	 620.00
			41524		WWTP SHAREPOINT MIGRATION C WWTP Sharepoint migration remote i 423.000.76.535.80.41.00	 481.25
					Total :	1,101.25
226932	8/10/2017	063306 SHERWIN-WILLIAMS	5847-1		PM: PAINT PM: PAINT 001.000.64.576.80.31.00 10.3% Sales Tax 001.000.64.576.80.31.00	 53.48 5.51

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 24

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 25

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226932	8/10/2017	063306 063306 SHERWIN-WILLIAMS	(Continued)		Total :	58.95
226933	8/10/2017	068489 SIRENNET.COM	0217726-IN		UNIT E140SO - SOLO LINEAR SURF Unit E140SO - Solo Linear Surfa 511.100.77.594.48.64.00 10.3% Sales Tax 511.100.77.594.48.64.00	1,032.80 106.38
			0217816-IN		UNIT E135,136PO - CONSOLE, AND Unit E135,136PO - Console, and Sup 511.100.77.594.48.64.00 Freight 511.100.77.594.48.64.00 10.3% Sales Tax 511.100.77.594.48.64.00	1,719.78 297.45 207.78
					Total :	3,364.25
226934	8/10/2017	036955 SKY NURSERY	T-0990633		PM: SAFER ENDALL INSECT PM: SAFER ENDALL INSECT 001.000.64.576.81.31.00 10.0% Sales Tax 001.000.64.576.81.31.00	32.97 3.30
					Total :	36.27
226935	8/10/2017	075543 SNO CO PUBLIC DEFENDER ASSOC	1886		JULY 2017 PUBLIC DEFENSE CONT JULY 2017 PUBLIC DEFENSE CONT 001.000.39.512.52.41.00	21,546.65
					Total :	21,546.65
226936	8/10/2017	037375 SNO CO PUD NO 1	2002-0255-4		WWTP: 7/6-8/3/17 FLOW METER 24 7/6-8/3/17 FLOW METER 2400 HIGH	16.60
			2003-2646-0		423.000.76.535.80.47.62 HUMMINGBIRD PARK 1000 EDMON HUMMINGBIRD PARK 1000 EDMON 001.000.64.576.80.47.00	18.32
			2004-9315-3		TRAFFIC LIGHT 22000 76TH AVE W TRAFFIC LIGHT 22000 76TH AVE W	

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 25

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 26

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226936	8/10/2017	037375 SNO CO PUD NO 1	(Continued)			
			2006-6395-3		111.000.68.542.64.47.00 ANWAY PARK 131 SUNSET AVE / MI ANWAY PARK 131 SUNSET AVE / MI	28.21
			2010-5432-7		001.000.64.576.80.47.00 BRACKETT'S LANDING NORTH 50 F BRACKETT'S LANDING NORTH 50 F	136.63
			2011-0356-1		001.000.64.576.80.47.00 TRAFFIC LIGHT 23800 FIRDALE AVI TRAFFIC LIGHT 23800 FIRDALE AVI	37.08
			2011-8789-5		111.000.68.542.64.47.00 TRAFFIC LIGHT 21132 76TH AVE W TRAFFIC LIGHT 21132 76TH AVE W	32.35
			2015-0127-7		111.000.68.542.64.47.00 LIFT STATION #14 7905 1/2 211TH P LIFT STATION #14 7905 1/2 211TH P	50.30
			2015-5730-3		423.000.75.535.80.47.10 CEMETERY BUILDING CEMETERY BUILDING	18.85
			2019-2991-6		130.000.64.536.50.47.00 WWTP: 6/23-7/21/17 FLOW METER WWTP: 6/23-7/21/17 FLOW METER	34.81
			2021-3965-5		423.000.76.535.80.47.62 BRACKETT'S LANDING SOUTH 100 BRACKETT'S LANDING SOUTH 100	16.60
			2021-6153-5		001.000.64.576.80.47.00 CEMETERY WELL PUMP CEMETERY WELL PUMP	17.17
			2021-9128-4		130.000.64.536.50.47.00 PEDEST CAUTION LIGHT 7801 2121 PEDEST CAUTION LIGHT 7801 2121	342.16
			2022-5063-5		111.000.68.542.63.47.00 9TH/CASPER LANDSCAPE BED / M 9TH/CASPER LANDSCAPE BED / M	17.17
			2025-2920-2		001.000.64.576.80.47.00 STREET LIGHTING (13 LIGHTS @ 4	18.85

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 26

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 27

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226936	8/10/2017	037375 SNO CO PUD NO 1	(Continued)		STREET LIGHTING (13 LIGHTS @ 4 111.000.68.542.63.47.00	123.89
			2025-7615-3		STREET LIGHTING (2029 LIGHTS @ STREET LIGHTING (2029 LIGHTS @ 111.000.68.542.63.47.00	15,477.70
			2025-7948-8		STREET LIGHTING (58 LIGHTS @ 2 STREET LIGHTING (58 LIGHTS @ 2 111.000.68.542.63.47.00	360.25
			2025-7952-0		WWTP: 7/1-7/31/17 ENERGY MGMT 7/1-7/31/17 WWTP ENERGY MANAC 423.000.76.535.80.47.61	9.76
			2047-1489-3		STREET LIGHTING (1 LIGHT @ 150' STREET LIGHTING (1 LIGHT @ 150' 111.000.68.542.63.47.00	4.98
			2047-1492-7		STREET LIGHTING (18 LIGHTS @ 2 STREET LIGHTING (18 LIGHTS @ 2 111.000.68.542.63.47.00	116.87
			2047-1493-5		STREET LIGHTING (5 LIGHTS @ 40 STREET LIGHTING (5 LIGHTS @ 40 111.000.68.542.63.47.00	58.35
			2047-1494-3		STREET LIGHTING (2 LIGHTS @ 10 STREET LIGHTING (2 LIGHTS @ 10 111.000.68.542.63.47.00	14.84
			2047-1495-0		STREET LIGHTING (26 LIGHTS @ 2 STREET LIGHTING (26 LIGHTS @ 2 111.000.68.542.63.47.00	92.98
			2053-0758-0		DECORATIVE & STREET LIGHTING DECORATIVE & STREET LIGHTING 111.000.68.542.64.47.00	116.60
					Total :	17,161.28
226937	8/10/2017	038300 SOUND DISPOSAL CO	103587		PARKS MAINT GARBAGE AND REC' PARKS MAINT GARBAGE AND REC' 001.000.64.576.80.47.00	1,089.65

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 27

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 28

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226937	8/10/2017	038300 038300 SOUND DISPOSAL CO	(Continued)			
					Total :	1,089.61
226938	8/10/2017	076086 STATEHOOD MEDIA LLC	2017ci-134		WOTS AD WOTS AD 123.000.64.573.20.41.40	800.00
					Total :	800.00
226939	8/10/2017	075971 STEEL MAGIC NORTHWEST	8/6 CP STEEL		8/6 CP STEEL 8/6 CP STEEL 117.100.64.573.20.41.00	600.00
					Total :	600.00
226940	8/10/2017	074406 STUVERUD, ROBERT	8/8 HMP STUVERUD		8/8 HMP STUVERUD 8/8 HMP STUVERUD 117.100.64.573.20.41.00	600.00
					Total :	600.00
226941	8/10/2017	040917 TACOMA SCREW PRODUCTS INC	18160371		STORM -MARKING PAINT Storm -Marking Paint 422.000.72.531.40.31.00 10.3% Sales Tax	159.69
			18169202		422.000.72.531.40.31.00 PM: SUPPLIES YOST PM: SUPPLIES YOST 001.000.64.576.80.31.00 10.3% Sales Tax 001.000.64.576.80.31.00	16.45 19.79 2.04
					Total :	197.97
226942	8/10/2017	013007 THE ESTATE OF EZRA J HASNER	56		LEOFF 1 REIMBURSEMENT LEOFF 1 REIMBURSEMENT 009.000.39.517.20.29.00	4,400.51
					Total :	4,400.51
226943	8/10/2017	027269 THE PART WORKS INC	INV15528		PM: YOST SHOWER HEAD PM: YOST SHOWER HEAD 001.000.64.576.80.31.00	84.48

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 28

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 29

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226943	8/10/2017	027269 THE PART WORKS INC	(Continued)		10.3% Sales Tax 001.000.64.576.80.31.00	8.70
					Total :	93.18
226944	8/10/2017	075691 TIBBOTT, NEIL	TIB052017		REIMBURSEMENT - TRAVEL CNU Conference Milage:~ 001.000.11.511.60.43.00 Parking 001.000.11.511.60.43.00	56.37 48.00
					Total :	104.37
226945	8/10/2017	072800 TOYOTA LIFT NORTHWEST	24112245		WWTP: ANNUAL FORKLIFT WARRA ANNUAL FORKLIFT WARRANTY INS 423.000.76.535.80.48.00 10.3% Sales Tax 423.000.76.535.80.48.00	187.50 19.37
			24833948		WWTP: ANNUAL HAND TRUCK WAI ANNUAL HAND TRUCK WARRANTY 423.000.76.535.80.48.00 10.3% Sales Tax 423.000.76.535.80.48.00	85.00 8.76
					Total :	300.57
226946	8/10/2017	074800 TURNSTYLE INC	BID-4302		BID/ED! CROSSWALK DESIGN BID/Ed! design work for crosswalk 140.000.61.558.70.41.00	1,750.00
					Total :	1,750.00
226947	8/10/2017	074494 UK SOCCER ELITE	5682 UK ELITE		5682 UK ELITE SOCCER INSTRUCT 5682 UK ELITE SOCCER INSTRUCT 001.000.64.571.25.41.00	2,970.00
					Total :	2,970.00
226948	8/10/2017	068724 US HEALTHWORKS MED GROUP OF WA	0735685-WA		HEP B VACCINE EMPLOYMENT HEP B VACCINE FOI	

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 29

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 30

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226948	8/10/2017	068724	US HEALTHWORKS MED GROUP OF WA (Continued)			
			0736700-WA		421.000.74.534.80.41.00 US HEALTHWORKS TESTING/VACC HEP B VACCINE	94.00
					001.000.41.521.10.41.00 DOT TESTING	94.00
					511.000.77.548.68.41.00	99.00
					Total :	287.00
226949	8/10/2017	067086	WASHINGTON CRANE AND HOIST CO	0033454-IN	WWTP: HOIST VFD SERVICE CALL HOIST VFD SERVICE CALL	
					423.000.76.535.80.48.00 10.3% Sales Tax	1,373.00
					423.000.76.535.80.48.00	141.42
					Total :	1,514.42
226950	8/10/2017	066238	WASHINGTON TRACTOR	1355656	UNIT 116 - SWITCH Unit 116 - Switch	
					511.000.77.548.68.31.10 9.1% Sales Tax	54.90
					511.000.77.548.68.31.10	5.00
					Total :	59.90
226951	8/10/2017	074609	WEST COAST ARMORY NORTH	JUL-17	INV#JUL-17 CUST ID - EDMONDS P RANGE USAGE - SAUNDERS 7/1/17	
					001.000.41.521.40.41.00	13.60
					RANGE USAGE - GAGNER 7/11/17	
					001.000.41.521.40.41.00	13.60
					RANGE USAGE - BRUGGMAN 7/16/	
					001.000.41.521.40.41.00	13.60
					RANGE USAGE - SWARTZ 7/16/17	
					001.000.41.521.40.41.00	13.60
					RANGE USAGE - GAGNER 7/21/17	
					001.000.41.521.40.41.00	13.60
					RANGE USAGE - SAUNDERS 7/27/1	
					001.000.41.521.40.41.00	13.60

Attachment: claim cks 08-10-17 (Approval of claim checks.)

Page: 30

vchlist
08/10/2017 7:58:21AM

Voucher List
City of Edmonds

Page: 31

Bank code : usbank

Voucher	Date	Vendor	Invoice	PO #	Description/Account	Amount
226951	8/10/2017	074609 WEST COAST ARMORY NORTH	(Continued)		10.3% Sales Tax	
					001.000.41.521.40.41.00	8.40
			JUN-17		INV#JUN-17 CUST ID - EDMONDS F	
					RANGE USAGE - SAUNDERS 6/4/17	
					001.000.41.521.40.41.00	13.60
					RANGE USAGE - MEHL 6/22/17	
					001.000.41.521.40.41.00	13.60
					RANGE USAGE - HAUGHIAN 6/29/1	
					001.000.41.521.40.41.00	13.60
					10.3% Sales Tax	
					001.000.41.521.40.41.00	4.20
					Total :	135.00
226952	8/10/2017	072627 WEST SAFETY SERVICES INC	7003494		MONTHLY 911 DATABASE MAINT	
					Monthly 911 database maint	
					512.000.31.518.88.48.00	100.00
					Total :	100.00
226953	8/10/2017	064213 WSSUA TREASURER	461		UMPS FOR ADULT SOFTBALL LEAG	
					UMPS FOR ADULT SOFTBALL LEAG	
					001.000.64.571.25.41.00	840.00
			465		UMPS FOR ADULT SUMMER SOFTE	
					UMPS FOR ADULT SUMMER SOFTE	
					001.000.64.571.25.41.00	1,085.00
					Total :	1,925.00
122 Vouchers for bank code : usbank						Bank total : 356,823.80
122 Vouchers in this report						Total vouchers : 356,823.80

Attachment: claim cks 08-10-17 (Approval of claim checks.)

PROJECT NUMBERS (By Project Title)

<u>Funding</u>	<u>Project Title</u>	<u>Project Accounting Number</u>	<u>Engineering Project Number</u>
STM	12th Ave & Sierra Stormwater System Improvements	c484	E5FE
STR	15th St. SW Walkway (Edmonds Way to 8th Ave)	c424	E3DC
STM	183rd PI SW Storm Repairs	c491	E6FE
SWR	2013 Sanitary Sewer Pipe Rehabilitation	c390	E2GB
SWR	2013 Sewerline Replacement Project	c398	E3GA
STR	2014 Chip Seals	c451	E4CB
STM	2014 Drainage Improvements	c433	E4FA
STM	2014 Lake Ballinger Associated Projects	c436	E4FD
STR	2014 Overlay Program	c438	E4CA
WTR	2014 Waterline Overlays	c452	E4CC
STM	2015 Citywide Drainage Improvements/Rehab Projects	c466	E5FA
STR	2015 Overlay Program	c463	E5CA
SWR	2015 Sewerline Overlays	i007	E5CC
SWR	2015 Sewerline Replacement Project	c441	E4GA
STR	2015 Traffic Calming	c471	E5AB
WTR	2015 Waterline Overlays	c475	E5CB
WTR	2015 Waterline Replacement Program	c440	E4JB
STR	2016 Curb Ramp Upgrades	i016	E6DC
STR	2016 Overlay Program	i008	E6CA
SWR	2016 Sanitary Sewer Replacement Projects	c469	E5GA
SWR	2016 Sewerline Overlays	i010	E6CC
WTR	2016 Water Comp Plan Update	c460	E4JC
WTR	2016 Waterline Overlays	i009	E6CB
WTR	2016 Waterline Replacement Projects	c468	E5JA
STR	2017 Curb Ramp Upgrades	i022	E7DA
STR	2017 Minor Sidewalk Program	i023	E7DB
STR	2017 Overlay Program	i018	E7CA
SWR	2017 Sanitary Sewer Replacement Project	i013	E6GA
SWR	2017 Sewerline Overlays	i020	E7CC
STR	2017 Traffic Calming	i021	E7AA
WTR	2017 Waterline Overlays	i019	E7CB
WTR	2017 Waterline Replacement Projects	i014	E6JB
SWR	2018/19 Sewerline Replacement Project	c492	E6GC
WTR	2018/19 Waterline Replacement Project	c493	E6JC
STR	220th Street Overlay Project	c462	E4CD
STM	224th & 98th Drainage Improvements	c486	E6FB

Attachment: FrequentlyUsedProjNumbers 08-10-17 (Approval of claim checks.)

PROJECT NUMBERS (By Project Title)

<u>Funding</u>	<u>Project Title</u>	<u>Project Accounting Number</u>	<u>Engineering Project Number</u>
WTR	224th Waterline Relocation (2013)	c418	E3JB
STR	228th St. SW Corridor Improvements	i005	E7AC
STR	236th St. SW Walkway (Edmonds Way to Madrona School)	c425	E3DD
STR	238th St. SW Walkway (100th Ave to 104th Ave)	c423	E3DB
STR	238th St. SW Walkway (Edmonds Way to Hwy 99)	c485	E6DA
STM	3rd Ave Rain Gardens	i012	E6FC
STR	76th Ave W at 212th St SW Intersection Improvements	c368	E1CA
STR	89th Pl W Retaining Wall	i025	E7CD
STR	9th Avenue Improvement Project	c392	E2AB
FAC	A/V Upgrades - Council Chambers	c476	E5LA
STR	ADA Curb Ramp Upgrades along 3rd Ave S	c426	E3DE
STR	ADA Transition Plan	s016	E6DB
STR	Audible Pedestrian Signals	i024	E7AB
STR	Bikelink Project	c474	E5DA
PRK	City Spray Park	c417	E4MA
SWR	Citywide CIPP Sewer Rehab Phase I	c456	E4GB
SWR	Citywide CIPP Sewer Rehab Phase II	c488	E6GB
STR	Citywide Pedestrian Crossing Enhancements	i026	E7DC
STR	Citywide Protected/Permissive Traffic Signal Conversion	i015	E6AB
WTR	Dayton St. Utility Replacement Project (3rd Ave to 9th Ave)	c482	E5JB
STM	Dayton Street & SR104 Storm Drainage Alternatives	c374	E1FM
PM	Dayton Street Plaza	c276	E7MA
STM	Dayton Street Storm Improvements (6th Ave - 8th Ave)	c472	E5FC
STM	Dayton Street Stormwater Pump Station	c455	E4FE
FAC	Edmonds Fishing Pier Rehab	c443	E4MB
STM	Edmonds Marsh Feasibility Study	c380	E2FC
General	Edmonds Waterfront Access Analysis	c478	E5DB
FAC	ESCO III Project	c419	E3LB
PRK	FAC Band Shell Replacement	c477	E6MB
WTR	Five Corners Reservoir Re-coating	c473	E5KA
STR	Five Corners Roundabout (212th Street SW @ 84th Avenue W)	c342	E1AA
PM	Fourth Avenue Cultural Corridor	c282	E8MA
STR	Hwy 99 Enhancements (Phase III)	c405	E2AD
STR	Hwy 99 Gateway Revitalization	s014	E6AA
SWR	Lake Ballinger Trunk Sewer Study	s011	E5GB
STM	LID Retrofits Perrinville Creek Basin	c434	E4FB

Attachment: FrequentlyUsedProjNumbers 08-10-17 (Approval of claim checks.)

PROJECT NUMBERS (By Project Title)

<u>Funding</u>	<u>Project Title</u>	<u>Project Accounting Number</u>	<u>Engineering Project Number</u>
SWR	Lift Station #1 Basin & Flow Study	c461	E4GC
STR	Minor Sidewalk Program	i017	E6DD
STM	North Talbot Road Drainage Improvements	c378	E2FA
STM	Northstream Culvert Repair Under Puget Drive	i011	E6FA
STM	Northstream Pipe Abandonment on Puget Drive	c410	E3FE
STM	NPDES (Students Saving Salmon)	m013	E7FG
STM	OVD Slope Repair & Stabilization	m105	E7FA
STM	Perrinville Creek Culvert Replacement	c376	E1FN
STM	Perrinville Creek Stormwater Flow Reduction Retrofit Study	c408	E3FC
FAC	Public Safety Controls System Upgrades	c444	E4LA
STM	Seaview Park Infiltration Facility	c479	E5FD
WWTP	Sewer Outfall Groundwater Monitoring	c446	E4HA
STR	SR104 Corridor Transportation Study	c427	E3AB
STR	SR104/City Park Mid-Block Crossing	c454	E4DB
UTILITIES	Standard Details Updates	s010	E5NA
STM	Storm Drain Improvements @ 9510 232nd St. SW	c495	E7FB
STM	Storm Drainage Improvements - 88th & 194th	c429	E3FG
STM	Stormwater Comp Plan Update	s017	E6FD
STR	Sunset Walkway Improvements	c354	E1DA
STM	SW Edmonds Basin #3-238th St. SW to Hickman Park Infiltration System	c379	E2FB
STM	SW Edmonds-105th/106th Ave W Storm Improvements	c430	E3FH
STR	Trackside Warning System	c470	E5AA
STR	Train Trench - Concept	c453	E4DA
STR	Transportation Plan Update	c391	E2AA
STM	Update Stormwater Management Code & Associated Projects	c467	E5FB
UTILITIES	Utility Rate Update	s013	E6JA
PRK	Veteran's Plaza	c480	E6MA
STM	Video Assessment of Stormwater Lines	c459	E4FF
PRK	Waterfront Restoration	m103	E7MA
STM	Willow Creek Daylighting/Edmonds Marsh Restoration	c435	E4FC
WWTP	WWTP Outfall Pipe Modifications	c481	E5HA
PRK	Yost Park Spa	c494	E6MC

Attachment: FrequentlyUsedProjNumbers 08-10-17 (Approval of claim checks.)

PROJECT NUMBERS (By Engineering Number)

<u>Funding</u>	<u>Engineering Project Number</u>	<u>Project Accounting Number</u>	<u>Project Title</u>
STR	E1AA	c342	Five Corners Roundabout (212th Street SW @ 84th Avenue W)
STR	E1CA	c368	76th Ave W at 212th St SW Intersection Improvements
STR	E1DA	c354	Sunset Walkway Improvements
STM	E1FM	c374	Dayton Street & SR104 Storm Drainage Alternatives
STM	E1FN	c376	Perrinville Creek Culvert Replacement
STR	E2AA	c391	Transportation Plan Update
STR	E2AB	c392	9th Avenue Improvement Project
STR	E2AD	c405	Hwy 99 Enhancements (Phase III)
STM	E2FA	c378	North Talbot Road Drainage Improvements
STM	E2FB	c379	SW Edmonds Basin #3-238th St. SW to Hickman Park Infiltration System
STM	E2FC	c380	Edmonds Marsh Feasibility Study
SWR	E2GB	c390	2013 Sanitary Sewer Pipe Rehabilitation
STR	E3AB	c427	SR104 Corridor Transportation Study
STR	E3DB	c423	238th St. SW Walkway (100th Ave to 104th Ave)
STR	E3DC	c424	15th St. SW Walkway (Edmonds Way to 8th Ave)
STR	E3DD	c425	236th St. SW Walkway (Edmonds Way to Madrona School)
STR	E3DE	c426	ADA Curb Ramp Upgrades along 3rd Ave S
STM	E3FC	c408	Perrinville Creek Stormwater Flow Reduction Retrofit Study
STM	E3FE	c410	Northstream Pipe Abandonment on Puget Drive
STM	E3FG	c429	Storm Drainage Improvements - 88th & 194th
STM	E3FH	c430	SW Edmonds-105th/106th Ave W Storm Improvements
SWR	E3GA	c398	2013 Sewerline Replacement Project
WTR	E3JB	c418	224th Waterline Relocation (2013)
FAC	E3LB	c419	ESCO III Project
STR	E4CA	c438	2014 Overlay Program
STR	E4CB	c451	2014 Chip Seals
WTR	E4CC	c452	2014 Waterline Overlays
STR	E4CD	c462	220th Street Overlay Project
STR	E4DA	c453	Train Trench - Concept
STR	E4DB	c454	SR104/City Park Mid-Block Crossing
STM	E4FA	c433	2014 Drainage Improvements
STM	E4FB	c434	LID Retrofits Perrinville Creek Basin
STM	E4FC	c435	Willow Creek Daylighting/Edmonds Marsh Restoration
STM	E4FD	c436	2014 Lake Ballinger Associated Projects
STM	E4FE	c455	Dayton Street Stormwater Pump Station
STM	E4FF	c459	Video Assessment of Stormwater Lines

Attachment: FrequentlyUsedProjNumbers 08-10-17 (Approval of claim checks.)

PROJECT NUMBERS (By Engineering Number)

<u>Funding</u>	<u>Engineering Project Number</u>	<u>Project Accounting Number</u>	<u>Project Title</u>
SWR	E4GA	c441	2015 Sewerline Replacement Project
SWR	E4GB	c456	Citywide CIPP Sewer Rehab Phase I
SWR	E4GC	c461	Lift Station #1 Basin & Flow Study
WWTP	E4HA	c446	Sewer Outfall Groundwater Monitoring
WTR	E4JB	c440	2015 Waterline Replacement Program
WTR	E4JC	c460	2016 Water Comp Plan Update
FAC	E4LA	c444	Public Safety Controls System Upgrades
PRK	E4MA	c417	City Spray Park
FAC	E4MB	c443	Edmonds Fishing Pier Rehab
STR	E5AA	c470	Trackside Warning System
STR	E5AB	c471	2015 Traffic Calming
STR	E5CA	c463	2015 Overlay Program
WTR	E5CB	c475	2015 Waterline Overlays
SWR	E5CC	i007	2015 Sewerline Overlays
STR	E5DA	c474	Bikelink Project
General	E5DB	c478	Edmonds Waterfront Access Analysis
STM	E5FA	c466	2015 Citywide Drainage Improvements/Rehab Projects
STM	E5FB	c467	Update Stormwater Management Code & Associated Projects
STM	E5FC	c472	Dayton Street Storm Improvements (6th Ave - 8th Ave)
STM	E5FD	c479	Seaview Park Infiltration Facility
STM	E5FE	c484	12th Ave & Sierra Stormwater System Improvements
SWR	E5GA	c469	2016 Sanitary Sewer Replacement Projects
SWR	E5GB	s011	Lake Ballinger Trunk Sewer Study
WWTP	E5HA	c481	WWTP Outfall Pipe Modifications
WTR	E5JA	c468	2016 Waterline Replacement Projects
WTR	E5JB	c482	Dayton St. Utility Replacement Project (3rd Ave to 9th Ave)
WTR	E5KA	c473	Five Corners Reservoir Re-coating
FAC	E5LA	c476	A/V Upgrades - Council Chambers
UTILITIES	E5NA	s010	Standard Details Updates
STR	E6AA	s014	Hwy 99 Gateway Revitalization
STR	E6AB	i015	Citywide Protected/Permissive Traffic Signal Conversion
STR	E6CA	i008	2016 Overlay Program
WTR	E6CB	i009	2016 Waterline Overlays
SWR	E6CC	i010	2016 Sewerline Overlays
STR	E6DA	c485	238th St. SW Walkway (Edmonds Way to Hwy 99)
STR	E6DB	s016	ADA Transition Plan

Attachment: FrequentlyUsedProjNumbers 08-10-17 (Approval of claim checks.)

PROJECT NUMBERS (By Engineering Number)

<u>Funding</u>	<u>Engineering Project Number</u>	<u>Project Accounting Number</u>	<u>Project Title</u>
STR	E6DC	i016	2016 Curb Ramp Upgrades
STR	E6DD	i017	Minor Sidewalk Program
STM	E6FA	i011	Northstream Culvert Repair Under Puget Drive
STM	E6FB	c486	224th & 98th Drainage Improvements
STM	E6FC	i012	3rd Ave Rain Gardens
STM	E6FD	s017	Stormwater Comp Plan Update
STM	E6FE	c491	183rd PI SW Storm Repairs
SWR	E6GA	i013	2017 Sanitary Sewer Replacement Project
SWR	E6GB	c488	Citywide CIPP Sewer Rehab Phase II
SWR	E6GC	c492	2018/19 Sewerline Replacement Project
UTILITIES	E6JA	s013	Utility Rate Update
WTR	E6JB	i014	2017 Waterline Replacement Projects
WTR	E6JC	c493	2018/19 Waterline Replacement Project
PRK	E6MA	c480	Veteran's Plaza
PRK	E6MB	c477	FAC Band Shell Replacement
PRK	E6MC	c494	Yost Park Spa
STR	E7AA	i021	2017 Traffic Calming
STR	E7AB	i024	Audible Pedestrian Signals
STR	E7AC	i005	228th St. SW Corridor Improvements
STR	E7CA	i018	2017 Overlay Program
WTR	E7CB	i019	2017 Waterline Overlays
SWR	E7CC	i020	2017 Sewerline Overlays
STR	E7CD	i025	89th PI W Retaining Wall
STR	E7DA	i022	2017 Curb Ramp Upgrades
STR	E7DB	i023	2017 Minor Sidewalk Program
STR	E7DC	i026	Citywide Pedestrian Crossing Enhancements
STM	E7FA	m105	OVD Slope Repair & Stabilization
STM	E7FB	c495	Storm Drain Improvements @ 9510 232nd St. SW
STM	E7FG	m013	NPDES (Students Saving Salmon)
PM	E7MA	c276	Dayton Street Plaza
PRK	E7MA	m103	Waterfront Restoration
PM	E8MA	c282	Fourth Avenue Cultural Corridor

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PROJECT NUMBERS (By New Project Accounting Number)

<u>Funding</u>	<u>Engineering Project Number</u>	<u>Project Accounting Number</u>	<u>Project Title</u>
PM	E7MA	c276	Dayton Street Plaza
PM	E8MA	c282	Fourth Avenue Cultural Corridor
STR	E1AA	c342	Five Corners Roundabout (212th Street SW @ 84th Avenue W)
STR	E1DA	c354	Sunset Walkway Improvements
STR	E1CA	c368	76th Ave W at 212th St SW Intersection Improvements
STM	E1FM	c374	Dayton Street & SR104 Storm Drainage Alternatives
STM	E1FN	c376	Perrinville Creek Culvert Replacement
STM	E2FA	c378	North Talbot Road Drainage Improvements
STM	E2FB	c379	SW Edmonds Basin #3-238th St. SW to Hickman Park Infiltration System
STM	E2FC	c380	Edmonds Marsh Feasibility Study
SWR	E2GB	c390	2013 Sanitary Sewer Pipe Rehabilitation
STR	E2AA	c391	Transportation Plan Update
STR	E2AB	c392	9th Avenue Improvement Project
SWR	E3GA	c398	2013 Sewerline Replacement Project
STR	E2AD	c405	Hwy 99 Enhancements (Phase III)
STM	E3FC	c408	Perrinville Creek Stormwater Flow Reduction Retrofit Study
STM	E3FE	c410	Northstream Pipe Abandonment on Puget Drive
PRK	E4MA	c417	City Spray Park
WTR	E3JB	c418	224th Waterline Relocation (2013)
FAC	E3LB	c419	ESCO III Project
STR	E3DB	c423	238th St. SW Walkway (100th Ave to 104th Ave)
STR	E3DC	c424	15th St. SW Walkway (Edmonds Way to 8th Ave)
STR	E3DD	c425	236th St. SW Walkway (Edmonds Way to Madrona School)
STR	E3DE	c426	ADA Curb Ramp Upgrades along 3rd Ave S
STR	E3AB	c427	SR104 Corridor Transportation Study
STM	E3FG	c429	Storm Drainage Improvements - 88th & 194th
STM	E3FH	c430	SW Edmonds-105th/106th Ave W Storm Improvements
STM	E4FA	c433	2014 Drainage Improvements
STM	E4FB	c434	LID Retrofits Perrinville Creek Basin
STM	E4FC	c435	Willow Creek Daylighting/Edmonds Marsh Restoration
STM	E4FD	c436	2014 Lake Ballinger Associated Projects
STR	E4CA	c438	2014 Overlay Program
WTR	E4JB	c440	2015 Waterline Replacement Program
SWR	E4GA	c441	2015 Sewerline Replacement Project
FAC	E4MB	c443	Edmonds Fishing Pier Rehab
FAC	E4LA	c444	Public Safety Controls System Upgrades

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PROJECT NUMBERS (By New Project Accounting Number)

<u>Funding</u>	<u>Engineering Project Number</u>	<u>Project Accounting Number</u>	<u>Project Title</u>
WWTP	E4HA	c446	Sewer Outfall Groundwater Monitoring
STR	E4CB	c451	2014 Chip Seals
WTR	E4CC	c452	2014 Waterline Overlays
STR	E4DA	c453	Train Trench - Concept
STR	E4DB	c454	SR104/City Park Mid-Block Crossing
STM	E4FE	c455	Dayton Street Stormwater Pump Station
SWR	E4GB	c456	Citywide CIPP Sewer Rehab Phase I
STM	E4FF	c459	Video Assessment of Stormwater Lines
WTR	E4JC	c460	2016 Water Comp Plan Update
SWR	E4GC	c461	Lift Station #1 Basin & Flow Study
STR	E4CD	c462	220th Street Overlay Project
STR	E5CA	c463	2015 Overlay Program
STM	E5FA	c466	2015 Citywide Drainage Improvements/Rehab Projects
STM	E5FB	c467	Update Stormwater Management Code & Associated Projects
WTR	E5JA	c468	2016 Waterline Replacement Projects
SWR	E5GA	c469	2016 Sanitary Sewer Replacement Projects
STR	E5AA	c470	Trackside Warning System
STR	E5AB	c471	2015 Traffic Calming
STM	E5FC	c472	Dayton Street Storm Improvements (6th Ave - 8th Ave)
WTR	E5KA	c473	Five Corners Reservoir Re-coating
STR	E5DA	c474	Bikelink Project
WTR	E5CB	c475	2015 Waterline Overlays
FAC	E5LA	c476	A/V Upgrades - Council Chambers
PRK	E6MB	c477	FAC Band Shell Replacement
General	E5DB	c478	Edmonds Waterfront Access Analysis
STM	E5FD	c479	Seaview Park Infiltration Facility
PRK	E6MA	c480	Veteran's Plaza
WWTP	E5HA	c481	WWTP Outfall Pipe Modifications
WTR	E5JB	c482	Dayton St. Utility Replacement Project (3rd Ave to 9th Ave)
STM	E5FE	c484	12th Ave & Sierra Stormwater System Improvements
STR	E6DA	c485	238th St. SW Walkway (Edmonds Way to Hwy 99)
STM	E6FB	c486	224th & 98th Drainage Improvements
SWR	E6GB	c488	Citywide CIPP Sewer Rehab Phase II
STM	E6FE	c491	183rd PI SW Storm Repairs
SWR	E6GC	c492	2018/19 Sewerline Replacement Project
WTR	E6JC	c493	2018/19 Waterline Replacement Project

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PROJECT NUMBERS (By New Project Accounting Number)

<u>Funding</u>	<u>Engineering Project Number</u>	<u>Project Accounting Number</u>	<u>Project Title</u>
PRK	E6MC	c494	Yost Park Spa
STM	E7FB	c495	Storm Drain Improvements @ 9510 232nd St. SW
STR	E7AC	i005	228th St. SW Corridor Improvements
SWR	E5CC	i007	2015 Sewerline Overlays
STR	E6CA	i008	2016 Overlay Program
WTR	E6CB	i009	2016 Waterline Overlays
SWR	E6CC	i010	2016 Sewerline Overlays
STM	E6FA	i011	Northstream Culvert Repair Under Puget Drive
STM	E6FC	i012	3rd Ave Rain Gardens
SWR	E6GA	i013	2017 Sanitary Sewer Replacement Project
WTR	E6JB	i014	2017 Waterline Replacement Projects
STR	E6AB	i015	Citywide Protected/Permissive Traffic Signal Conversion
STR	E6DC	i016	2016 Curb Ramp Upgrades
STR	E6DD	i017	Minor Sidewalk Program
STR	E7CA	i018	2017 Overlay Program
WTR	E7CB	i019	2017 Waterline Overlays
SWR	E7CC	i020	2017 Sewerline Overlays
STR	E7AA	i021	2017 Traffic Calming
STR	E7DA	i022	2017 Curb Ramp Upgrades
STR	E7DB	i023	2017 Minor Sidewalk Program
STR	E7AB	i024	Audible Pedestrian Signals
STR	E7CD	i025	89th PI W Retaining Wall
STR	E7DC	i026	Citywide Pedestrian Crossing Enhancements
STM	E7FG	m013	NPDES (Students Saving Salmon)
PRK	E7MA	m103	Waterfront Restoration
STM	E7FA	m105	OVD Slope Repair & Stabilization
UTILITIES	E5NA	s010	Standard Details Updates
SWR	E5GB	s011	Lake Ballinger Trunk Sewer Study
UTILITIES	E6JA	s013	Utility Rate Update
STR	E6AA	s014	Hwy 99 Gateway Revitalization
STR	E6DB	s016	ADA Transition Plan
STM	E6FD	s017	Stormwater Comp Plan Update

Attachment: FrequentlyUsedProjNumbers 08-10-17 (Approval of claim checks.)

PROJECT NUMBERS (By Funding)

<u>Funding</u>	<u>Project Title</u>	<u>Project Accounting Number</u>	<u>Engineering Project Number</u>
FAC	A/V Upgrades - Council Chambers	c476	E5LA
FAC	Edmonds Fishing Pier Rehab	c443	E4MB
FAC	ESCO III Project	c419	E3LB
FAC	Public Safety Controls System Upgrades	c444	E4LA
General	Edmonds Waterfront Access Analysis	c478	E5DB
PM	Dayton Street Plaza	c276	E7MA
PM	Fourth Avenue Cultural Corridor	c282	E8MA
PRK	City Spray Park	c417	E4MA
PRK	FAC Band Shell Replacement	c477	E6MB
PRK	Veteran's Plaza	c480	E6MA
PRK	Waterfront Restoration	m103	E7MA
PRK	Yost Park Spa	c494	E6MC
STM	12th Ave & Sierra Stormwater System Improvements	c484	E5FE
STM	183rd Pl SW Storm Repairs	c491	E6FE
STM	2014 Drainage Improvements	c433	E4FA
STM	2014 Lake Ballinger Associated Projects	c436	E4FD
STM	2015 Citywide Drainage Improvements/Rehab Projects	c466	E5FA
STM	224th & 98th Drainage Improvements	c486	E6FB
STM	3rd Ave Rain Gardens	i012	E6FC
STM	Dayton Street & SR104 Storm Drainage Alternatives	c374	E1FM
STM	Dayton Street Storm Improvements (6th Ave - 8th Ave)	c472	E5FC
STM	Dayton Street Stormwater Pump Station	c455	E4FE
STM	Edmonds Marsh Feasibility Study	c380	E2FC
STM	LID Retrofits Perrinville Creek Basin	c434	E4FB
STM	North Talbot Road Drainage Improvements	c378	E2FA
STM	Northstream Culvert Repair Under Puget Drive	i011	E6FA
STM	Northstream Pipe Abandonment on Puget Drive	c410	E3FE
STM	NPDES (Students Saving Salmon)	m013	E7FG
STM	OVD Slope Repair & Stabilization	m105	E7FA
STM	Perrinville Creek Culvert Replacement	c376	E1FN
STM	Perrinville Creek Stormwater Flow Reduction Retrofit Study	c408	E3FC
STM	Seaview Park Infiltration Facility	c479	E5FD
STM	Storm Drain Improvements @ 9510 232nd St. SW	c495	E7FB
STM	Storm Drainage Improvements - 88th & 194th	c429	E3FG
STM	Stormwater Comp Plan Update	s017	E6FD

Attachment: Frequently Used Project Numbers 08-10-17 (Approval of claim checks.)

PROJECT NUMBERS (By Funding)

<u>Funding</u>	<u>Project Title</u>	<u>Project Accounting Number</u>	<u>Engineering Project Number</u>
STM	SW Edmonds Basin #3-238th St. SW to Hickman Park Infiltration System	c379	E2FB
STM	SW Edmonds-105th/106th Ave W Storm Improvements	c430	E3FH
STM	Update Stormwater Management Code & Associated Projects	c467	E5FB
STM	Video Assessment of Stormwater Lines	c459	E4FF
STM	Willow Creek Daylighting/Edmonds Marsh Restoration	c435	E4FC
STR	15th St. SW Walkway (Edmonds Way to 8th Ave)	c424	E3DC
STR	2014 Chip Seals	c451	E4CB
STR	2014 Overlay Program	c438	E4CA
STR	2015 Overlay Program	c463	E5CA
STR	2015 Traffic Calming	c471	E5AB
STR	2016 Curb Ramp Upgrades	i016	E6DC
STR	2016 Overlay Program	i008	E6CA
STR	2017 Curb Ramp Upgrades	i022	E7DA
STR	2017 Minor Sidewalk Program	i023	E7DB
STR	2017 Overlay Program	i018	E7CA
STR	2017 Traffic Calming	i021	E7AA
STR	220th Street Overlay Project	c462	E4CD
STR	228th St. SW Corridor Improvements	i005	E7AC
STR	236th St. SW Walkway (Edmonds Way to Madrona School)	c425	E3DD
STR	238th St. SW Walkway (100th Ave to 104th Ave)	c423	E3DB
STR	238th St. SW Walkway (Edmonds Way to Hwy 99)	c485	E6DA
STR	76th Ave W at 212th St SW Intersection Improvements	c368	E1CA
STR	89th Pl W Retaining Wall	i025	E7CD
STR	9th Avenue Improvement Project	c392	E2AB
STR	ADA Curb Ramp Upgrades along 3rd Ave S	c426	E3DE
STR	ADA Transition Plan	s016	E6DB
STR	Audible Pedestrian Signals	i024	E7AB
STR	Bikelink Project	c474	E5DA
STR	Citywide Pedestrian Crossing Enhancements	i026	E7DC
STR	Citywide Protected/Permissive Traffic Signal Conversion	i015	E6AB
STR	Five Corners Roundabout (212th Street SW @ 84th Avenue W)	c342	E1AA
STR	Hwy 99 Enhancements (Phase III)	c405	E2AD
STR	Hwy 99 Gateway Revitalization	s014	E6AA
STR	Minor Sidewalk Program	i017	E6DD
STR	SR104 Corridor Transportation Study	c427	E3AB

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PROJECT NUMBERS (By Funding)

<u>Funding</u>	<u>Project Title</u>	<u>Project Accounting Number</u>	<u>Engineering Project Number</u>
STR	SR104/City Park Mid-Block Crossing	c454	E4DB
STR	Sunset Walkway Improvements	c354	E1DA
STR	Trackside Warning System	c470	E5AA
STR	Train Trench - Concept	c453	E4DA
STR	Transportation Plan Update	c391	E2AA
SWR	2013 Sanitary Sewer Pipe Rehabilitation	c390	E2GB
SWR	2013 Sewerline Replacement Project	c398	E3GA
SWR	2015 Sewerline Overlays	i007	E5CC
SWR	2015 Sewerline Replacement Project	c441	E4GA
SWR	2016 Sanitary Sewer Replacement Projects	c469	E5GA
SWR	2016 Sewerline Overlays	i010	E6CC
SWR	2017 Sanitary Sewer Replacement Project	i013	E6GA
SWR	2017 Sewerline Overlays	i020	E7CC
SWR	2018/19 Sewerline Replacement Project	c492	E6GC
SWR	Citywide CIPP Sewer Rehab Phase I	c456	E4GB
SWR	Citywide CIPP Sewer Rehab Phase II	c488	E6GB
SWR	Lake Ballinger Trunk Sewer Study	s011	E5GB
SWR	Lift Station #1 Basin & Flow Study	c461	E4GC
UTILITIES	Standard Details Updates	s010	E5NA
UTILITIES	Utility Rate Update	s013	E6JA
WTR	2014 Waterline Overlays	c452	E4CC
WTR	2015 Waterline Overlays	c475	E5CB
WTR	2015 Waterline Replacement Program	c440	E4JB
WTR	2016 Water Comp Plan Update	c460	E4JC
WTR	2016 Waterline Overlays	i009	E6CB
WTR	2016 Waterline Replacement Projects	c468	E5JA
WTR	2017 Waterline Overlays	i019	E7CB
WTR	2017 Waterline Replacement Projects	i014	E6JB
WTR	2018/19 Waterline Replacement Project	c493	E6JC
WTR	224th Waterline Relocation (2013)	c418	E3JB
WTR	Dayton St. Utility Replacement Project (3rd Ave to 9th Ave)	c482	E5JB
WTR	Five Corners Reservoir Re-coating	c473	E5KA
WWTP	Sewer Outfall Groundwater Monitoring	c446	E4HA
WWTP	WWTP Outfall Pipe Modifications	c481	E5HA

Attachment: FrequentlyUsedProjNumbers 08-10-17 (Approval of claim checks.)

City Council Agenda Item

Meeting Date: 08/15/2017

Amendments to Downtown Business Improvement District By-Laws

Staff Lead: Patrick Doherty

Department: Economic Development

Preparer: Patrick Doherty

Background/History

After over four years since their inception in 2013, it is appropriate to update the by-laws of the Edmonds Downtown Improvement Direct (dba “Edmonds Downtown Alliance” and hereinafter referred to as “Alliance”) Board that govern the conduct of the Alliance Board, purposes and programs, membership, purchasing, meeting requirements, etc. The purpose of these proposed amendments is to align the code provisions more closely with the evolution of the month-to-month conduct of business over the past four years and to conform more closely with the Edmonds City Code provisions at 3.75, as also proposed to be amended.

Staff Recommendation

Approve on 8/15/17 Council Consent Agenda.

Narrative

Here is a brief summary of the most salient amendments proposed, as approved by the Edmonds Downtown Alliance Board at its July 13, 2017 meeting:

Sec. 3.1 - The names “Edmonds Downtown Alliance,” aka “the Alliance” and “Ed!”, are hereby included. “The Alliance” is used throughout the by-laws.

Sec. 3.2 - Purpose statement is updated slightly to rename “organization” to “administration.”

Sec. 5.2 - Powers and duties of the Secretary are clarified to note that it is his/her duty to ensure that the Alliance administrator (if hired) execute all or part of the responsibility of maintaining meeting minutes, etc.

A subsection (c) is added regarding purchasing that authorizes the President, Treasurer and Alliance Administrator to make purchases pursuant to Board approvals. The President and Treasurer only are allowed to make purchases not to exceed \$250 without prior Board approval so long as such purchases are directly related to Board-approved work plan programs, projects or activities.

Sec. 5.4 - Verbiage is clarified regarding the responsibilities of each working committee with regard to overseeing the programs and projects under its purview, with the default that the Board president, vice president or other designee may provide such oversight in their absence.

Committee names are updated to reflect the latest Council-approved work plan and current practice.

Sec. 8.3 - Clarification is added that governmental entities, public utilities and nonprofits shall be exempt from assessment, yet may voluntarily become Alliance members if they choose to pay the corresponding assessment fees.

Sec. 9.2 - Proposal to send draft minutes of prior meetings to the Board three working days (not five) before a subsequent meeting. Clarification is added that notice of regular Board meetings will follow state statutory provisions.

Sec. 9.3 - Proposal that special meetings may be called only upon request of 3 or more Board members, not just the president, and requiring applicable special-meeting notice per state law and city code provisions.

Council Finance Committee reviewed these proposed by-law amendments at its 8/8/17 meeting and voted to move the proposed amendments to the 8/15/17 Council Consent Agenda for approval.

Attachments:

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PROPOSED AMENDMENTS

BY-LAWS

OF THE

EDMONDS DOWNTOWN BUSINESS IMPROVEMENT DISTRICT MEMBERS ADVISORY BOARD

(Approved by the Alliance Board on July ~~23~~¹³, ~~2013~~²⁰¹⁷)

ARTICLE I

Section 1.1 **Definitions.** As used in these Bylaws, the following terms shall have the following meanings:

- (a) "Board" means the Edmonds Downtown Business Improvement District Members Advisory Board
- (b) "RCW" means the Revised Code of Washington.
- (c) "ECC" means the City of Edmonds Code.

ARTICLE II

PURPOSE OF THE BOARD

Section 2.1 **Advisory Capacity.** The Board shall serve in advisory capacity regarding the uses of Business Improvement District assessments collected under ECC Chapter 3.75.020 and 3.75.040 which shall include recommending annual business plans pursuant to ECC 3.75.120

ARTICLE III

NAME AND PURPOSE OF THE EDMONDS DOWNTOWN BUSINESS IMPROVEMENT DISTRICT

Section 3.1. **Name of Business Improvement District.** The name of the Edmonds Downtown Business Improvement District is ~~the Ed! - Edmonds Downtown Business Improvement District Alliance~~ (the "Alliance" EDBID). ~~The board of the EDBID may decide, by majority vote, to utilize and file a "doing business as" name for business and operations purposes.~~

Section 3.2. **Purpose of Edmonds Downtown Business Improvement District.** The purpose of the ~~EDBID~~ Alliance as an economic unit shall be to encourage, promote, and participate in activities

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enhancing the general economic conditions ~~of the EDBID~~ within the Alliance boundaries by engaging in activities related to the following:

- A. Marketing & Hospitality: may include maps/brochures/kiosks/directories, web site, social media, marketing/advertising campaigns, holiday decorations, street performers/artists, historic education/heritage advocacy, special public events
- B. Safety & Cleanliness: may include maintenance, security, pedestrian environment enhancements
- C. Appearance & Environment: may include design enhancements, neighborhood advocacy & communication, streetscapes/lighting/furniture
- D. Transportation: may include transportation alternatives, directional signage, parking management & mitigation
- E. Business Recruitment & Retention: may include education/seminars, market research, business recruitment
- F. OrganizationAdministration: may include contract staff & professional services, administration costs

ARTICLE IV

ESTABLISHMENT OF EDMONDS DOWNTOWN BUSINESS IMPROVEMENT DISTRICT

Section 4.1. Composition of ~~EDBID~~the Alliance. City of Edmonds Ordinance 3909, adopted on January 15, 2013, added a new Chapter to the Edmonds City Code Titled Chapter 3.75, Business Improvement District, thus establishing the Edmonds Downtown Business Improvement District. The Business Improvement District consists of rate paying members of the business community within a defined ~~EDBIDAlliance~~BID boundary (see Attachment A). Governmental entities, public utilities, nonprofits operating under Internal Revenue Code section 501(c)(3) will not be assessed. ECC 3.75.120 calls for the establishment of a Board to direct the affairs of the ~~EDBID~~Alliance.

ARTICLE V

OFFICERS AND BOARD MEMBERSHIP

Section 5.1. ~~EDBID~~Alliance Board

- a. Composition: The ~~EDBID~~Alliance Board shall consist of seven to eleven ~~EDBID~~Alliance members. To the best possible degree, members of the ~~EDBID~~Alliance Board will be composed of both open door and by appointment ~~EDBID~~Alliance members in rough proportion to the dollar value of assessments to be levied on each classification of businesses. Additionally, ~~EDBID~~Alliance members strive for Board makeup that represents distribution by district location and types of service, retail, and size of business. Per ECC 3.75.120, the City of Edmonds Finance and Community Services/Economic Development Directors, -shall serve as non-voting ex officio members of the Board..

- b. Eligibility: All members, in good standing and having fulfilled the requirements of membership, of the ~~EDBID~~ Alliance shall be eligible to serve on the Board. Each Board member will serve a term of three (3) years. Approximately one-third (1/3) of the authorized number of board members shall be elected each year at an annual meeting of ~~EDBID~~ Alliance members for terms of three (3) years each, from and after election, by a majority vote of ~~EDBID~~ Alliance members in attendance at the annual meeting, or by absentee ballot. After the completion of two (2) consecutive three (3) year terms, one (1) year must elapse before the member is again eligible for re-election to the Board.
- c. Annual Election: Members will be notified of the time and place of the Annual Meeting of Members in an announcement accompanying the solicitation of nominees to the Board for the following year. This notification shall also go to the City of Edmonds. This notification must be sent at least 30 days prior to the Annual Meeting and shall require that all board nominations be received at least 14 days prior to the date of the annual meeting. The membership committee of the ~~EDBID~~ Alliance Board shall compile all nominations received and shall provide nominations, as necessary, in addition to those received from ~~EDBID~~ the Alliance members. A second notice of the Annual Meeting, along with a list of candidates, and an absentee ballot, will be sent to all members at least seven (7) calendar days prior to the Annual Meeting (see Article VIII, Section 8.1). The absentee ballot may be returned no later than the date of the Annual Meeting. An agenda of business to be conducted will also be included. This agenda must include the item of annual election of Board members. The Annual Meeting ~~must~~ shall be held in the month of April, or as determined by the ~~EDBID~~ Alliance Board.
- d. Vacancies: If for any reason a place on the ~~EDBID~~ Alliance Board becomes vacant before a term of membership expires, the vacancy shall be filled by appointment by the president of the ~~EDBID~~ Alliance Board with the majority approval of the ~~EDBID~~ Alliance Board at the time of the appointment.
- e. Attendance at Meetings. Attendance at all meetings is expected of Board members. Any member of the Board anticipating an absence from a scheduled meeting shall notify the President of the Board or Secretary in advance of the meeting.
- f. Removal of Members.
 - (a) If a member of the Board is absent from three (3) consecutive regular meetings of the Board without reasonable cause as determined by the Board, such member may be considered to have tendered his or her resignation to the Members Advisory Board, and may be notified in writing by the Secretary of that fact. The ~~board~~ Board has discretion to waive such resignation for reasons deemed valid.
 - (b) Members of the Board may be removed by the Edmonds City Council for misfeasance or for other reasons pursuant to general removal provisions enacted by the Edmonds City Council for boards and commissions.
- g. Conflicts of Interest.

(a) No Board member shall be beneficially interested, directly or indirectly, in any contract which may be made by, through or under the supervision of the Board, in whole or in part, or which may be made for the benefit of his or her office, or accept, directly or indirectly, any compensation, gratuity or reward in connection with such contract from any other person beneficially interested therein, except as provided by paragraph (b) below.

(b) A Board member is not interested in a contract, within the meaning of paragraph (a) above and RCW 42.23.030, if (i) he or she has only a remote interest in the contract (as that term is defined in RCW 42.23.040), (ii) the extent of his or her interest is disclosed to the Board and noted in the official minutes of the Board prior to the formation of the contract, and (iii) thereafter the Board authorizes, approves or ratifies the contract in good faith by a vote of its membership sufficient for the purpose without counting the vote of the member having the remote interest.

~~(b)~~(c) Alliance Board members shall serve without compensation.

Section 5.2. Officers. The Officers of the ~~EDBID~~Alliance Board shall be a President, Vice President, Secretary, and Treasurer.

- a. Election and Terms of Officers. The officers of the Board shall be elected from among its members at the first regular Annual Meeting following the Annual Meeting of Members held in each calendar year. Officers may be elected to successive terms, provided, however, that no person shall serve as an officer for more than two (2) consecutive terms.
- b. Powers and Duties. The officers of the Board shall have the following duties:
 - a. President: The president shall preside over the Annual Meeting of the ~~EDBID~~Alliance members and all Board meetings. The president shall prepare meeting agendas in consultation with the Secretary or ~~EDBID~~the Alliance administrator (if hired) as deemed appropriate by the president. The president shall appoint members to fill any unexpired term of the ~~EDBID~~Alliance Board as described under Article V, Section 5.1 (d) Vacancies. The president shall be responsible for the overall governance and direction of the ~~EDBID~~Alliance.
 - b. Vice-President: The vice-president shall fulfill all the functions of the president in his/her absence. The vice-president shall ensure that either he/she or the ~~EDBID~~Alliance administrator (if hired) maintains a current membership roster as set forth in Article VII, Section 4 and sends it, semi-annually, to the Program Coordinator of the City's Economic Development Department.
 - c. Secretary: The secretary shall have the responsibility to record and verify all minutes of the Board meetings and prepare and send agendas for board meetings and annual meetings of members in addition to posting Alliance

meeting minutes on the ~~EDBID~~Alliance website or ensure that the Alliance administrator (if hired) execute all or part of this responsibility.

d. Treasurer: The treasurer shall have the responsibility to oversee the finances of the ~~EDBID~~Alliance and provide the Board and City of Edmonds with quarterly financial statements and the ~~EDBID~~Alliance members with an annual financial statement. He/she shall serve as liaison to the City of Edmonds on all matters of mutual financial concern. He/she will work with the ~~EDBID~~Alliance administrator or board contracted agent on all employee, state and federal tax reporting.

c. Purchasing. The President and Treasurer and Alliance Administrator (if hired) are authorized to make purchases pursuant to Board approvals. The President and Treasurer may make purchases not to exceed two hundred fifty dollars (\$250) without prior Board approval so long as such purchases are directly related to Board-approved work plan programs, projects or activities.

Section 5.3. ~~EDBID~~Alliance Officer Nominations. The nomination and election of the officers of the ~~EDBID~~Alliance shall be done at the start of the first Board meeting following the Annual Meeting [see Article VIII, Section 8.2(a)] with the nominating and electing process as follows.

- a. The floor shall be opened to nominations and closed when all nominations have been made. Nominees must be present, or have submitted a letter of intent.
- b. The nominees for each office shall be announced.
- c. An election by secret and written ballot shall be made by those members in attendance at the ~~Alliance~~~~EDBID~~ Board meeting. Each office will be voted on separately.
- d. The ~~EDBID~~Alliance administrator (if hired) shall tally the votes and announce the winners. In the event of a tie, an election will be conducted between the two individuals and the process repeated until a winner is proclaimed. In the event of no ~~EDBID~~Alliance administrator, the ~~board~~ President shall appoint a board member to tally the votes and announce a winner.

+

Section 5.4. Standing Committees. During the third quarter of each year, the ~~EDBID~~Alliance Board shall develop an operational budget and form standing committees. At ~~EDBID~~Alliance Board discretion, committees may be added or removed, but the following requirements will apply:

- (1) Each non-officer Board member will participate on a committee.
- (2) The president may be a non-voting ex-officio member of all committees.
- (3) Committee membership may include member volunteers and others with a tangible interest in the welfare of the ~~EDBID~~Alliance as determined by the Board.

(4) Committee members shall annually, during the ~~3rd~~-third quarter, recommend a schedule of future action and activities for the following year's work plan and budgeting purposes. Once the budget is approved by the Alliance Board and Edmonds City Council, each committee shall oversee the schedule of projects, with the assistance of the Alliance administrator (if hired). In the absence of committee oversight of the schedule of projects, the Alliance Board president, vice president or other designated Board member may provide such oversight.

~~(5) Each committee shall recommend an adequate schedule of projects to the EDBID Alliance Board and administrator. In the event of no EDBID Alliance administrator, schedules will be supervised by the EDBID Alliance Board President, Vice President, or designated Board Member or EDBID Alliance member.~~

~~(6) Notification of meetings will appear on the EDBID website.~~

~~(7) The EDBID Board members shall serve without compensation.~~

~~Once the budget is approved by the EDBID Board and Edmonds City Council, each committee shall oversee the schedule of projects.~~

a. Marketing ~~and Hospitality~~ Committee

The committee shall direct activities toward stimulation of general commerce, promotion and creation of the EDBID Alliance's image and creation of marketing strategies in order to attract targeted groups.

b. Appearance and ~~Capital Improvements~~ Environment (includes beautification and maintenance) Committee

In coordination with other local efforts, the committee shall plan for possible capital, beautification and maintenance projects as appropriate within and extending to the boundaries of the EDBID Alliance. The committee shall recommend an adequate schedule for pedestrian amenities, street areas and EDBID Alliance parking enhancements, signage improvements, and any other beautification or maintenance projects as may be approved by the EDBID Alliance Board. In the event of no EDBID Alliance administrator, the schedule will be supervised by the EDBID Alliance Board President, Vice President, or designated Board Member or EDBID Alliance member.

c. Professional Business ~~Education~~ Resources, Recruitment and Retention Committee

The committee shall research and make recommendations for coordinating business education/training classes/seminars, helping recruit businesses, and market research. The committee may also develop a list of product categories and services currently available in the area. The most desirable mix of products and services

shall be reviewed and recommended by this committee based on market research. The committee shall make recommendations to the ~~EDBID~~Alliance Board for how to best utilize information and research for the good of the ~~EDBID~~Alliance.

d. ~~Membership~~ Communications and Outreach Committee

The committee shall be responsible for overall communications with ~~EDBID~~Alliance members and the nomination process for ~~EDBID~~Alliance Board members. The committee shall be alert during the year to identify those members who have shown an interest and desire to serve on the ~~EDBID~~Alliance Board and its committees, in order to provide assistance to the ~~EDBID~~Alliance in selection of ~~EDBID~~Alliance Board members. This committee will assure that officers carry out the requirements of the Annual Meeting (see Article V, Section 5.1(c) and Article VIII, Section 8.1) and shall be responsible for general communications with members (via mail, email, website or any other form of communication).

ARTICLE VI

PROGRAM MANAGEMENT

Pursuant to ECC.3.75.140, the ~~EDBID~~Alliance Board ~~will~~ may create a separate organization or entity, incorporated with the State of Washington, responsible for the management of ~~EDBID~~Alliance administration and programs. This entity will enter into an agency agreement with the City of Edmonds.

ARTICLE VII

EDMONDS BUSINESS IMPROVEMENT DISTRICT CONSULTANT SERVICES

The ~~EDBID~~Alliance Board may contract for consultant services under the direction of the president with the concurrence of the Board. Such consultants' remuneration and duties may be governed by a contract or employment and description of job duties. For services outside of the purchasing threshold, as identified in the [City of Edmonds](#) purchasing policy adopted by the ~~EDBID~~Alliance Board, a proposal process will be required. All eligible proposers, including ~~BID~~-Alliance members or associates, shall have the option to propose. To make the selection procedure as transparent as possible, the instructions to consultants shall specify the evaluation criteria and the period of validity of submittals. The ~~EDBID~~Alliance Board will review and process all proposals based on pre-determined criteria and overall value (economic and otherwise) to the ~~EDBID~~Alliance.

ARTICLE VIII

MEMBERSHIP

Section 8.1. Membership shall consist of all business owners located within the boundaries of the Edmonds Downtown Business Improvement District as detailed in Attachment A and Ordinance 3909.

Section 8.2. In the case of corporations or partnerships, the business shall designate an individual and his/her alternate to represent it officially. There shall be no duplication or expansion of membership by reason of the internal organization of any member company.

Section 8.3. ~~No stock or shares in the EDBID Alliance will be issued, and the interest of each member in the EBID shall be equal.~~ Membership ~~shall be open to all~~ shall consist of all rate-paying persons, partnerships or business owners who maintain a place of business within the boundaries of the ~~EDBID Alliance~~. Governmental entities, public utilities, nonprofits operating under Internal Revenue Code section 501(c)(3) shall ~~not be assessed~~ be exempt from assessment and membership. However, voluntary payment of the ~~appropriate~~ corresponding assessment amounts per ECC3.75.040 by an exempt entity, shall constitute membership in the ~~EDBID Alliance~~.

Section 8.4. The ~~EDBID Alliance~~ vice president, with the assistance of the Alliance administrator (if hired) shall be responsible for the preparation of a membership list setting forth the names and addresses of members and the official representative and alternate representative of each. A representative once designated shall be conclusively presumed to continue in the capacity until the receipt of written notice, from an officer of the member firm, naming a replacement.

Section 8.5. No stock or shares in the Alliance will be issued.

ARTICLE IX

MEETINGS OF THE EDMONDS DOWNTOWN BUSINESS IMPROVEMENT DISTRICT

Section 9.1. Annual Meeting of Members. The Annual Meeting of Members of the ~~EDBID Alliance~~ shall be held in the month of April for the purpose of electing members of the ~~EDBID Alliance~~ Board of Directors. Other business shall include any appropriate items. Notice of the Annual Meeting shall accompany the solicitation of nominees sent at least thirty (30) days prior to the Annual Meeting. It is the responsibility of members to notify the ~~EDBID Alliance~~ and City of Edmonds of any change of address or ownership.

Section 9.2. Meetings of ~~EDBID Alliance~~ Board.

- a. At the next regularly scheduled ~~EDBID Alliance~~ Board meeting following the Annual Meeting of Members, the Board will elect the officers of the ~~EDBID Alliance~~ Board. (See Article V, Section 5.3)
- b. The ~~EDBID Alliance~~ Board shall meet no less frequently than once a quarter, on the second Thursday of the first month of each quarter at 8:00 am, except that no meeting is required in December. Draft minutes of the previous ~~EDBID Alliance~~ Board meeting and any

communications from standing committees will be delivered to each member of the ~~EDBID~~Alliance Board ~~five-three (53)~~ days prior to the meeting. Notice of the regular Board meetings will be provided as required by state statutory provisions.

- c. The ~~EDBID~~Alliance's secretary, or designee, shall keep accurate minutes of the proceedings and decisions of the ~~EDBID~~Alliance Board meetings. The ~~EDBID~~Alliance's secretary shall ~~verifies~~ the Board minutes prior to ~~them being delivered~~ delivery to the ~~EDBID~~Alliance Board prior to the following meeting, in addition to posting Board-approved Alliance~~EDBID~~ meeting minutes on the ~~EDBID~~-Alliance website.
- d. Any member may attend any meeting of the ~~EDBID~~Alliance Board and this policy shall be posted on the ~~EDBID~~Alliance website. Upon request, a member may speak to an item before the ~~EDBID~~Alliance Board for a period of time as determined by the Board. Upon request, a member may raise issues for discussion by the Board, but notification to Board members must precede the discussion. Non-Board member Alliance members may not vote at Board Meetings.
- e. For the purposes of conducting business, a majority gathering of ~~EDBID~~Alliance Board Members will be considered a quorum of the ~~EDBID~~Alliance.
- f. A majority of those voting, if quorum is present, shall constitute a deciding vote by the ~~EDBID~~Alliance.
- g. The president ~~or designee~~ may declare emergency Board meetings and waive required notice pursuant to RCW 42.30.070; ~~however, an effort must be made to meet all requirements if possible.~~
- ~~h. Three (3) or more EDBID Alliance Board members can set special meetings of the EDBID Alliance Board, provided all requirements of notification are met.~~

Section 9.3. Special Meetings. Special meetings of the ~~EDBID~~Alliance may be held any time upon the request of three (3) or more Alliance Board members ~~the president~~ who may desire to call such a meeting, providing that these ~~president Board members~~ first ~~notifies~~ notify the ~~secretary~~ community services/economic development director, who shall ~~send~~ provide notice according to the public notice requirements for special meetings set out by state statutory and city code provisions. ~~to each EDBID member at least ten (10) days prior to the time fixed for such special meeting.~~

ARTICLE X

SEAL

The ~~EDBID~~Alliance shall have no seal until such a time as the ~~EDBID~~Alliance Board and president of the ~~EDBID~~Alliance may adopt one as part of an appropriate resolution.

ARTICLE XI

AMENDMENT OF BY-LAWS

These by-laws may be altered, supplemented, amended or repealed at any regular or special meeting of the ~~EDBD~~Alliance Board as designated in board quorum, Article VIII, section 2 (f), provided notice of the proposed change has been mailed to all members at least ten (10) days prior to the meeting at which such proposed change is to be considered.

ARTICLE XII

ASSESSMENT

No member of the ~~EDBD~~Alliance shall be personally liable for the debts or liabilities of the ~~EDBD~~Alliance, except to the extent of any unpaid portion of dues or assessments or signed contracts with the ~~EDBD~~Alliance. Dues or assessments shall be established per City of Edmonds Ordinance 3909. It is expressly provided that, without limiting the generality of this provision, no assessment, charge or levy shall ever be made by any receiver, trustee in bankruptcy, assignee for the benefit of creditors, court or judgment creditor.

ARTICLE XIII

PROCEDURE

The rules contained in the current edition of *Robert's Rules of Order Newly Revised*, and more specifically, the modified rules for small boards and committees, shall govern the ~~EDBD~~Alliance in all cases to which they are applicable and in which they are not inconsistent with these bylaws and any special rules of order the ~~EDBD~~Alliance may adopt.

City Council Agenda Item

Meeting Date: 08/15/2017

Resolution to Designate Highway 99 Subarea as Residential Targeted Area (10 min)

Staff Lead: Patrick Doherty

Department: Economic Development

Preparer: Patrick Doherty

Background/History

In order to implement the Multi-Family Tax Exemption Program (MFTE) in the Highway 99 Subarea as a means of encouraging redevelopment that contains market-rate and affordable-housing units, the City Council approved a Resolution of Intent on July 18, 2017 to designate the Highway 99 Subarea as a residential targeted area, pursuant to RCW 84.14.

The Resolution of Intent gave notice of the time and place of a hearing to be held by the governing authority to consider the designation of the residential targeted area as 7:00 pm, August 15, 2017 in Edmonds City Council Chambers.

In accordance with RCW 84.14.040(2), publication of the notice of the August 15, 2017 public hearing was provided in a newspaper of general circulation for at least two consecutive weeks, no less than 7 and no more than 30 days prior to the hearing, on July 30, 2017 and August 6, 2017.

Staff Recommendation

Upon conclusion of the Public Hearing at the 8/15/17 Council Regular Meeting, consider for approval.

Narrative

Upon conclusion of the scheduled public hearing at the regular Council meeting of August 15, 2017, the City Council may adopt the attached Resolution to designate the Highway 99 Subarea as a residential targeted area for purposes of implementing the MFTE in the Highway 99 Subarea.

The MFTE is a tool intended to encourage development in the Subarea of projects that include market-rate and affordable housing units.

The accompanying proposed minor code amendments to ECC Chapter 3.38 will serve to implement the MFTE program in the newly designated residential targeted area.

At its 8/8/17 meeting, the Parks, Planning and Public Works Committee voted to forward this item to the 8/15/17 Council Regular meeting for potential action after the Public Hearing.

Attachments:

DRAFT RESO to DESIGNATE MFTE - HWY 99

RESOLUTION NO. _____

A RESOLUTION OF THE CITY OF EDMONDS, WASHINGTON, TO DESIGNATE THE HIGHWAY 99 SUBAREA AS A RESIDENTIAL TARGETED AREA FOR THE PURPOSE OF ESTABLISHING A MULTIFAMILY TAX EXEMPTION PROGRAM

WHEREAS Chapter 84.14 of the Revised Code of Washington provides for temporary special valuations for eligible improvements associated with multifamily housing, especially within urban centers deficient in planned residential development; and

WHEREAS, the Edmonds City Council adopted a new Subarea Plan and related zoning code amendments for the Highway 99 Corridor on August 15, 2017, designating the Corridor as an urban center whose purpose includes fostering development of a full range of commercial and residential uses, including market-rate and affordable housing in stand-alone and mixed-use developments; and

WHEREAS, multifamily residential development is lagging in the Highway 99 Subarea, while commercial development there and residential development elsewhere in the City is occurring; and

WHEREAS, implementation of the Multi-Family Tax Exemption program, authorized by 84.14 RCW, may provide sufficient incentive to encourage residential development in the Highway 99 Subarea; and

WHEREAS, in order to establish a Multifamily Tax Exemption Program, the City must designate one or more residential targeted areas within which the temporary special valuation may be granted to qualifying residential improvements; and

WHEREAS, RCW 84.14.040(2) states that a governing authority may adopt a resolution of intent to designate one or more residential targeted areas; and

WHEREAS, in accordance with the requirements stipulated at RCW 84.14.040(2), the Edmonds City Council passed a Resolution of Intent on July 18, 2017 to designate the Highway 99 Subarea as a residential targeted area; and

WHEREAS, in accordance with RCW 84.14.040(2), the Resolution of Intent gave notice of the time and place of a hearing to be held by the governing authority to consider the designation of the residential targeted area as 7:000 pm, August 15, 2017 in Edmonds City Council Chambers; and

WHEREAS, in accordance with RCW 84.14.040(2), publication of the notice of the August 15, 2017 public hearing was provided in a newspaper of general circulation for at least two consecutive weeks, no less than 7 and no more than 30 days prior to the hearing, on July 30, 2017 and August 6, 2017; and

WHEREAS, the hearing on the proposed residential targeted area was held as scheduled on August 15, 2017;

NOW, THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF EDMONDS, WASHINGTON AS FOLLOWS:

Section 1. The City Council of the City of Edmonds finds as follows:

- (a) The Highway 99 Subarea, as identified in the Council-approved Highway 99 Subarea Plan, constitutes an urban center;
- (b) The Highway 99 Subarea lacks sufficient available, desirable, and convenient residential housing, including affordable housing, to meet the needs of the public who would be likely to live in the Highway 99 Subarea, if the affordable, desirable, attractive, and livable places to live were available; and
- (c) The designation of the Highway 99 Subarea as a residential targeted area will stimulate the construction of new multifamily housing, including affordable housing.

Section 2. In light of the above findings, the City Council of the City of Edmonds designates the Highway 99 Subarea, as identified in the Highway 99 Subarea Plan approved by City Council on August 15, 2017, as a residential targeted area for the purposes of establishing a Multifamily Tax Exemption Program.

RESOLVED this 15th day of August 2017.

APPROVED:

DAVID O. EARLING, MAYOR

ATTEST/AUTHENTICATED:

CITY CLERK, SCOTT PASSEY

FILED WITH THE CITY CLERK:
PASSED BY THE CITY COUNCIL:
RESOLUTION NO.

City Council Agenda Item

Meeting Date: 08/15/2017

Proposed Resolution supporting merger of SNOCOM and SNOPAC (30 min.)

Staff Lead: Al Compaan and Tom Mesaros

Department: City Clerk's Office

Preparer: Scott Passey

Background/History

A task force has been working over the past 7 months discussing the potential merger of the two PSAPs. This presentation is designed to bring the Council up to date on those discussions and also to discuss a potential resolution for the council to consider. The resolution is not an approval of the merger, but it would voice support that the merger is important for improved public safety. Both Kurt Mills, executive director of SNOPAC, and Terry Peterson, executive director of SNOCOM, will be presenting to the full council.

Staff Recommendation

This is for council discussion in anticipation of a resolution calling for the support of the proposed merger.

Narrative

Opportunity to voice support for the proposed merger as the boards of SNOCOM and SNOPAC finalize the merger agreement for member cities, the county and the county's various public safety entities to consider.

Attachments:

Resolution to Support Consolidation of PSAPs 8.10.17

Resolution to Support Consolidation of PSAPs (Unanimous) 8.10.17

JTF Update for Stakeholders 20170809 - Edmonds

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF EDMONDS, WASHINGTON, TO SUPPORT THE CONSOLIDATION OF SNOAC AND SNOCOM PUBLIC SAFETY ANSWERING POINTS (PSAPs).

WHEREAS, SNOCOM and SNOAC are separate PSAP agencies created by interlocal agreement which provide 911-call answering, fire/police and emergency medical dispatch, and ancillary services, including administration of public safety communication systems in Snohomish County; and

WHEREAS, the service area boundaries of SNOCOM and SNOAC do not coincide with the jurisdictional boundaries of local agencies served by SNOCOM and SNOAC, and also result in a Jointly Served Area (JSA) within Snohomish County where more than 40,000 911 calls are transferred from the PSAP to the other each year, with the potential to delay or complicate emergency response; and

WHEREAS, continued operation of SNOCOM and SNOAC as separate entities would preclude elimination of the JSA, and forestall opportunities for operational efficiencies and savings; and

WHEREAS, following objective analysis by an expert consultant, and after extensive and inclusive examination and discourse by the SNOCOM Board of Directors, SNOAC Board of Directors, the Joint Task Force created by SNOCOM and SNOAC, the Joint SNOCOM-SNOAC Board Meetings, and the staff thereof, it has become evident that consolidation of the two PSAPs offers new opportunities to advance the health, safety, and welfare of the public and first responders, which is the paramount purpose of the government agencies involved; and

WHEREAS, the examination referenced above also confirmed that consolidating the two PSAPs into one entity will allow for new economies of scale and a potential cost savings of \$1.1 million annually, as well as support future opportunities for cost avoidance; and

WHEREAS, the combined SNOCOM and SNOAC boards have passed resolutions in favor of proposals put forth by the Joint Task Force, and the Edmonds City Council supports those proposals and the desire to have a consolidation effective January 1, 2018; and

WHEREAS, the primary goal of consolidation is to eliminate call transfers and any delay to the public when calling 911 in Snohomish County; and

WHEREAS, after careful consideration of the implications of consolidating SNOCOM and SNOPAC, the Edmonds City Council wishes to express its support for the consolidation of SNOCOM and SNOPAC, **NOW THEREFORE**,

THE CITY COUNCIL OF THE CITY OF EDMONDS DOES RESOLVE AS FOLLOWS:

Section 1. The Edmonds City Council proclaims its support for the consolidation of SNOCOM and SNOPAC, and its intent to ratify the interlocal agreements needed to achieve this consolidation.

Section 2. The Edmonds City Council encourages each of the agencies served by SNOCOM or SNOPAC to actively support the consolidation of SNOCOM and SNOPAC.

RESOLVED this ____ day of _____, 2017.

APPROVED:

MAYOR, DAVID O. EARLING

ATTEST/AUTHENTICATED:

CITY CLERK, SCOTT PASSEY

FILED WITH THE CITY CLERK: _____

PASSED BY THE CITY COUNCIL: _____

RESOLUTION NO. _____

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF EDMONDS, WASHINGTON, TO SUPPORT THE CONSOLIDATION OF SNOAC AND SNOCOM PUBLIC SAFETY ANSWERING POINTS (PSAPs).

WHEREAS, SNOCOM and SNOAC are separate PSAP agencies created by interlocal agreement which provide 911-call answering, fire/police and emergency medical dispatch, and ancillary services, including administration of public safety communication systems in Snohomish County; and

WHEREAS, the service area boundaries of SNOCOM and SNOAC do not coincide with the jurisdictional boundaries of local agencies served by SNOCOM and SNOAC, and also result in a Jointly Served Area (JSA) within Snohomish County where more than 40,000 911 calls are transferred from the PSAP to the other each year, with the potential to delay or complicate emergency response; and

WHEREAS, continued operation of SNOCOM and SNOAC as separate entities would preclude elimination of the JSA, and forestall opportunities for operational efficiencies and savings; and

WHEREAS, following objective analysis by an expert consultant, and after extensive and inclusive examination and discourse by the SNOCOM Board of Directors, SNOAC Board of Directors, the Joint Task Force created by SNOCOM and SNOAC, the Joint SNOCOM-SNOAC Board Meetings, and the staff thereof, it has become evident that consolidation of the two PSAPs offers new opportunities to advance the health, safety, and welfare of the public and first responders, which is the paramount purpose of the government agencies involved; and

WHEREAS, the examination referenced above also confirmed that consolidating the two PSAPs into one entity will allow for new economies of scale and a potential cost savings of \$1.1 million annually, as well as support future opportunities for cost avoidance; and

WHEREAS, the combined SNOCOM and SNOAC boards have passed resolutions in favor of proposals put forth by the Joint Task Force, and the Edmonds City Council supports those proposals and the desire to have a consolidation effective January 1, 2018; and

WHEREAS, the primary goal of consolidation is to eliminate call transfers and any delay to the public when calling 911 in Snohomish County; and

WHEREAS, after careful consideration of the implications of consolidating SNOCOM and SNOPAC, the Edmonds City Council wishes to express its support for the consolidation of SNOCOM and SNOPAC, **NOW THEREFORE**,

THE CITY COUNCIL OF THE CITY OF EDMONDS DOES RESOLVE AS FOLLOWS:

Section 1. By unanimous vote, the Edmonds City Council proclaims its support for the consolidation of SNOCOM and SNOPAC, and its intent to ratify the interlocal agreements needed to achieve this consolidation.

Section 2. By unanimous vote, the Edmonds City Council encourages each of the agencies served by SNOCOM or SNOPAC to actively support the consolidation of SNOCOM and SNOPAC.

RESOLVED this ____ day of _____, 2017.

APPROVED:

MAYOR, DAVID O. EARLING

ATTEST/AUTHENTICATED:

CITY CLERK, SCOTT PASSEY

FILED WITH THE CITY CLERK: _____

PASSED BY THE CITY COUNCIL: _____

RESOLUTION NO. _____

9-1-1 Consolidation Presentation for Edmonds City Council

PREPARED BY: KURT MILLS, TERRY PETERSON
AUGUST 2017

Background

Three separate entities:

SNOPAC PSAP – Formed in the 70's, located in Everett, a consolidated 9-1-1 Center serving roughly 70% of Snohomish County's population.

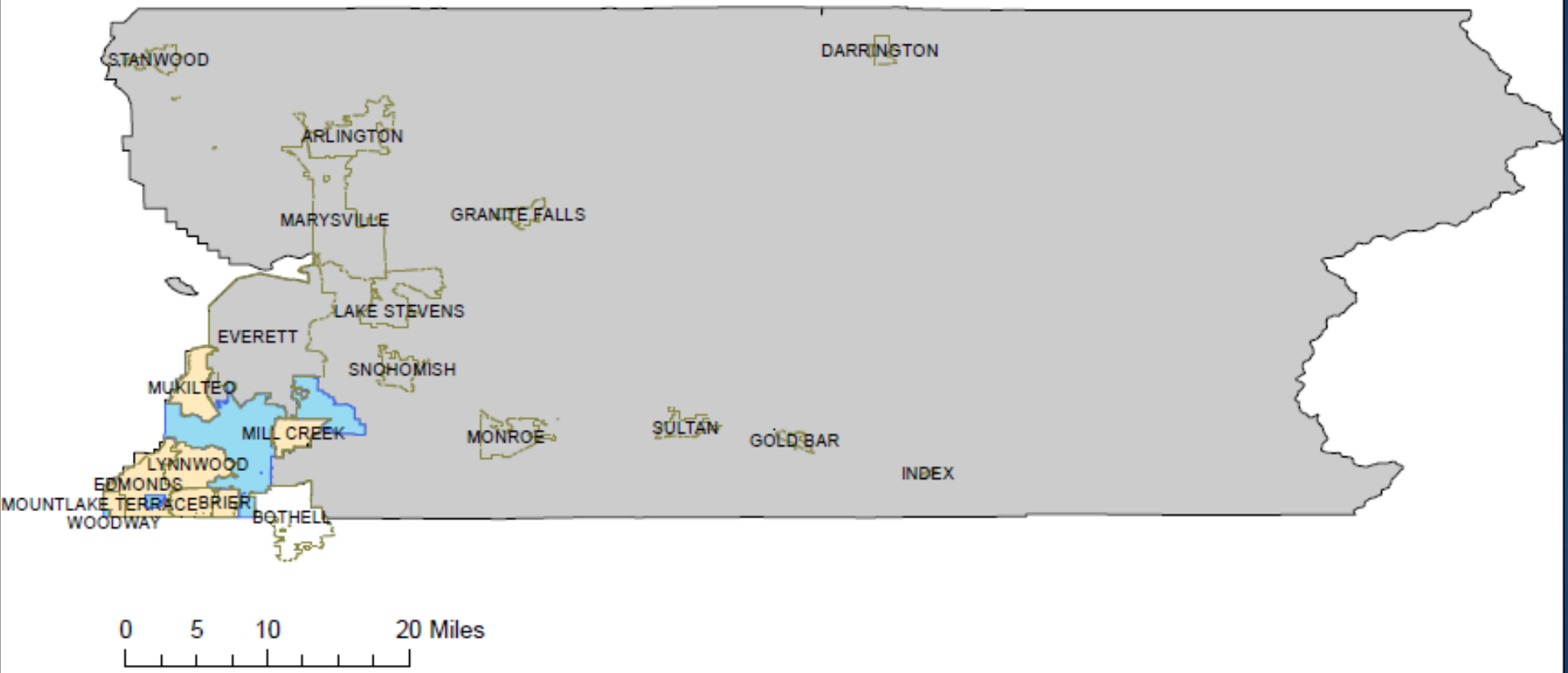
SNOCOM PSAP – Formed in the 70's, located in Mountlake Terrace, consolidated 9-1-1 Center serving roughly 30% of Snohomish County's population.

SERS – Formed in the 90's, Emergency **Radio** System, group responsible for the voice radio system used by police, fire and 911 countywide.

Legend

PSAP

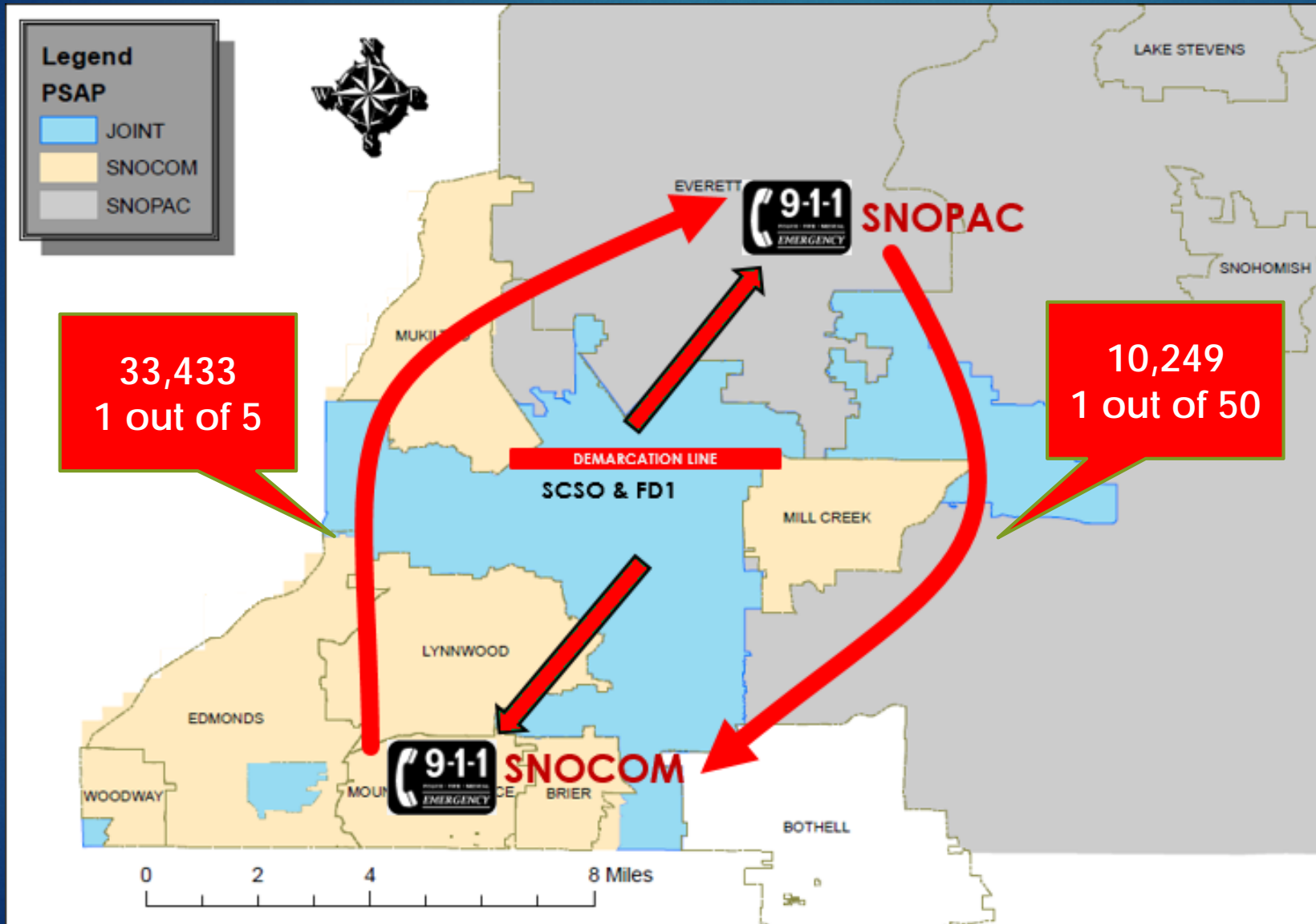
- JOINT
- SNOCOM
- SNOPAC



Background

- A serious service deficiency exists within a Jointly Served Area (**JSA**) covered by **FD1/SNOCOM** and **SCSO/SNOPAC**
 - Excessive 9-1-1 transfers resulting in delays to emergencies
 - Includes an imbalance of funding in relation to workload
- County E911 funded a three-part study completed in March 2015 which concluded “...*significant opportunity for efficiency and effectiveness gains through consolidation...*” and would address the existing 9-1-1 transfers issue. It included several other options including virtual consolidation and co-location
- A Snohomish County Joint Task Force (JTF) was formed to further consider these options

Why are we looking at consolidation?



- ▶ 2016: 44K Transfers
 - ▶ 66% within the JSA
 - ▶ 34% along borders
- ▶ Over 11 days of hold time a year
- ▶ Significant life/safety issue for the public and for police and fire service providers

Joint Task Force (JTF)

- Includes Operational and Board Representatives from SNOCOM, SNOPAC & SERS and an independent facilitator with consolidation experience
 - Formed in early 2016 at direction of SNOCOM & SNOPAC Boards
- Multiple process check-points through Joint (SNOCOM/SNOPAC) Board Meetings to validate process and direction
- SERS is ex-officio until PSAPs make a decision about consolidation
- Open transparent process with regular updates to stakeholders including sharing documents online

Primary Decision Points

- **Service Levels:** Goal of eliminating SNOPAC/SNOCOM Transfers.
- **Costs:** Consider opportunities for cost savings. Labor savings over time through attrition.
- **Resiliency/Redundancy:** Current two PSAP model offers important resilience and goal not to lose this.
- **Governance:** Structure which ensures a fair voice for diverse group including police/fire, large/medium/small agency

Multiple Solutions Evaluated

- Evaluated a number of options focused on service levels and resolve SNOPAC/SNOCOM 9-1-1 Transfer problem
 - Includes two other options that attempt to address transfers through routing and procedural changes
 - Concluded that both options have significant operational and fiscal impacts to the PSAPs

Status of Primary Decision Points

Excessive 9-1-1 Transfer

Annual 9-1-1 transfers of 45,000-50,000 is a serious problem

- SNOCOM/SNOPAC Transfers are completely eliminated through consolidation
- Maintain or improve service levels and eliminate other operational inefficiencies present in a two-PSAP setting

Cost

Both Boards sought ways to decrease costs

- Projected savings of just under \$1MM annually
- Labor savings recognized over time through attrition (estimated within 2 years)

Resiliency/Redundancy

Current two-PSAP environment provides good resiliency

- Maintain existing fully-equipped SNOCOM facility as warm backup (secondary location)
 - Prepaid rent through 2023

Governance

- Consensus reached on board make-up and general set of rules.

Estimated One-Time Transition Costs

	Low Estimate	High Estimate
Office space Renovation	\$ 500,000	\$ 750,000
Organizational/Team Building/Reorganization	\$ 30,000	\$ 50,000
Phone system (reconfigure as Multi-Node)	\$ 30,000	\$ 50,000
Contract Negotiations	\$ 90,000	\$ 120,000
Recorder Modifications	\$ 5,000	\$ 10,000
New ILA Development	\$ 15,000	\$ 30,000
Corporate Structure Legal Review	\$ 4,000	\$ 6,000
Technology Transition (reconfiguration)	\$ 30,000	\$ 125,000
Contingency	\$ 50,000	\$ 100,000
Radio Console Reorganization (MCC7500)	\$ 15,000	\$ 20,000
Parking Needs at SNOPAC	\$ 50,000	\$ 250,000
Total	\$ 819,000	\$ 1,511,000

* Matrix Consulting Study estimated transition costs at between \$229,500 & \$372,500

Governance

- ▶ Consensus on majority of governance issues
 - ▶ The County and all local governments which are police or fire service providers, or which contract for service, can join.
 - ▶ **15 member board**
 - ▶ 10 Police Seats
 - ▶ 5 Fire seats
 - ▶ Board members selected by caucuses of large, medium & small agencies
 - ▶ **Plus 1 non-voting** board member selected by contract agencies
 - ▶ Elected and operational staff serve on board
 - ▶ Supermajority vote on key decisions: 70% and at least 1 fire vote
 - ▶ 1 year of “rate smoothing” for those significantly impacted by new assessment formula

Supermajority Vote Items

- ▶ Adding a new member
- ▶ Terminating a member
- ▶ Amending the ILA (unless unanimous consent required by law)
- ▶ Amendments to bylaws
- ▶ Approving the budget if it exceeds last year's budget by a rate over CPI-U + 4%
- ▶ Capital expenditures exceeding \$500,000
- ▶ Dissolution or merger/consolidation of the agency
- ▶ Hiring the Executive Director of the Agency.

Rate Smoothing

- ▶ New assessment formula projected to provide savings for most.
- ▶ One agency expected to see 30+% increase
- ▶ Rate smoothing used to reduce the “pain” associated with change of assessment formula
 - ▶ Those agencies seeing 9% or greater savings will contribute up to 30% of that savings in 2019 to those agencies seeing 9% or great cost increase.

Next Steps in Process

- ▶ Detailed financial reviews (completed)
- ▶ May thru July – Individual agency briefings (mostly completed)
- ▶ Over the summer – Work on new Interlocal Agreement, transition budget, timeline
- ▶ Sept – Joint Board review and advisory position on new ILA
- ▶ Oct thru Dec – Depending on Joint Board Action, Individual agencies re-briefed and have decision whether to sign on to new ILA
- ▶ New Agency could start work as early as **Jan 2018**, operating out of two physical locations (SNOPAC & SNOCOM's existing facilities)
- ▶ **Jan 2019** merging of operations into single facility with existing SNOCOM facility transitioning into a warm backup.

Questions? Comments?

Information and regular updates available:

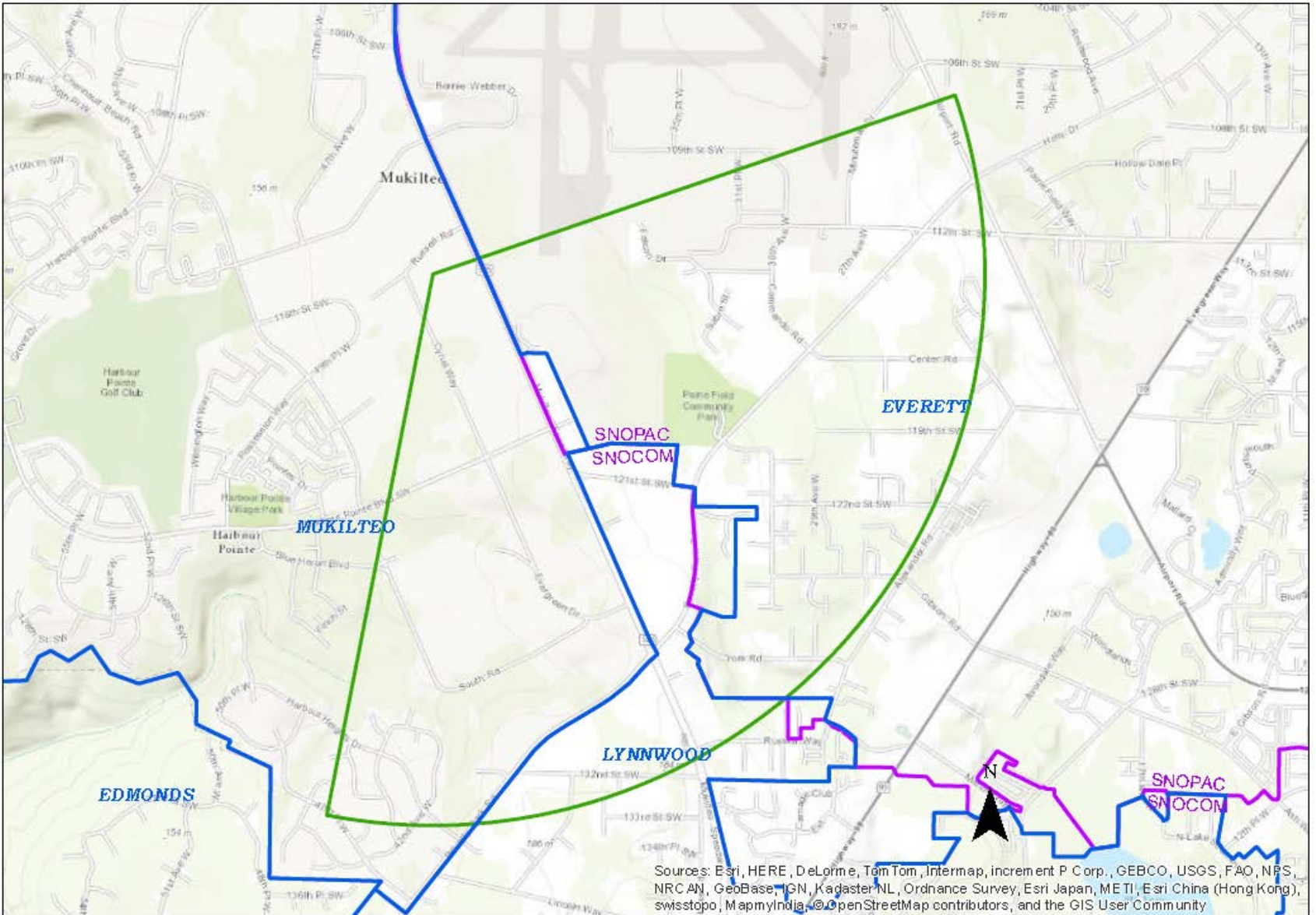
http://www.snopac911.us/Consolidation_Discussion.html

<http://www.snocom.org/public-records-request/snocom-snopac-consolidation-documents>

Reference Slides

Will the call transfer issue go away with Next-Gen 911 Technology?

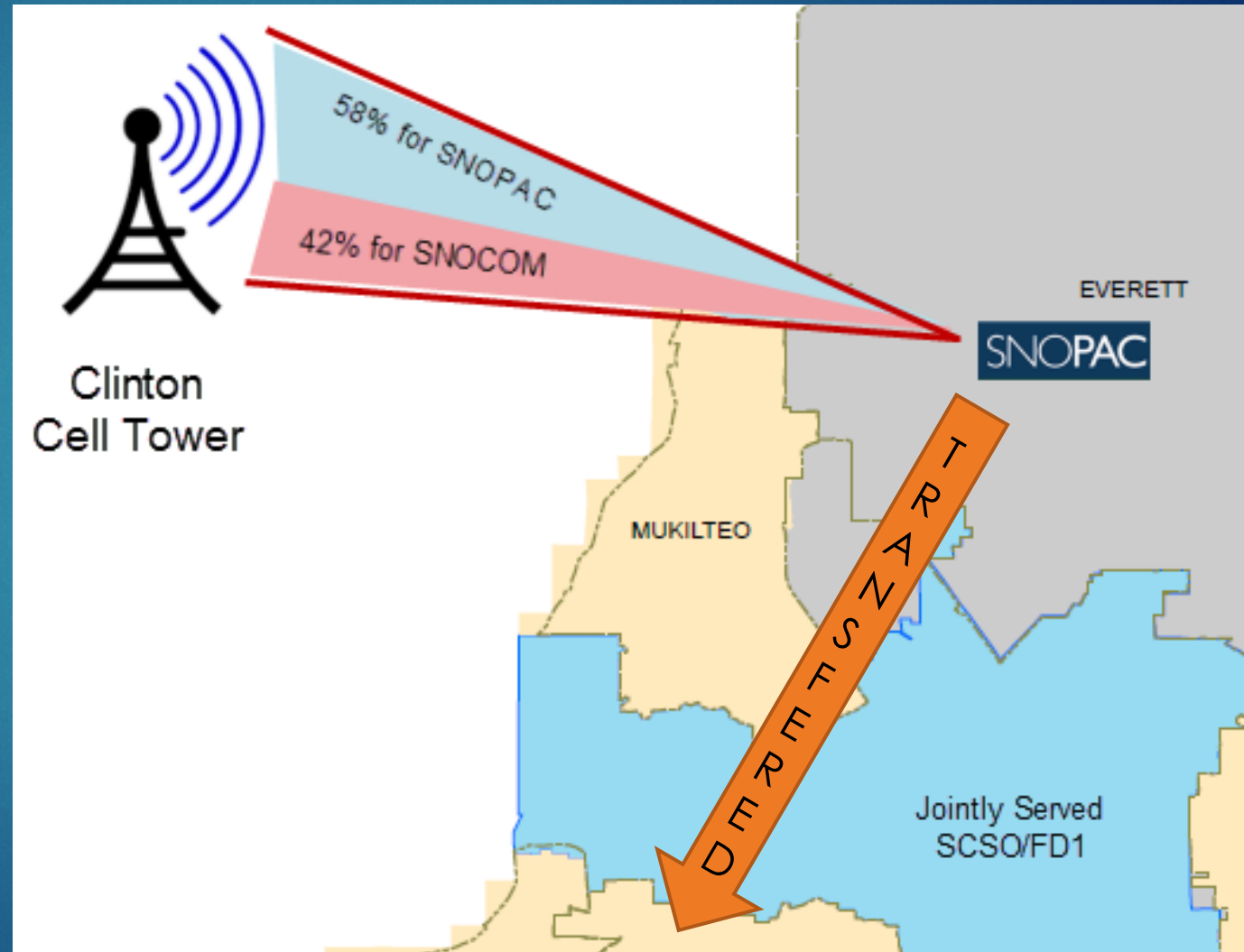
- ▶ Next Gen 911 **Will have NO impact** on 911 call transfers in the SCSO/FD1 Jointly Served Area
 - ▶ Next-gen technologies cannot discern a Police call from a Fire/EMS call and automatically route the call based on the needs of the caller
- ▶ Next Gen 911 **WILL** reduce some 911 call transfers along border areas
 - ▶ Requires new ESI network currently underway
 - ▶ Requires carrier adoption (not mandated)
 - ▶ Requires device compatibility, some legacy devices not supported



SNOCO Unique ID	Original PSAP	Recom. PSAP	Call Count	SNOCOM	SNOPAC Calls	818	SNOCOM Calls	1155
SNO-3726	SNOCOM	SNOPAC	Coverage Area	SNOPAC	SNOPAC Area	24964838	SNOCOM Area	19924133
			Population	SNOPAC	SNOPAC Pop.	619	SNOCOM Pop.	376

CPP Example: Mukilteo Shooting

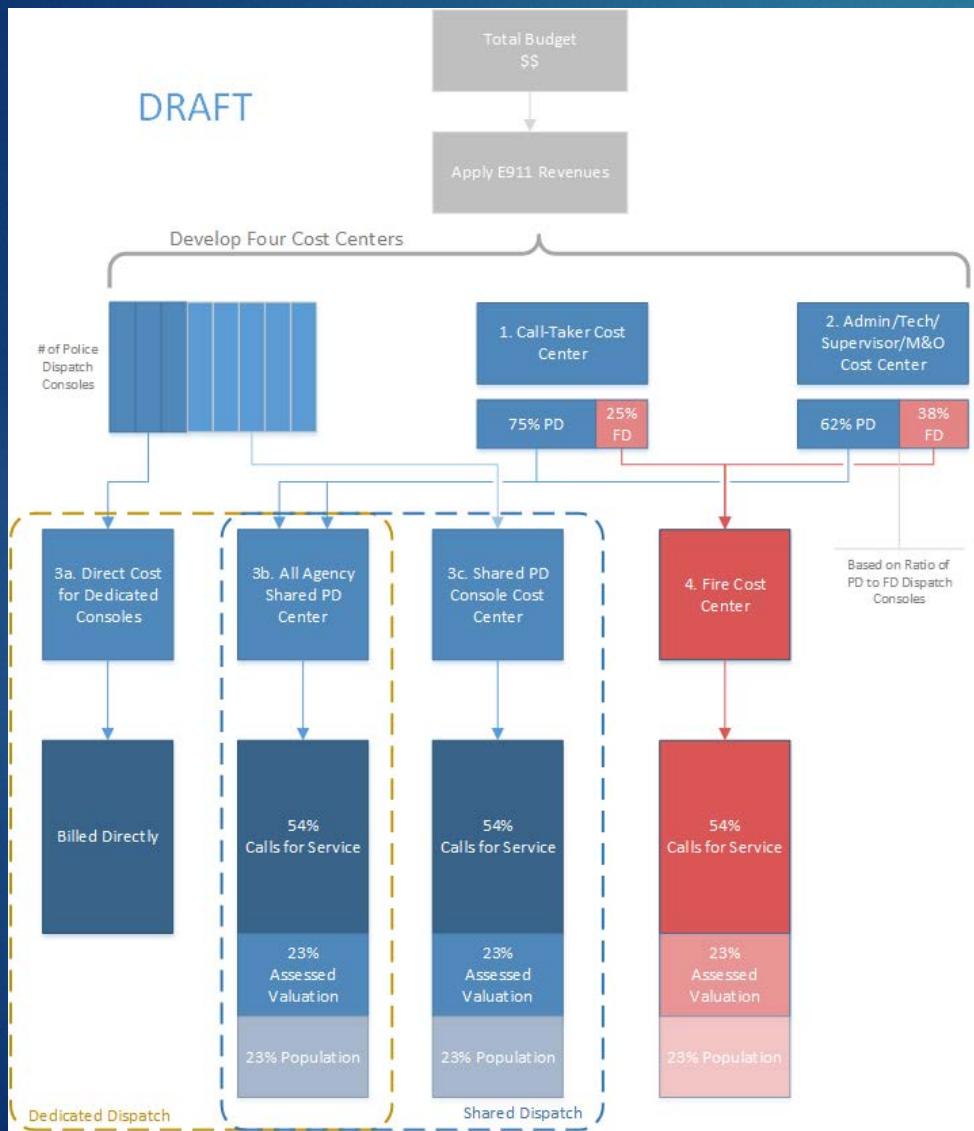
- ▶ Parts of Everett & Mukilteo served by a cell tower in Clinton (across the water)
- ▶ All calls on that tower route to SNOPAC
 - ▶ Based on our analysis 58% of those calls are for SNOPAC
 - ▶ There are other cell towers which serve both SNOPAC & SNOCOM.



Proposed Assessment Formula

- ▶ Hybrid of SNOCOM & SNOPAC's existing formulas
- ▶ Uses SNOPAC's cost-center approach to assign costs appropriately (Police/Fire, Shared/Dedicated Dispatch)
- ▶ Uses SNOCOM's ratio of Calls for Service, Population and Assessed Value
- ▶ Adheres to assessment framework principles approved in January

Assessment Formula Walk-Thru



Please refer to Handouts

Consolidated Cost Centers

Cost Center	FTEs	Sub Total	Revenues	Assessment Total
Police	45.5	\$ 4,644,012	\$ 53,539	\$ 4,590,473
Fire	28.5	\$ 2,902,507	\$ 12,810	\$ 2,889,697
Call-Taking	22.0	\$ 2,243,560	\$ 2,243,560	\$ -
Supervision	16.0	\$ 2,034,688	\$ -	\$ 2,034,688
Admin/Tech	32.0	\$ 4,609,745	\$ -	\$ 4,609,745
M&O	N/A	\$ 2,902,016	\$ 3,013,393	\$ (111,376)
Total	144.0	\$ 19,336,529	\$ 5,323,302	\$ 14,013,227

Step 1. Determine total budget and apply E911 Revenue

Step 2. Determine Per Console/Type Cost

Console Type	Consoles #	Consoles %	Console \$	Total
Police	8	62%	\$ 573,809	\$ 4,590,473
Fire	5	38%	\$ 577,939	\$ 2,889,697

Step 3. Determine Shared PD/FD, Dedicated Cost

Console Assignment	Consoles #	Console \$
Everett	2	\$ 1,147,618
Marysville	1	\$ 573,809
Shared Police	5	\$ 2,869,046
Shared Fire	5	\$ 2,889,697

Step 4. Determine PD/FD Split of Admin/IT/Supervisor/M&O Cost Center

Cost Center	Consoles %	Ad/IT/Sup/M&O \$
Police	62%	\$ 4,020,343
Fire	38%	\$ 2,512,714
Total		\$ 6,533,057

Step 5a. Determine **Dedicated Console Assessment = (Dedicated Console \$) + (PD Share of Ad/It/Sup/M&O \$)**

Step 5b. Determine **Shared PD Console Assessment = (Shared PD Console \$) + (PD Share of Ad/It/Sup/M&O \$)**

Step 5c. Determine **Shared FD Console Assessment = (Shared FD Console \$) + (FD Share of Ad/It/Sup/M&O \$)**

Step 6. Apply **Individual Agency Percentage based on CFS, AV, and Population.**

Option 4 – Attrition & “Right Size”

- ▶ Based on historical attrition trends we can expect call taker and dispatch staff will be “right-sized” within the first year
- ▶ Turnover of supervisory staff is less frequent and reasonable to expect right-sizing within two years

Pro-Forma Options Requested

- ▶ Option 1 – Status Quo
- ▶ Option 2A – Reroute JSA calls to SNOPAC
- ▶ Option 2B – Reroute JSA calls to SNOCOM
- ▶ Option 3 – Aggressive Cross PSAP Call Entry
- ▶ Option 4A & 4B – Consolidated Agency
 - ▶ Option 4A – one facility (SNOPAC's current facility)
 - ▶ Option 4B – maintaining SNOCOM's facility as a "warm back-up"

Fiscal

- ▶ 10-Yr Pro Forma Budgets were developed
 - ▶ Compare status quo (two separate PSAPs) against several alternative models including consolidation
 - ▶ Stand-alone options resulted in operational and fiscal impacts
 - ▶ Consolidated Option resulted in annual cost reductions of \$1M+ with equal or improved staffing and service levels

City Council Agenda Item

Meeting Date: 08/15/2017

Adoption of Subarea Plan Zone Map Amendments, Development Regulations, and Planned Action for Highway 99 area (30 min.)

Staff Lead: Shane Hope

Department: Planning Division
Preparer: Diane Cunningham

Background/History

Revitalizing the Highway 99 area has been a goal of the City Council and the public for years. Over time, various studies were prepared and several transportation safety projects were planned. However, a clear vision and a broad-reaching strategy for the future had not been articulated. In early 2016, based on a budget allocation approved by the City Council, work began on a new Highway 99 Subarea Plan, planned action EIS, and implementing development regulations.

Significant public information and input were provided. (An overview of the public process has been noted in previous reports to the Council.)

The first major product being considered by the City Council is the Draft Highway 99 Subarea Plan. *(See Attachment 1 for the ordinance that contains the Subarea Plan.)* The second major product being considered by the Council has two components - the Zoning Map amendments and the Development Regulations consisting of a major update to the General Commercial Chapter and minor amendments to the Sign Code. *(See Attachments 2 and 3 for two slightly different versions of ordinance language, but with each including the same Zoning Map, General Commercial code chapter, and Sign Code amendment.)* The final major product is the Planned Action Ordinance *(see Attachment 4)*. More explanation about these items is contained in the Narrative section of this memo.

Between January 1 and August 1, 2017, the City Council held eight meeting sessions on the plan or related code changes for the Highway 99 area. Of these meetings, the minutes from the Council's three public hearings (held Feb. 21 for the Subarea Plan, June 20 for the Zoning Map and Development Regulations, and July 31 for the Planned Action ordinance) are provided here, along with the Council's July 31 minutes for review of the Subarea Plan and Development Regulations *(See Attachments 5, 6, 7, and 8, respectively.)* Also, draft minutes from the Planning Board's July 26 meeting are attached here *(Attachment 9)*.

Staff Recommendation

Take action to adopt the following:

1. The Subarea Plan ordinance (Attachment 1);
2. The "Code Changes" ordinance (Attachment 2)--or if Council prefers--Attachment 3 that has the narrow applicability exception.
3. The Planned Action ordinance (Attachment 4).

NOTE: These could be directly adopted August 15 or placed on the next Consent Agenda.

Narrative

Subarea Plan

The proposed Subarea Plan (*see Attachment 1*) has evolved through the public process. It builds on values identified at community meetings, especially: Connectivity, Walkability, Safety, Enhancement of destinations, Healthy businesses, Affordable housing, and Beautification. The Plan provides a vision for land uses and development over the long-term. It calls for more housing, especially affordable housing, and identifies needed transportation improvements. It includes implementation strategies, policies, and action steps.

Zoning Map and Development Regulations

Two slightly different "code changes" ordinances have been prepared for adopting the Highway 99 area development regulations. They are alike in every way, except that one ordinance (*Attachment 2*) uses standard language that would mean the date for vesting under the existing (not new) regulations ends five days after publication of the adopted ordinance (typically 7-10 days after actual adoption). Thus, any building permit submitted after that effective date would be subject to the new regulations. The alternative "code changes" ordinance (*Attachment 3*) adds a special Section 5 that would allow a limited opportunity for a building permit application to not be subject to the new regulations if the project had design review approval between July 1, 2017 and August 15, 2017. This option relates to a particular development proposal that, as of August 9, had design review approval but for which a building permit had not been submitted. (The vesting period is typically tied to the date of a complete building permit application.) Meanwhile, the project applicant has noted an intent to submit a building permit application during the week of August 14--which would be ahead of the effective date of the ordinance. Both versions of the ordinance (i.e., the one with the standard language and the one with language that allows a narrow exception for applicability) are part of this packet. The Council may choose which one to move forward for adoption.

The proposed General Commercial (CG) development regulations (*Exhibit 1 of Attachments 2 and 3*) and the proposed minor Sign Code amendment (*Exhibit 2 of Attachments 2 and 3*) are the same as reviewed at the City Council's July 31 meeting. The CG regulations provide for site development standards and building design standards, consistent with the Subarea Plan. The sign code amendment limits new pole signs in the CG district to a 14-foot height. This is also consistent with the Subarea Plan.

Exhibit 3 of Attachments 2 and 3 provide for the zoning map changes that have been discussed. The changes consolidate the CG and CG 2 zones and convert several multifamily sites within the CG corridor to CG. This is consistent with the current Comprehensive Plan and with the proposed Subarea Plan.

The following Question-and-Answer section addresses several key issues discussed at recent Council meetings regarding the code changes.

Question: How does the City's proposed code for the CG district prevent "oppressive" **building design**, specifically regarding tall, flat walls (such as have been observed in some other cities)?

Answer: This question came up from a Council member at the July 31 meeting and a response was promised for the next meeting. The straightforward answer is that the proposed CG Code has building design standards that address this issue. For example, 16.60.030.D requires the following:

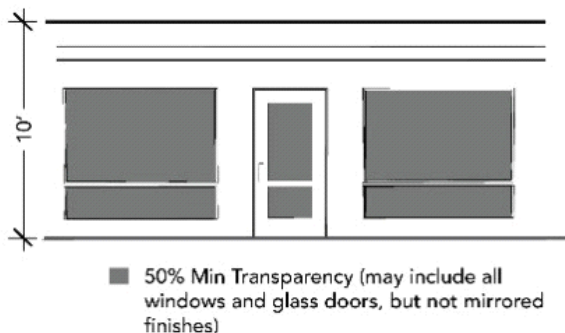
1. General. To provide variety and interest in appearance, the following design elements should

be considered, and a project shall demonstrate how at least four of the elements will be used to vary the design of the site:

- a. Building massing and unit layout,
- b. Placement of structures and setbacks,
- c. Location of pedestrian and vehicular facilities,
- d. Composition and character of open space, plant materials and street trees,
- e. Variety in architectural elements, façade articulation, and/or building materials,
- f. Roof variation in slope, height and/or materials.

2. Building Design and Massing.

- a. Buildings shall convey a visually distinct “base” and “top, which may be achieved through differences in massing elements and/or architectural details.
- b. The bulk and scale of buildings of over 3,000 square feet in footprint shall be mitigated through the use of massing and design elements such as façade articulation and modulation, setbacks, step-backs, distinctive roof lines or forms, and other design details.
- c. Primary Frontage: On the primary frontage, to provide visual connection between activities inside and outside the building, 50% of the building façade between 2 and 10 feet in height, as measured from the adjacent sidewalk, shall be comprised of windows or doors that are transparent, the bottom of which may not be more than 4 feet above the adjacent sidewalk



- . A departure from this standard may be approved when the façade will not be visible from the public street due to the placement of other buildings on the site, provided that the requirements of subsection “e “ in this section shall apply.
 - i. On the primary frontage, no vehicle parking shall be located within the first 20 feet of the first level of a building facing the street except where such parking is underground.
- d. All Other Building Frontages: All street-facing facades within 30 feet of a public street, other than for the primary frontage or those facing an alley or the last block of a dead-end street, shall comply with the standard below.
 - i. Thirty percent (30%) of the building façade between 2 and 10 feet in height shall be made of windows or doors that are transparent, the bottom of which may not be more than four feet above the adjacent sidewalk. Windows shall not be mirrored or have glass tinted darker than 40% in order to meet this requirement.
- e. Wall treatment: Building facades not subject to all requirements of ECDC 16.60.030.D.2.c or ECDC 16.60.030.D.2.d are intended to not display blank,

unattractive walls to the public or to other building tenants. To accomplish this, walls greater than 30 feet in length shall have architectural treatment that incorporates at least four of the following elements into the design of the facade:

- i. Masonry (except for flat concrete block).
- ii. Concrete or masonry plinth at the base of the wall.
- iii. Belt courses of a different texture and color.
- iv. Projecting cornice.
- v. Projecting metal or wood canopy.
- vi. Decorative tilework.
- vii. Trellis containing planting.
- viii. Medallions.
- ix. Artwork or wall graphics.
- x. Vertical differentiation.
- xi. Decorative lighting fixtures.
- xii. Glazing
- xiii. An architectural element not listed above that is approved by the director to meet the intent of this subsection.

Question: How are **pedestrian areas** treated in the draft CG regulations, compared to the existing CG regulations?

Answer: In the proposed regulations, a pedestrian area is required. It must include a streetscape zone next to the curb of at least 5 feet, a clear pedestrian zone of at least 5-10 feet and a pedestrian activity area behind that, which may include walkways, benches, and other pedestrian amenities. However, under existing regulations, no specific requirements are provided for a pedestrian area.

Question: How are **amenity spaces** treated in the draft CG regulations, compared to the existing CG regulations?

Answer: The draft regulations require amenity space equaling at least five percent of the building footprint. (Note: The language also recognizes that amenity space may be shared by more than one property owner.) Amenity spaces must include pedestrian-oriented features, such as seating, paths, gazebos, and artwork. This compares to the existing CG regulations, which require no amenity space.

Question: How are **heights, setbacks, and stepbacks** treated in the draft CG regulations compared to the existing CG regulations?

Answer: The proposed regulations are stricter for some aspects and the same for other aspects. *Attachment 10* shows two information tables for comparison. One table shows the dimensional requirements copied from the proposed regulations; the other table shows the dimensional requirements copied from the existing regulations.

Comparing tables reveals that:

- The height limits would remain the same (except that CG and CG2 would be consolidated)
- The minimum street setback would be increased under the new regulations from 4 feet to 10 feet (except that vehicle sales use would be allowed a minimum street setback of 5 feet)

- The minimum side and rear setbacks would remain the same, meaning no minimum except 15 feet is required adjacent to single-family zones, same as now.

In addition, the draft regulations require an additional 10-foot “stepback” for all CG buildings greater than 25 feet in height when the building is adjacent to a single-family zone. Any portion of a building above 55 feet in height, adjacent to a single-family zone, must be stepped back 20 feet from the required setback (which means at least 35 feet total-i.e., a 15-foot setback plus a 20-foot “stepback”-from the single-family zoned property). This is in contrast with the existing regulations, which require NO additional stepbacks from single-family zones for buildings greater than 25 feet in height.

Clearly, the proposed new requirements for setbacks and stepbacks are, in some aspects, the same but, in other aspects, stricter than the existing code for this area.

Question: Do the vehicle parking standards provide incentives to favor **transit-oriented development**?

Answer: The proposed parking standards require a minimum 0.75 vehicle parking spaces for each residential unit that is less than 700 square feet; 1.25 spaces for residential units between 700 and 1100 square feet, and otherwise, 1.75 parking spaces for each unit. (Note: Additional guest parking must be provided too.) However,, the applicant has several options for a reduced number of parking spaces related to transit-oriented development--for example, if the project is within 1/4 mile from a bus rapid transit station or provides car-share or bike-share facilities on site.

Question: Is **vehicle parking** allowed on the first floor facing the street?

Answer: A specific limitation was added last month to the proposed code. The limitation prohibits vehicle parking within the first 20 feet of a building where the building faces a primary street, unless the parking is underground.

Question: How do the proposed regulations encourage or incentivize “green” and sustainable development?

Answer: The proposed regulations:

- Require bicycle storage for most multifamily housing (this is not required anywhere else in Edmonds)
- Require electric vehicle charging infrastructure for all new development that includes housing (this is not required anywhere else in Edmonds and exceeds the standard of most other cities in our region)
- Establish a walkable and transit-friendly character in multiple ways (which recognizes that reducing vehicle usage will result in less carbon emissions and that people who walk more have a better chance of being healthy)
- Require amenity space that promotes sustainability-for example, having places for people to play and socialize, places for trees and vegetation (while specifically encouraging native plant species).

In addition, new development will be encouraged to take advantage of solar programs and other renewable energy. One way that the City could incentivize solar is to allow a fee reduction in the permit fee schedule for projects that use solar. This approach could be considered as part of an update to the permit fee resolution. Alternatively, projects with solar components could be

prioritized for plan review. (The downside of the latter is that other plans that were submitted first would be bumped back in the queue.)

Planned Action Code

The Growth Management Act and the State Environmental Policy Act provide for a “planned action” process that jurisdictions can use to streamline the environmental review process for projects that have already been covered by an Environmental Impact Statement (EIS) developed for a subarea. This is, in part, because the EIS will have analyzed impacts of development and identified mitigation measures to address the impacts.

To memorialize the EIS work for the subarea, cities may adopt a “planned action ordinance” or code that specifies the type and amount of development that may be allowed to proceed in the subarea without additional SEPA review. Submittal of a SEPA checklist and all other applicable laws and regulations would still apply.

In Edmonds, an EIS was prepared for the Highway 99 subarea. The draft was issued June 2 and a public comment period, including a public open house, followed. The final EIS was issued and posted online August 3. This is the first time in Edmonds that this level of environmental analysis for a large area where development is planned over the long-term, has been undertaken. A planned action ordinance (*see Attachment 4*) has been prepared. It reflects findings from the Highway 99 Subarea EIS (which is currently available online at <http://www.edmondshwy99.org/>), especially for mitigation and for tracking key development impacts. The Planning Board held a public hearing on the draft planned action code on July 26 and recommended Council adoption. (*See Attachment 9, Planning Board minutes*). The City Council held its public hearing on the planned action code on July 31 (*see Attachment 8, City Council minutes*). Note: The public comments at the planned action hearing generally related to the CG development regulations, not the planned action proposal.)

Next Steps

Next steps include:

- August 15 - City Council meeting for potential action to adopt the Subarea Plan, the zoning map and development regulations, and the planned action. This may be done by voting to adopt the ordinances on August 15 or placing some or all of the ordinances on the next Consent Agenda.
- After ordinance adoption, other steps may be taken, such as considering a multifamily tax exemption program and beginning detailed design work for Highway 99 improvements.

Attachments:

Attachment 1: Subarea Plan Ordinance_2017-08-09
 Attachment 2: Code Changes
 Attachment 3: Code Changes with Exception
 Attachment 4: HWY99 PAO DRAFT
 Attachment 5: Council Hearing Minutes_170221
 Attachment 6: Council Hearing Minutes_170620
 Attachment 7: Council Hearing Minutes PAO_170731
 Attachment 8: Council Minutes Subarea_170731
 Attachment 9: PB DRAFT Hearing Minutes_170726
 Attachment 10: CG Comparison Tables

ORDINANCE NO. _____

AN ORDINANCE OF THE CITY OF EDMONDS,
WASHINGTON, ADOPTING THE HIGHWAY 99 SUBAREA
PLAN AS AN ELEMENT OF THE CITY'S COMPREHENSIVE
PLAN.

WHEREAS, planning for the Highway 99 subarea has been actively underway since the beginning of 2016; and

WHEREAS, revitalization of this area (including highway corridor improvements) has been a high priority for the City Council; and

WHEREAS, public outreach for the Highway 99 subarea planning project has been extensive; and

WHEREAS, a draft Highway 99 Subarea Plan was recommended by the Planning Board and presented at a City Council public hearing on February 21, 2017; and

WHEREAS, the City Council had further review and discussion at its March 21, 2017 meeting, which was followed by minor clean-up and clarification of the draft plan; and

WHEREAS, on June 6, 2017 the Council again reviewed and discussed the plan; and

WHEREAS, the proposed Highway 99 Subarea Plan (*see **Attachment A***) presents a vision and strategy for revitalizing and enhancing the Highway 99 area in Edmonds; and

WHEREAS, it has been prepared in consultation with state and regional transportation agencies, as well as with adjacent jurisdictions, and in a manner consistent with the state Growth Management Act; and

WHEREAS, the Subarea Plan, upon adoption, becomes an element of the City's Comprehensive Plan; and

WHEREAS, a Planned Action Draft Environmental Impact Statement for the Highway 99 Subarea Plan was issued on June 2, 2017 with a public comment period running through July 3, 2017; and

WHEREAS, the Planned Action Final Environmental Impact Statement for the Highway 99 Subarea Plan was issued on August 4, 2017 with an appeal period ending on August 18, 2017; and,

WHEREAS, no appeals on the adequacy of the Planned Action Final Environmental Impact Statement for the Highway 99 Subarea Plan were received; and,

WHEREAS, under the GMA, a subarea plan may be initially adopted independently of the regular Comprehensive Plan amendment cycle; and

NOW, THEREFORE,

THE CITY COUNCIL OF THE CITY OF EDMONDS, WASHINGTON, DO ORDAIN AS FOLLOWS:

Section 1. The City Council hereby makes the following findings as required by ECDC 20.00.050:

- A. The City Council finds that the proposed Subarea Plan is consistent with the provisions of the Edmonds Comprehensive Plan and is in the public interest.
- B. The City Council finds that the proposed Subarea Plan would not be detrimental to the public interest, health, safety or welfare of the city.
- C. The City Council finds that the proposed Subarea Plan would maintain the appropriate balance of land uses within the city.
- D. The City Council finds that the property shown in **Attachment B** hereto is physically suitable for the proposed Subarea Plan and the anticipated land use development, including, but not limited to, access, provision of utilities, compatibility with adjoining land uses and absence of physical constraints.

Section 2. The Edmonds Comprehensive Plan is hereby amended to incorporate the Highway 99 Subarea Plan as set forth in **Attachment A**, which is attached hereto and incorporated herein by this reference as if fully set forth herein, and that the plan is hereby adopted.

Section 3. The Edmonds Comprehensive Plan and, specifically, the map designating land uses is hereby amended to redesignate the area shown in **Attachment B** as the Highway 99 Subarea. The Development Services Director is hereby authorized to reflect this redesignation on all current land use maps of the City.

Section 4. Severability. If any section, subsection, clause, sentence, or phrase of this ordinance should be held invalid or unconstitutional, such decision shall not affect the validity of the remaining portions of this ordinance.

Section 5. Effective Date. This ordinance, being an exercise of a power specifically delegated to the City legislative body, is not subject to referendum and shall take effect five (5) days after passage and publication of an approved summary thereof consisting of the title.

APPROVED:

MAYOR DAVE EARLING

ATTEST/AUTHENTICATED:

CITY CLERK, SCOTT PASSEY

APPROVED AS TO FORM:
OFFICE OF THE CITY ATTORNEY:

BY _____
JEFF TARADAY

FILED WITH THE CITY CLERK:
PASSED BY THE CITY COUNCIL:
PUBLISHED:
EFFECTIVE DATE:
ORDINANCE NO. _____

SUMMARY OF ORDINANCE NO. _____

of the City of Edmonds, Washington

On the ____ day of _____, 2017, the City Council of the City of Edmonds, passed Ordinance No. _____. A summary of the content of said ordinance, consisting of the title, provides as follows:

AN ORDINANCE OF THE CITY OF EDMONDS,
WASHINGTON, ADOPTING THE HIGHWAY 99
SUBAREA PLAN AS AN ELEMENT OF THE CITY'S
COMPREHENSIVE PLAN.

The full text of this Ordinance will be mailed upon request.

DATED this ____ day of _____, 2017.

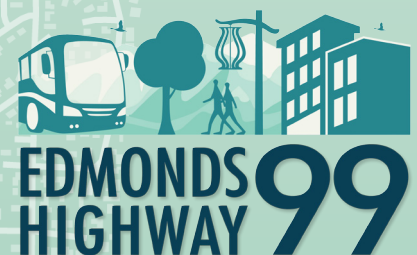
CITY CLERK, SCOTT PASSEY

4840-7251-8158, v. 1

EDMONDS HIGHWAY 99

SUBAREA PLAN

DRAFT JUNE 2017



ACKNOWLEDGEMENTS

CONTENTS

INTRODUCTION.....2

Document Overview
The Planning Context
The Planning Process
Past Planning Efforts
Planned Action EIS Overview

VISION + COMMUNITY VALUES.....8

Vision Goals
Community Values

BACKGROUND + EXISTING CONDITIONS.....12

The Study Area
Unique Districts
Existing Land Use Patterns
Existing Transportation
Existing Economic Conditions and Market Trends

COMMUNITY + STAKEHOLDER ENGAGEMENT.....26

Technical Advisory Committee
Key Stakeholder Interviews
Community Visioning Workshop
Public Open House
Online Survey and General Comments
Web Outreach and Social Media
Engagement Outcomes

CONSTRAINTS + CHALLENGES.....34

Land Use Constraints + Challenges
Economic Constraints + Challenges
Transportation/Infrastructure Constraints &
Challenges

OPPORTUNITIES.....38

Land Use Opportunities
Economic Opportunities
Transportation/Infrastructure Opportunities

ALTERNATIVE SCENARIOS.....44

Land Use and Transportation Scenarios
Scenario Building Blocks
Development Capacity Analysis

IMPLEMENTATION STRATEGIES, POLICY RECOMMENDATIONS + ACTIONS.....50

Zoning and Development
Affordable Housing
Signage and Wayfinding
Transit
Transportation Infrastructure

APPENDIX.....99

A. Planned Action Environmental Impact Statement



INTRODUCTION

The Subarea Plan is a vision and action plan to enhance the Highway 99 area, support prolonged economic prosperity in the corridor area, and build a more attractive place for the Edmonds community to live, work, and play.

The City of Edmonds initiated the Edmonds Highway 99 Subarea Plan to address future land use and transportation needs on and around the Highway 99 corridor. The plan acts as a guide for future development of the corridor area, and includes specific actions and investments designed to bring positive changes to the community.

DOCUMENT OVERVIEW

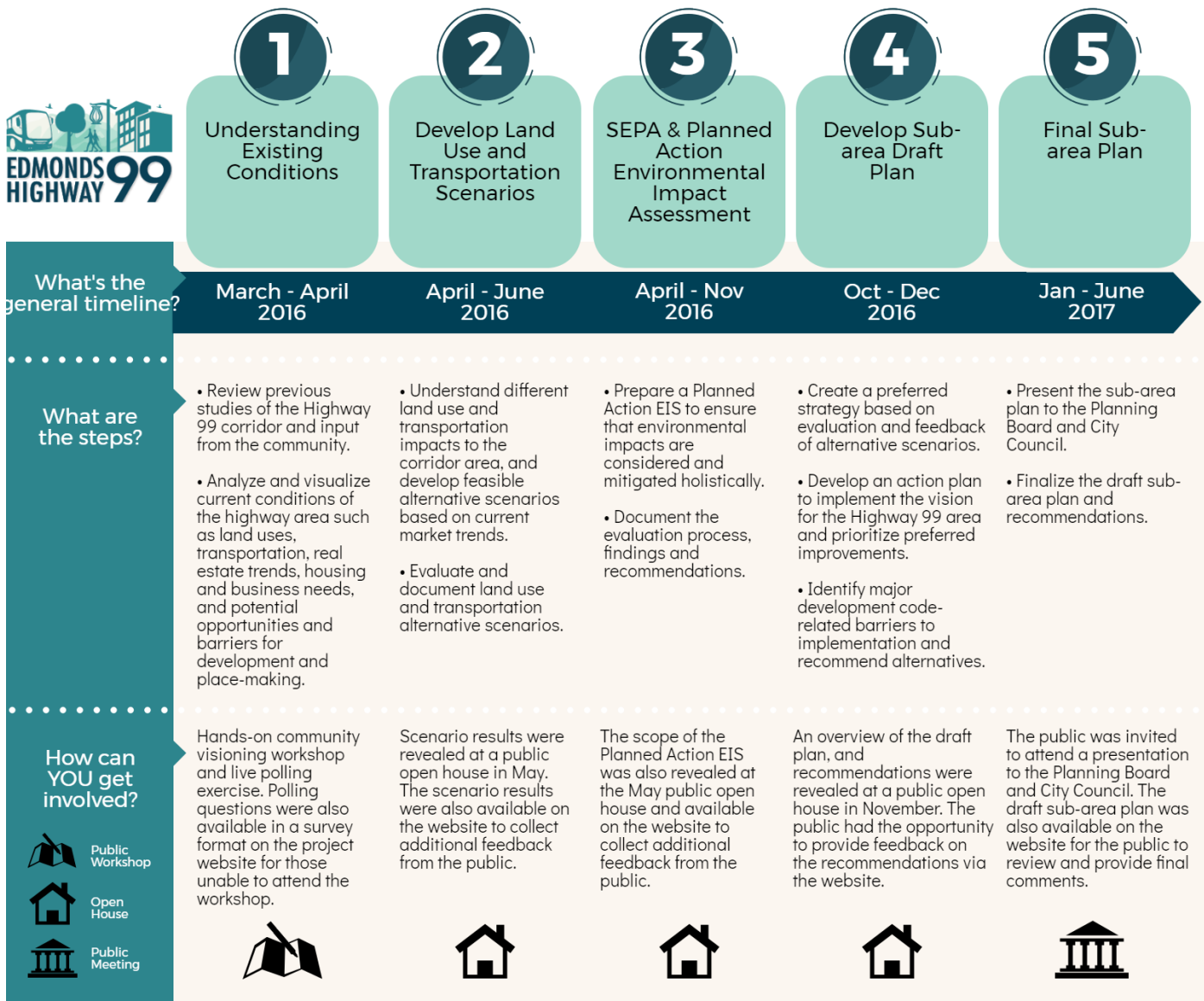
This document provides guidance for how the corridor should grow and change in the future and sets forth the opportunities and actions needed to address the challenges on Highway 99. The Plan identifies the constraints and opportunities for land use, transportation, and economic development. It describes two alternative scenarios representing different intensities of investment and redevelopment in the short- and long-term future. The accompanying Implementation Strategy lays out the investments, policy changes, and short-, medium-, and long-term actions to transform the Highway 99 area into a vibrant, mixed-use, transit-oriented corridor.

THE PLANNING CONTEXT

As part of the ten-year state transportation budget adopted in 2015, \$10 million was allocated for improvements to Highway 99 in Edmonds. The first \$1 million will be available in the early years of planning for the corridor. The Subarea Plan helps make the case for obtaining significant additional federal, state, and regional grant funds to implement the policies and strategies set forth in this plan. Successful implementation of the plan will depend on a secure source of funding and collaborative decision-making from state legislators and city officials.

Neighboring directly south of Highway 99 in Edmonds, the City of Shoreline has embarked on significant improvements to Aurora Avenue within its boundaries, emphasizing improvements for transit and pedestrian use. Continuing this momentum in Edmonds will benefit the Edmonds community as well as the broader region creating a livable, vibrant community around high-capacity transit that visitors, businesses, and residents can take full advantage of.

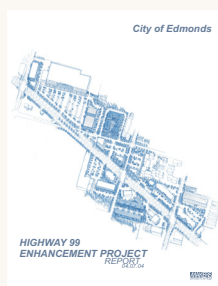
THE PLANNING PROCESS



PAST PLANNING EFFORTS

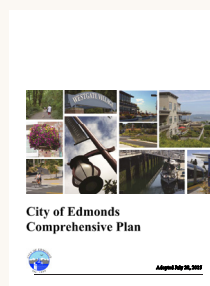
During a City Council retreat in 2002, the Highway 99 corridor was identified as one of the areas of greatest potential for generating tax revenue for the city. Subsequently, the Highway 99 Task Force was formed at the City Council retreat in 2003 to study and make recommendations on how to maximize economic growth along the Highway 99 corridor. As a result, the City of Edmonds Highway 99

Enhancement project began engaging neighborhood representatives and business and property owners in 2004 to identify key local objectives and recommendations along the corridor. This subarea plan is intended to augment the work started in 2004 and set forth concrete actions steps to move towards implementing land use and transportation improvements on Highway 99. The Highway 99 Subarea Plan is a result of many years of study and careful planning.



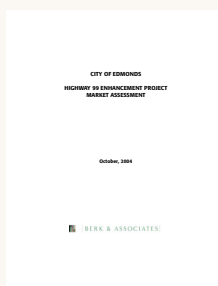
2004 Highway 99 Enhancement Project

This report identifies local objectives and development opportunities for Comprehensive Plan and Zoning Amendments eventually adopted in 2004. The plan outlined concepts for four focus areas along the corridor and made recommendations for furthering redevelopment efforts.



2015 City of Edmonds Comprehensive Plan

The Comprehensive Plan identifies Highway 99 as a major activity center "intended to encourage the development of a pedestrian and transit oriented area focused on two master planned developments, Swedish/Edmonds medical center and Edmonds-Woodway High School, with a related high-intensity development corridor along Highway 99."



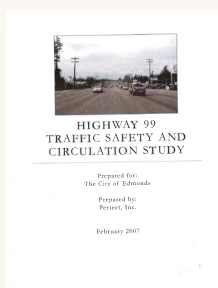
2004 Highway 99 Enhancement Project Market Assessment

This report is a market feasibility assessment of commercial and residential development near Highway 99. The study identified enhancement scenarios, market factors, multifamily housing considerations, and short-term retail development opportunities. The report also identified barriers to further development, including the need to improve left turns and highway crossings.



2014 City of Edmonds Comprehensive Transportation Plan

The Transportation Plan serves as the transportation element of the City's Comprehensive Plan. It identifies transportation infrastructure and services needed to support projected land use within the city through the year 2035. Several intersections along Highway 99 were identified for transportation improvements to provide safer access management throughout the corridor and additional safety and urban design improvements.



2007 Highway 99 Traffic Safety and Circulation Study

The Traffic Safety and Circulation Study evaluated the transportation system's needs based on current and future traffic and land use conditions, developed a prioritized list of multi-modal solutions to the transportation needs of the study area, and identified projects for early implementation and incorporation into the City's Capital Improvement Plan (CIP).

PLANNED ACTION ENVIRONMENTAL IMPACT STUDY (EIS) OVERVIEW

A Planned Action EIS is an upfront assessment of environmental conditions, potential impacts, and mitigation measures for the Edmonds Highway 99 Subarea, rather than a piecemeal analysis on a project-by project basis. As such, the EIS provides developers certainty and predictability while streamlining the environmental review and permitting process and furthering the goals of the State Environmental Policy Act (SEPA) and the Growth Management Act (GMA). Planned actions still need to meet the City's development regulations and to obtain necessary permits.

The alternatives considered in the Draft EIS for the Subarea Plan include No Action (Alternative 1) and the Preferred Alternative (Alternative 2). Under Alternative 1, future growth would continue based on existing development regulations and past development trends. Alternative 2 assumes future mixed use growth with an emphasis on residential, commercial, and office development and assumes a new vision for the area supported by transportation system improvements and updates to existing development regulations. See Appendix A for a more detailed description of impacts for each alternative.

FIGURE 1: PLANNED ACTION EIS PROCESS



TABLE 1: KEY FEATURES OF EACH ALTERNATIVE IN THE EIS

FEATURES	ALT. 1 NO ACTION	ALT. 2 PREFERRED ALTERNATIVE
New Housing Units by 2035	1,224	3,325
New Jobs by 2035	2,317	3,013
New commercial sq.ft. by 2035	994,958	1,634,685
Comprehensive Plan	No change	Proposed update to Highway 99 subarea maps and text to clearly identify three distinct districts in the subarea anchored around major transportation gateways and employment clusters (See Figure 2, page 14.)
Zoning Designations	No change	Rezone the CG2, RM-1.5, BN, and portions of the RM-2.4 and BC zones throughout the study area to CG (see page 56).
Development Code Amendments	Existing development regulations would remain unchanged	<p>In summary, development code amendments include the following (For a more detailed description of code amendments, see the Recommendations chapter on page 50.)</p> <ul style="list-style-type: none"> » Building height and CG zone consolidation » Transit-supportive parking standards » Building frontage standards » Building transparency standards » Park lot location » Pedestrian Activity Zone » Ground floor setback » Upper story setbacks
Transportation Improvements	Future improvements would continue to occur on an incremental basis with new development and as planned by the City's Transportation Master Plan and WSDOT plans.	Improvements to the Highway 99 Corridor and adjacent local streets would include measures to maintain level of service standards, increase east/west connectivity, provide greater bicycle and pedestrian mobility, and improve access to transit (see page 83 for a complete list of proposed improvements).



VISION + COMMUNITY VALUES

The Vision for the Edmonds Highway 99 area establishes a framework for the Subarea Plan and describes an ideal snapshot of how the area could evolve in the future.

The vision is based on the knowledge and ideas of Edmonds residents and stakeholders gathered during public workshops, stakeholder interviews, and through public surveys. The Edmonds community shared a wide-range of creative ideas for improvements that will enable people to enjoy safe and easy access to Highway 99's diverse services and amenities, better access to the area's robust transit system, and more opportunities for affordable housing, jobs, and destinations.

THE VISION GOALS

The Vision goals here represent the themes that surfaced throughout community discussions with Edmonds residents and stakeholders. They describe the qualities residents want to see in the Highway 99 corridor area.

Economic Development

Stimulate the economy by attracting and encouraging new businesses, investment, and redevelopment.

Safety and Walkability

Create a safe and comfortable place for pedestrians, bicyclists, and motorists to move along and get across Highway 99.

Housing and Development

Encourage and incentivize mixed use development, affordable housing, office/commercial and other types of development.

Identity

Establish a distinct identity along the corridor that supports existing cultural destinations and amenities and creates a welcoming and attractive environment for visitors and residents alike.

Transportation

Create more efficient and accessible connections between districts and destinations, and other transit centers/stations.

COMMUNITY VALUES



CONNECTIVITY

Better connections and access for pedestrians, bicyclists, and transit riders to destinations and amenities in the area.



WALKABILITY

Create walkable neighborhoods and commercial centers where visitors can walk safely and comfortably at all hours of the day.



SAFETY

Better connections and access for pedestrians, bicyclists, and transit riders to destinations and amenities in the area.



HEALTHY BUSINESSES

Bring in new businesses and jobs to the area, encourage existing businesses to thrive, and provide good quality retail and shopping amenities.



DESTINATIONS

Enhance distinct districts in the area such as the Health District and the International District to create more vibrant destinations and an even better sense of place.



BEAUTIFICATION

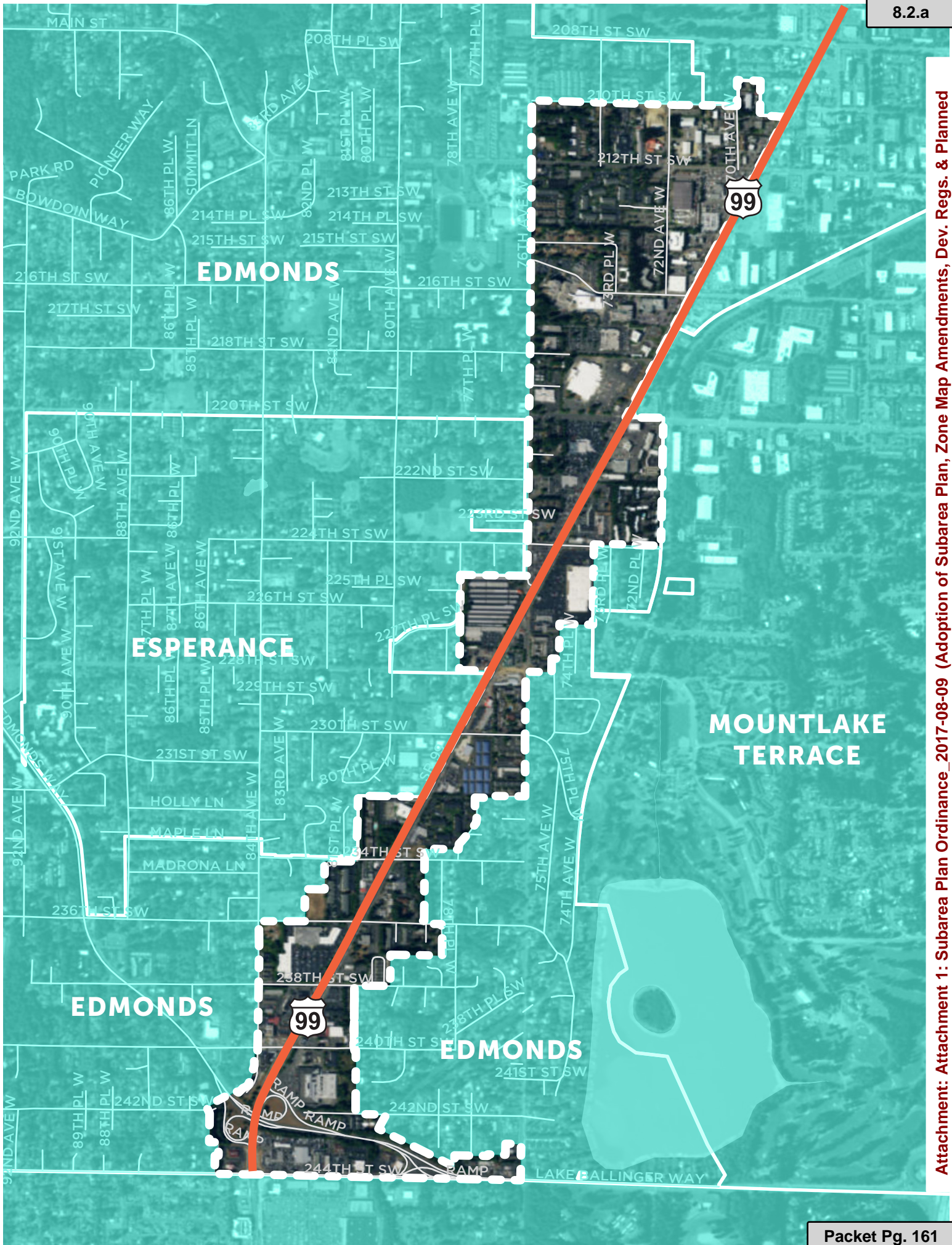
Create a more attractive place for residents and visitors through landscaping and urban design.



AFFORDABLE HOUSING

Encourage affordable housing options for a mix of income levels - low income, workforce, and moderate-income.

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BACKGROUND + EXISTING CONDITIONS

THE STUDY AREA

The Edmonds Highway 99 subarea is approximately 2 miles bordered by several jurisdictions – Lynnwood, Mountlake Terrace, Shoreline, and the Unincorporated Snohomish County neighborhood of Esperance. The subarea is a major urban center for Edmonds with professional services and retail amenities along the corridor. There are distinct subdistricts and is already a horizontal mixed-use district. However, buildings along HWY 99 are predominately highway-oriented, set far back from the road with large surface parking lots in front, which results in an unpleasant and unsafe environment for pedestrians. Many of the buildings are old and reaching the end of their natural life. As Edmonds contemplates where new growth and economic development can occur within the community, this stretch of HWY 99 has been identified for change and there is broad support for a reimagining of its historic role as just a high-speed highway.

The study area currently has approximately **3,800 jobs and 1,600 housing units**. There are about **2.4 jobs for every housing unit**. As such, the subarea is currently an employment destination, with more than twice as many employees as households. Attracting more housing, especially affordable housing, to this jobs-rich area will increase walking and biking trips and reduce the need for more auto trips in the area.

Key Assets in the Corridor Area:

1. Opportunity to build on the momentum of ongoing neighborhood improvements in Shoreline along Highway 99 while creating a mixed-use corridor that is distinctly Edmonds.
2. High-quality transit facilities already in place – Community Transit, Sound Transit, and Swift BRT – providing links to housing, employment and other amenities to the Seattle Metro region.
3. Distinct districts are already emerging along the corridor area – the International District and Health District – that provide core services and amenities for Edmonds' diversifying population.
4. Corridor area is already a major urban center and mixed-used district for Edmonds with retail uses adjacent to apartments and single-family neighborhoods.
5. The business and developer community and residents alike are ready to see positive changes in the area and are excited for the opportunity to create a pedestrian-friendly, transit-oriented environment.

UNIQUE DISTRICTS

The area has three distinct subdistricts with major local and regional destinations along the corridor.

HEALTH CARE DISTRICT

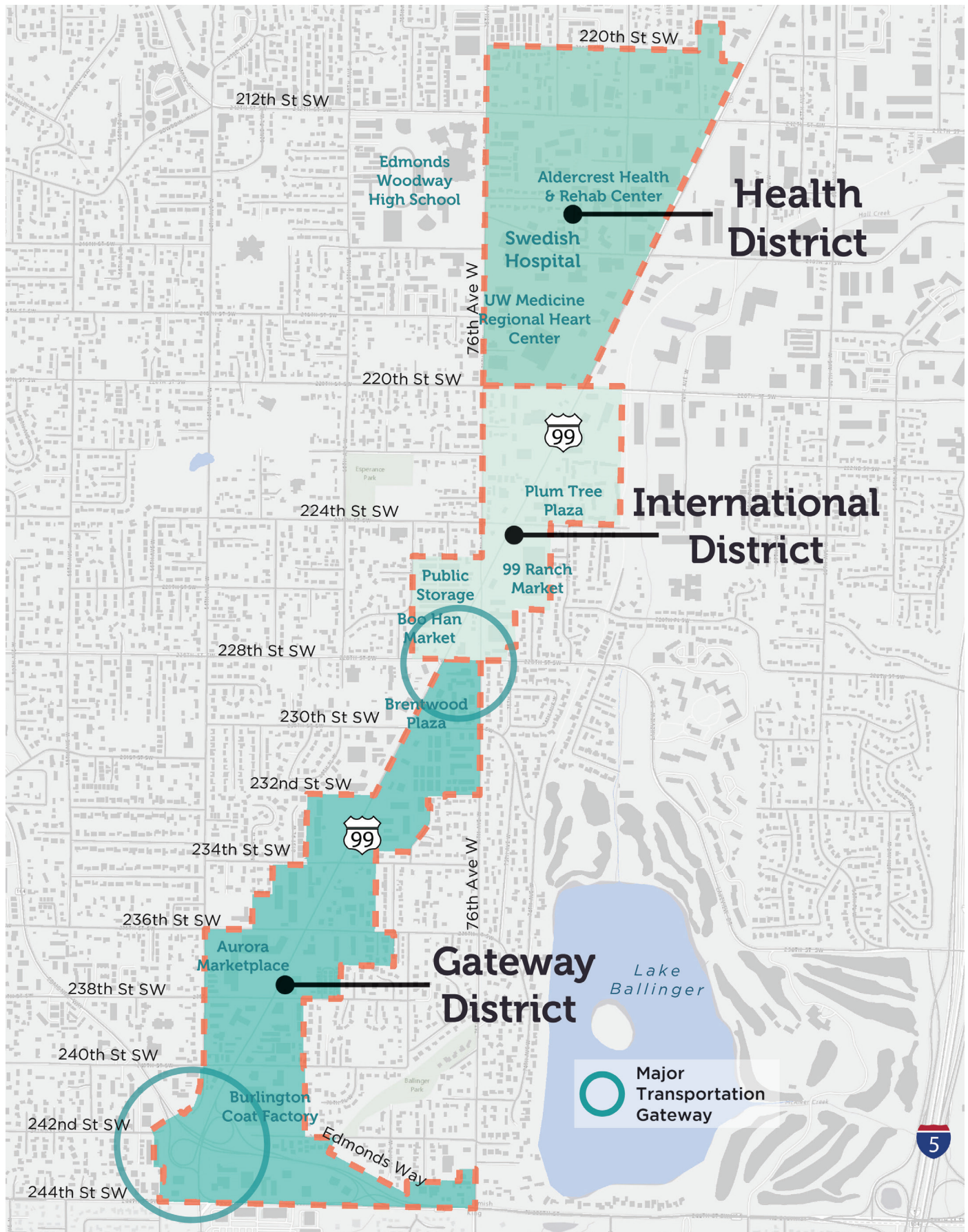
Located approximately between SW 208th St. to SW 220th St., the Health Care District is home to variety of health care facilities and offices, most notably the Swedish Hospital Edmonds Campus. The Edmonds campus includes 34 facilities and services, 217 beds, over 450 physicians and specialists and more than 1,400 staff members. The hospital provides medical and surgical services including Level IV Trauma emergency medicine, diagnostic, treatment and support services. There are many other medical clinics and offices across Highway 99 in Lynnwood as well as schools and higher education such as Edmonds Woodway High School, Mountlake Terrace High School, and Edmonds Community College, within approximately 1.5 miles from the Health Care District core on Highway 99.

The Health Care District is not only a provider of vital health services for the region, but also a growing incubator for medical research, partnerships, health and wellness advocacy, and education opportunities.



Edmonds Health Care District

FIGURE 2: DISTINCT DISTRICTS AND MAJOR DESTINATION WITHIN CORRIDOR AREA



INTERNATIONAL DISTRICT

Located approximately between SW 224th St and SW 238th St, the International District is a major cluster of Asian-owned businesses, particularly Korean-American businesses, with diverse restaurants, grocers, and shops. The International District is already a regional destination for culture, food, and entertainment – but there is an opportunity to strengthen the identity of this district and help it thrive in the long term.

The SR 99 International District Enhancement Project was a key recommendation identified in the 2004 Enhancement Study and the 2006 Market Analysis to build on the growing cluster of international businesses, largely anchored by the Ranch 99 Market, Boo Han Plaza, and other specialty plazas. In 2006, the City began efforts through federal grant funds to strengthen the International District identity by improving the area's visual identity and aesthetics, and implement pedestrian-oriented improvements with new gateway elements including a new pedestrian level lighting, new district identification signage on custom light poles, resurfacing of the island on 76th Avenue and a solar lit sculptural piece on the island as part of the gateway.



International businesses along Highway 99. Left: Boo Han Plaza; right: Ranch 99 Market

A major transportation gateway on 228th and Highway 99 is planned to create safe and easy access across the highway and connect to the recently completed bicycle lane that flows to the future regional trail (Interurban Trail) and to the Mountlake Terrace Transit Center. These transportation improvements will provide a critical connection for both local residents and regional transit riders to the International District. The gateway design potential here will also help solidify the identity of the district.

GATEWAY DISTRICT

The Gateway District, located approximately between SW 234th and the 104 Interchange north of SW 205th St, is the first introduction to Edmonds on Highway 99. This area was identified in the Enhancement Study as “Residential Area Retail Center” and “Hotels Area Improvement”. However, the Edmonds community expressed a strong desire for a “gateway” and distinct transition point in and out of Edmonds during a community workshop. A recognizable marker identifying the entry point to the city will help unify Edmonds as a place with rich history, arts, culture, food, and sense of place.

EXISTING LAND USE PATTERNS

The subarea today contains a mix of land uses including commercial, residential, industrial, public, and educational uses. Commercial uses make up the largest amount of square footage, which includes office, retail, services, and motels. Multi-family residential uses make up the second largest amount, most which are apartments but also townhomes or duplexes. Much of the surrounding areas to the east and west of the study area are developed as single family residential, and the residential uses on the edges of the study area help to provide transitions to these areas.

There are vacant parcels located throughout the study area with a concentration in the south-central part of the study area. Existing development is generally low-intensity, with one- or two-story buildings and large surface parking areas. There are several plaza-type developments with larger scale grocers and marketplaces such as 99 Ranch Market, Boo Han Market, Aurora Marketplace, and Burlington Coat Factory. The Swedish Edmonds Campus is within a cluster of commercial uses including medical offices and clinics and some higher-intensity and higher-scale buildings.

ZONING ASSESSMENT

The study area has many zoning designations applied within it, and often in odd configurations. The haphazard pattern reflects both zones inherited from the County when the area was annexed and a variety of zone changes in specific places over the past several decades. The Comprehensive Plan designates nearly all of the study area with a single Highway 99 Corridor designation.

As of 2016, there are eight zoning designations:

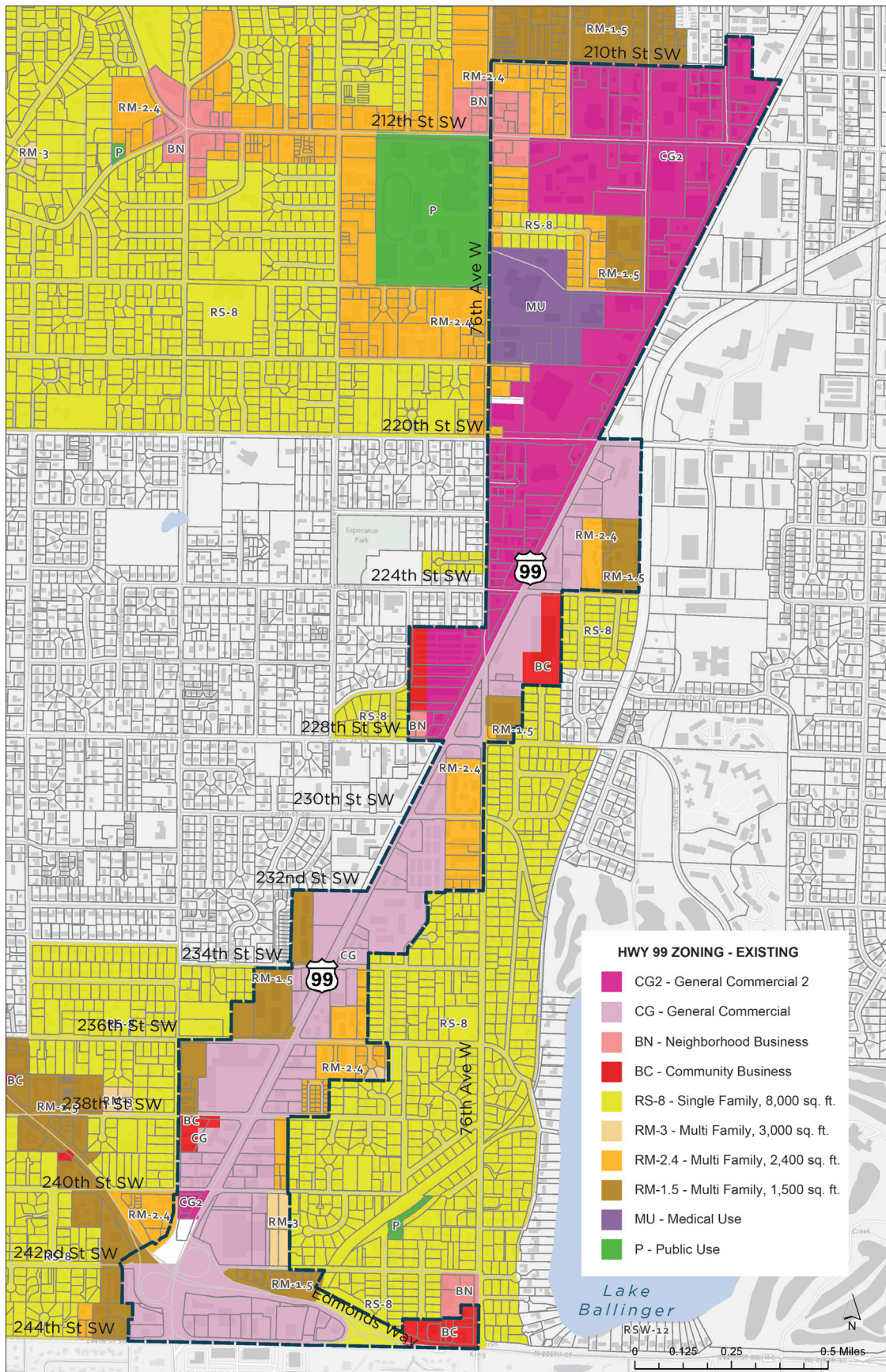
- » **CG and CG2:** General Commercial
- » **NB:** Neighborhood Business
- » **BC:** Community Business
- » **MU:** Medical Use
- » **RM-1.5:** Multifamily
- » **RM-2.4:** Multifamily
- » **RS-8:** Single Family Residential

Most the study area is zoned either General Commercial (CG or CG2) with Multifamily (RM-1.5) adjacent to the corridor. 65% of the study area is within CG and CG2 and 8% is within RM-1.5. The CG zone allows buildings up to 60 feet tall and the CG2 zone allows buildings up to 75 feet tall. Zoning for

TABLE 2: DISTRIBUTION OF LAND USES WITHIN STUDY AREA

DEVELOPED LAND USE TYPE	TOTAL SQUARE FEET	% OF TOTAL
Commercial	5,729,924	50%
Multi-Family Residential	3,070,474	27%
Industrial	1,123,311	10%
Single Family Residential	643,907	6%
Public & Educational	808,607	7%
TOTAL	11,376,223	100%

FIGURE 3: CURRENT ZONING MAP FOR STUDY AREA



the areas surrounding the study area is established by the cities of Edmonds, Lynnwood, Mountlake Terrace, Shoreline and Snohomish County. In general, areas to the south, west and northeast are zoned for a mix of commercial and residential uses; areas to the east, southeast and southwest are zoned primarily for single family residential uses; and areas to the north and northwest are zoned for a mix of residential and public uses. Specific zoning designations in the area surrounding the study area are shown in Table 3.

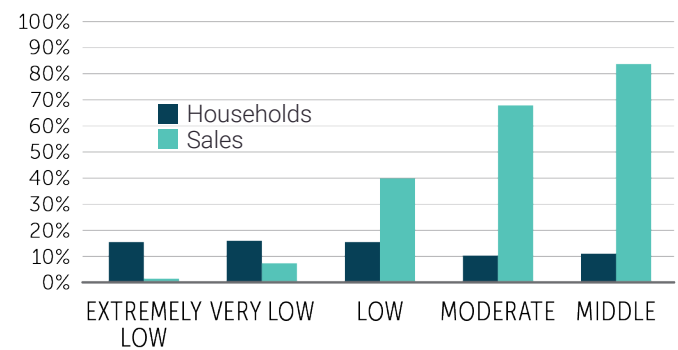
TABLE 3: ZONING DESIGNATIONS IN THE STUDY AREA

EDMONDS ZONING DESIGNATIONS	% OF TOTAL
CG2 – General Commercial 2	32%
CG – General Commercial	33%
BN – Neighborhood Business	2%
BC – Community Business	4%
RS-8 – Single Family, 8,000 sq. ft.	2%
RM-3 – Multi Family, 3,000 sq. ft.	1%
RM-2.4 – Multi Family, 2,400 sq. ft.	12%
RM-1.5 – Multi Family, 1,500 sq. ft.	8%
MU – Medical Use	5%

EXISTING HOUSING

For the greater metropolitan region in which Edmonds is located, housing supply has not been keeping up with demand. Housing needs—along with housing costs—have been rising rapidly. In fact, data was recently obtained through the Alliance for Housing Affordability about the availability of affordable housing along Highway 99 in the Edmonds area. Research conducted in November 2016 showed that the number of rental units affordable to households below 80% of the area median income was very limited and little housing at all was available at rents affordable to households below 50% of the area median income. The same can be said for those wanting to own a home. (Note: “Affordable” rent for this purpose is considered to be a rental amount that does not exceed 30% of the household income.)

FIGURE 4: HOME SALE AFFORDABILITY GAP IN THE CITY OF EDMONDS



Source: US Census Bureau, American Community Survey, 2008-2012

TABLE 4: DISTRIBUTION OF RENT AFFORDABILITY BY SIZE IN THE CITY OF EDMONDS

INCOME LEVEL	STUDIO	1-BED	2-BED	3-BED	4+ BED
Extremely Low	No	No	No	No	No
Very Low	Limited	Limited	Limited	Limited	No
Low	Yes	Yes	Yes	Limited	No
Moderate	Yes	Yes	Yes	Yes	Limited
Middle	Yes	Yes	Yes	Yes	Yes

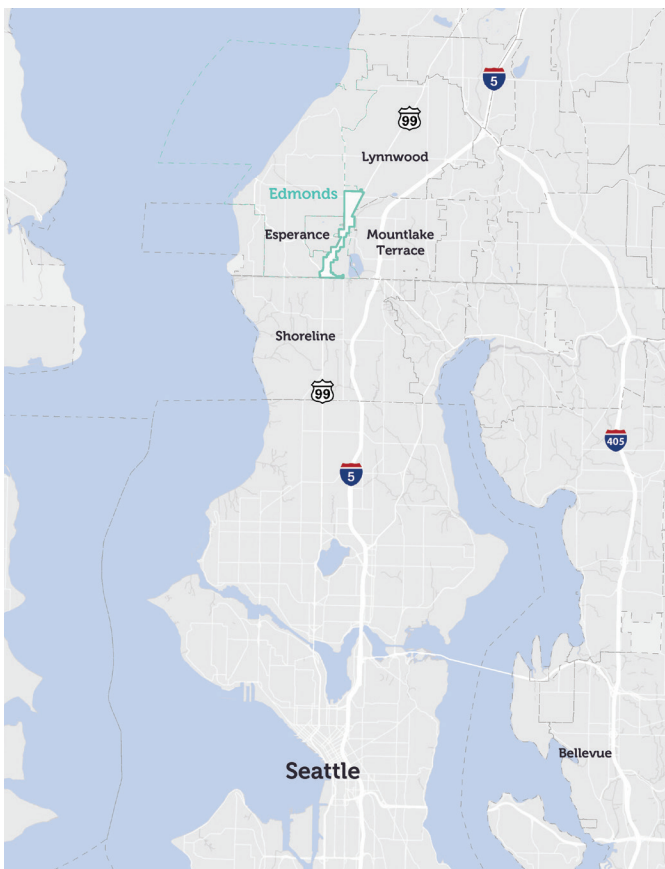
Source: Dupre and Scott, 2013

EXISTING TRANSPORTATION

REGIONAL ACCESS

SR-99 is the highest-traffic carrying arterial in Edmonds and runs north to Everett, and south through Shoreline to Seattle and the Tacoma metropolitan area. The subarea can be accessed through SR-104 which runs east-west through the southern-most border of the subarea between the Edmonds-Kingston Ferry dock in Downtown Edmonds to I-5 east of Highway 99. I-5 runs almost parallel to the east of Highway 99 providing regional access from surrounding cities to the north and south.

FIGURE 5: REGIONAL CONTEXT



STREET NETWORK

Highway 99 serves as a principal arterial in Edmonds providing north-south mobility and access to businesses and services along the corridor. Highway 99's diagonal orientation through a mostly gridded street pattern creates difficult and low visibility turns onto and from the highway for drivers and poses a major safety risk for bicyclists and pedestrians. The minor arterials in the subarea are on 238th Street, 228th Street, 220th Street, and 212th Street providing through traffic east-west. Many local streets are broken and do not have complete connections to other local streets and arterials, which is typical of a more suburban street network. A short portion of the right-of-way between 228th and 234th Street is located in Esperance, an unincorporated township of Snohomish County.

TRANSIT SERVICE

The subarea currently has a very robust transit network with enhancement and service frequency increases planned in the future. The subarea is served by Community Transit, Sound Transit, and Swift BRT lines. There are two Community Transit Swift Bus Rapid Transit (BRT) stops along the corridor and many local bus stops with several transit connections between transit providers.

Edmonds sits at the intersection of two major transit providers. The Community Transit Swift BRT system serves Edmonds and areas north, while the King County Metro BRT system serves areas south of Edmonds. A transfer station is located just over the border in Shoreline adjacent to the large Costco and Home Depot shopping center. Transfers between transit providers can cause significant delay in travel times if arrival and departure times are not coordinated tightly. King County Metro and Community Transit are coordinating times to ensure efficient transfer times, but the City will continue to monitor this to ensure Edmonds has quick and efficient access to Seattle and other points south.

FIGURE 6: CURRENT TRANSIT SERVICE

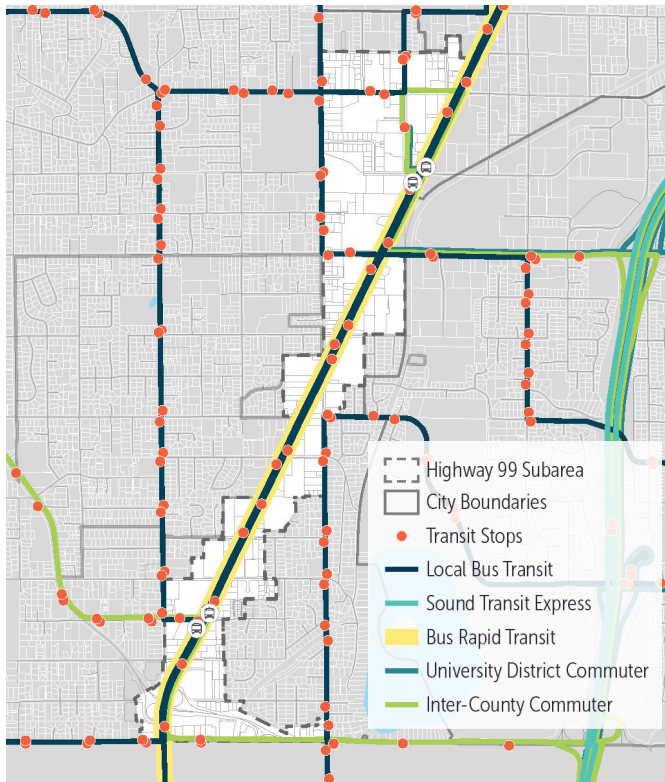


FIGURE 7: EXISTING SIDEWALKS

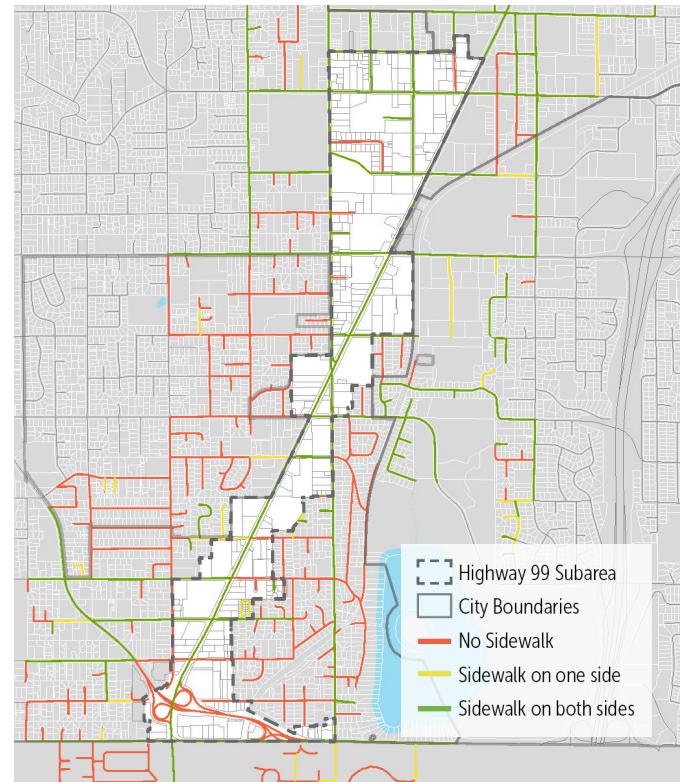


FIGURE 8: CURRENT TRANSIT FREQUENCY

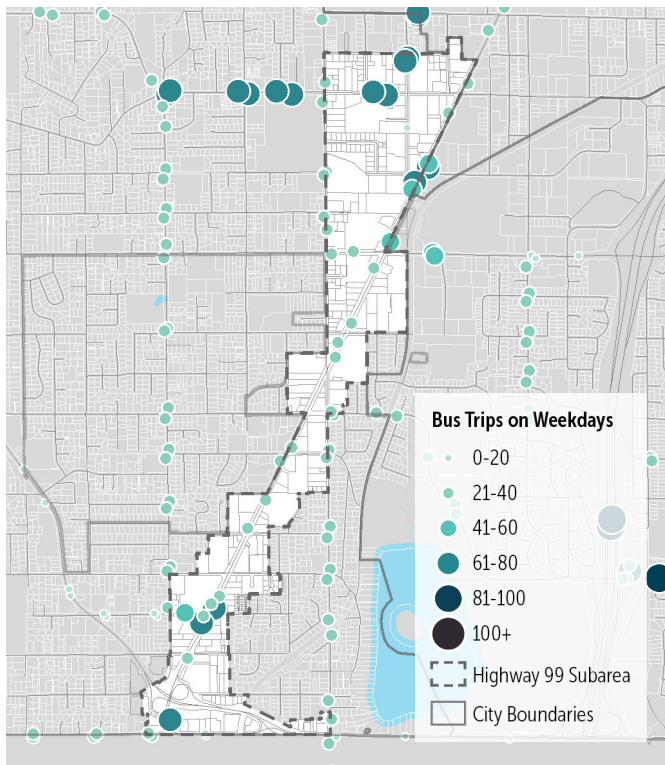
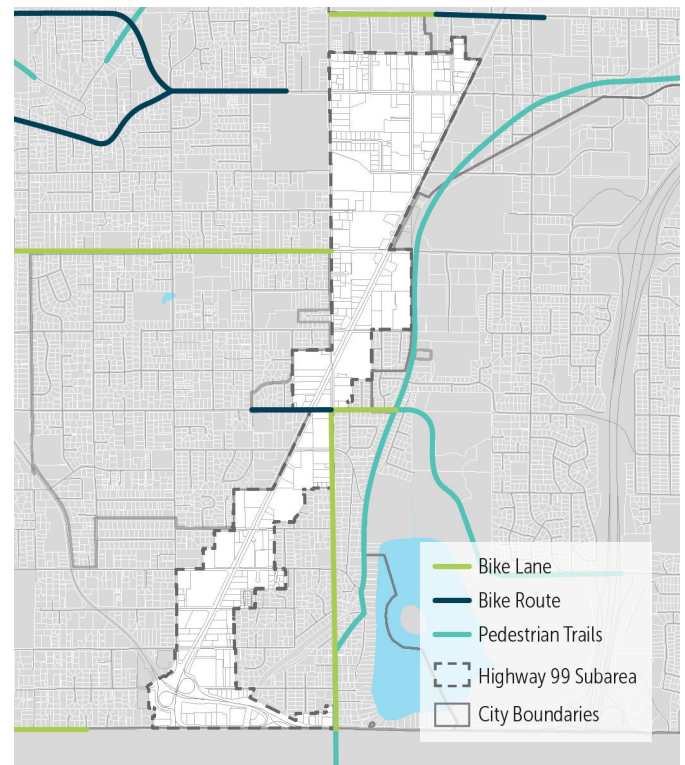


FIGURE 9: EXISTING BIKE AND TRAIL INFRASTRUCTURE



PEDESTRIAN AND BICYCLE FACILITIES

Most area roads have sidewalks, but certain key streets that provide access to and across HWY 99 currently lack sidewalks. Roads that form the border of Edmonds and other jurisdictions, such as 228th west of HWY 99, pose unique problems for improvement and cost sharing. Improvements to substandard streets have been explored and recommendations have been identified in this plan's project list.

Bicycle facilities are limited in the study area, particularly protected bicycle lanes. The proximity to the Interurban Trail, a major regional trail connecting Edmonds to communities north and south, is east of HWY 99 and has limited access from the center of Edmonds to the west. This process examined improved and new bicycle connections through the study area and included several in the recommended project list.

PLANNED TRANSIT SERVICE

A new commuter rail station is planned for nearby Mountlake Terrace. 228th will connect Edmonds and the HWY 99 study area to this new regional transit option. The extension of the Link light rail is scheduled for completion in 2023. This plan has specific recommendations for how to improve linkages and wayfinding between the future rail station and the HWY 99 corridor.

PLANNED TRANSPORTATION IMPROVEMENTS

The City of Edmonds identifies planned transportation improvements surrounding the Highway 99 subarea in their 2015 Comprehensive Plan and in studies conducted for specific operational and safety conditions within the corridor. The 2015 Comprehensive Plan develops a balanced multimodal transportation system to serve the anticipated growth throughout the City and region. Within the Edmonds Highway 99

subarea the planned transportation improvements emphasize access to Highway 99 and connections to the downtown, surrounding residential neighborhoods and major transportation nodes. The improvements fall into several general areas including:

» **Traffic safety and access management improvements:**

This area of improvement involves installation of vehicular channelization and median devices between intersections to separate traffic flows and restrict turns to and from driveways. These devices are intended to reduce crashes by eliminating mid-block vehicular conflicts caused by multiple driveways, minor uncontrolled intersections, and the continuous two-way left turn lane comprising the majority of Highway 99.

A recent example of the implementation of access management is the segment of Highway 99 between 224th Street SW and 228th Street SW where the center median of Highway is a combination of Type-C curbing ("c-curb") and a raised concrete barrier median. This installation prohibits all movements that would normally cross the centerline and provides bi-directionality by allowing u-turns at the signalized intersections at either end of the segment.



Type-C curbing and raised concrete barrier median at intersection of 76th Avenue and Highway 99 between 224th Street and 228th Street

- » **Expansion of the citywide bicycle network:** The City's 2015 Comprehensive Transportation Plan includes proposed bicycle facilities that expand the coverage of the existing bikeway system and connect various parts of the City. Within the plan area the proposed bikeway network emphasizes new east-west crossings of Highway 99 with connections to the Interurban Trail as well as north-south facilities paralleling Highway 99. The proposed bikeway network in the plan area is a combination of bicycle lanes (exclusively marked lanes for bikes) and designated bicycle routes where bicyclists and motorists share travel lanes.

Bike facilities are frequently implemented in stages as part of other roadway improvement projects. For example, bike lanes were added to 220th Street SW between 84th Avenue W and 76th Avenue W as part of a recent pavement overlay improvement of 220th Street SW. In the next stage of bikeway implementation, bike lanes will be added to 76th Avenue W from 220th Street SW to Olympic View Drive.



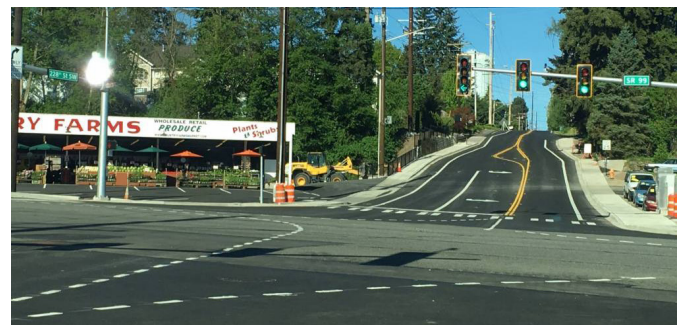
Example of exclusively marked bike lane



Example of bicycle route shared with bicyclists and motorists

- » **Intersection improvements for vehicular, pedestrian and bicycle safety:** This area of improvements focuses on closing gaps in the pedestrian system by constructing sidewalks but also includes reconstructing intersection approaches on streets intersecting Highway 99 to delineate traffic lanes, improve traffic signal phasing, and add bike lanes—all of which are intended to improve safety and also improve multimodal circulation for accessing and crossing Highway 99. These planned improvements are considered "Complete Streets" projects since they emphasize improving conditions for all users.

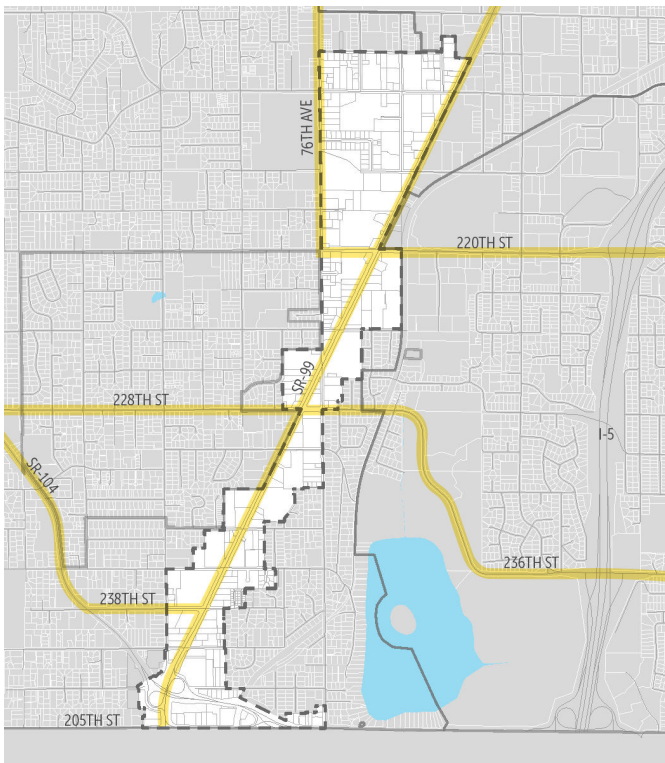
A recent example of this type of improvement was the extension of 228th Street SW from Highway 99 to 76th Avenue W—a short, but strategic, complete street segment that improves pedestrian and bicycle circulation, access to transit, and removes a substantial obstacle to a crucial complete street corridor—228th Street SW/Lakeview Drive—connecting the plan area to Sound Transit's Mountlake Terrace transit terminal and future light rail station.



Recently complete intersection improvements on 228th and Highway 99 to 76th Ave.

- » **Priority Transit Corridors:** Improvements that focus on improving the mobility of transit on key transit corridors, improving pedestrian and bicycle access to transit stops, and enhancing transit stop environments with pedestrian amenities to encourage ridership. Priority Transit Corridors include Highway 99, 220th Street SW, 228th Street SW, and 238th Street SW each of which are currently important routes for Community Transit's local and Swift Rapid routes. The types of improvements in this category such as Transit Signal Priority, improved bus service and route interconnection, accessibility, and improved stops and stations, are cooperative projects between multiple agencies.

FIGURE 10: PRIORITY TRANSIT CORRIDOR



EXISTING ECONOMIC CONDITIONS & MARKET TRENDS

In February 2016, an assessment of the development market and round of developer interviews was conducted. The results of the assessment and the interviews is summarized with the following five points. Further discussion of each is below.

1. Edmonds is a good location for development, but has its challenges.
2. Residential uses will likely be the primary driver of development along the corridor.
3. Parking is an important factor in development form and feasibility.
4. The impact of right-of-way improvements on development was mixed.
5. Development incentives and regulatory process can support (or hinder) redevelopment.

1. EDMONDS IS A GOOD LOCATION FOR DEVELOPMENT, BUT IT HAS CHALLENGES

Edmonds has a positive impression. It's viewed as a desirable community that people want to live and as a place they would consider building. Highway 99 in Edmonds does have some specific challenges, specifically crime, vehicle access, and the lack of large developable sites.



High parking requirements may impede new development

2. RESIDENTIAL USES WILL LIKELY BE THE PRIMARY DRIVER OF DEVELOPMENT ALONG THE CORRIDOR

Apartment projects will likely be the main driver of development along the corridor. Most developers are looking to do sizable projects of 150 units or more, which requires larger sites. Mixed use development is fairly likely although the retail portion may be a drag on the project financially initially. Office development is not likely in the near-term, and demand for medical office use is not clear.



Mixed-use, residential development

3. PARKING IS AN IMPORTANT FACTOR IN DEVELOPMENT FORM AND FEASIBILITY

Accommodating parking is an important factor in redevelopment, particularly in a suburban and auto-oriented environment transitioning to being more dense and walkable. Future projects along the corridor will likely need structured or underground parking to maximize the development potential of a site. The cost of different parking types and site size and characteristics are important variables for parking configuration (surface/tuck-under/underground). The number of spots per unit desired will vary depending on the project, but high minimum requirements and/or inflexible standards can be a significant barrier to new development.

4. THE IMPACT OF RIGHT-OF-WAY IMPROVEMENTS ON DEVELOPMENT WAS MIXED

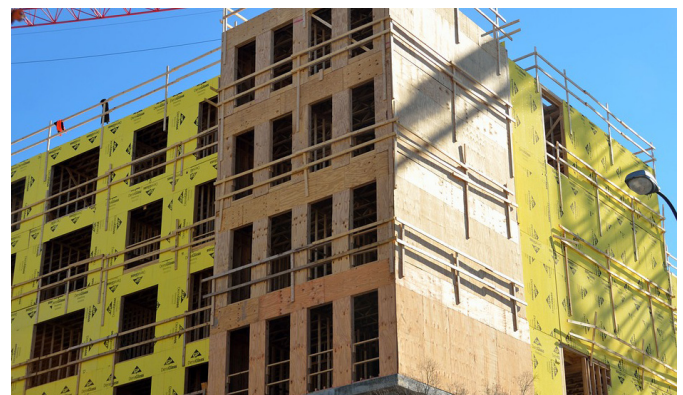
The City of Shoreline has made significant improvements along Highway 99. One interviewee saw the improvements as critical to development. Several interviewees viewed them as nice to have, but not critical. Access, particularly for retail use, is viewed as important and something to be maintained if any improvements are made to Highway 99 in Edmonds.



Right-of-way improvements, like improved sidewalks, will be a critical part of new development

5. DEVELOPMENT INCENTIVES AND REGULATORY PROCESS CAN SUPPORT (OR HINDER) REDEVELOPMENT

The multifamily tax exemption (MFTE) program has been used by a number of developers in communities around the region, and is considered an effective incentive. Permitting and timeliness of the permit process is also considered important for facilitating development.



The permitting process is an important aspect of development.



COMMUNITY + STAKEHOLDER ENGAGEMENT

The Subarea Plan is representative of input gathered from the overall community as well as key stakeholders, and other interested parties throughout the planning process.

The public involvement process involved stakeholder interviews, a Technical Advisory Committee, a community visioning workshop, polling, and two open houses as well as online outreach and surveys. The community's participation during the planning process helped shaped the overarching vision and community values for the Highway 99 area which ultimately guides the recommended strategies in the Plan. The process also helped build public support around the near and long-term approach towards growth and prosperity for the Highway 99 area.

Workshop participants during a design exercise about where they prefer to see different types of improvements in the study area.



Members of the Technical Advisory Committee

City of Edmonds

- » Bertrand Hauss, Transportation Engineer

Community Transit

- » Eric Goodman, Transportation Service Planner
- » Carol Thompson, Director of IT Operations

Sound Transit

- » Patrice Hardy, Government Relations Manager
- » Kathy Leotta, Senior Transportation Planner

Washington State Department of Transportation (WSDOT)

- » Annie Johnson, Transportation Planner
- » Mike Swires, Traffic Engineer

Puget Sound Regional Council (PSRC)

- » Ben Bakkenta, Growth Management Planning Program Manager

TECHNICAL ADVISORY COMMITTEE (TAC)

The committee met 3 times during the planning process. This group consists of representatives from several transit agencies including the Washington State's Department of Transportation (WSDOT), Community Transit, Sound Transit. The TAC also included a representative from the Puget Sound Regional Council (PSRC) and a transportation engineer from the City of Edmonds. In addition, representatives from Snohomish County and the Cities of Shoreline, Mountlake Terrace, and Lynnwood also participated in TAC meetings.

KEY STAKEHOLDER INTERVIEWS

Six focus-group style meetings were conducted, including a round of developer interviews, a meeting with property owners, non-profit organizations, and a representative from the Swedish Edmonds Campus.

COMMUNITY VISIONING WORKSHOP

Over 50 people participated in the visioning workshop in March 2016, which involved a fun and interactive map-based design workshop and live polling questions using keypads that display results from all participants after each question.

Outreach Activity Timeline



Both activities asked participants to envision and share their ideas about what kinds of changes they would like to see in the future, such as commercial, housing, or mixed use development, new pedestrian crossing, safety improvements, new traffic signals, traffic calming measures, wider sidewalks and others.

PUBLIC OPEN HOUSES

Two public open houses were held in May and November 2016 and served as an opportunity to learn about the project, hear from elected leaders, and converse with the project team and share their thoughts on the project. The May open house revealed near and long-term development and transportation scenarios for the study area, and the proposed scope of the Planned Action EIS. The November open house unveiled the draft recommended implementation strategies and before and after visualizations of what life could be like in the Highway 99 area.

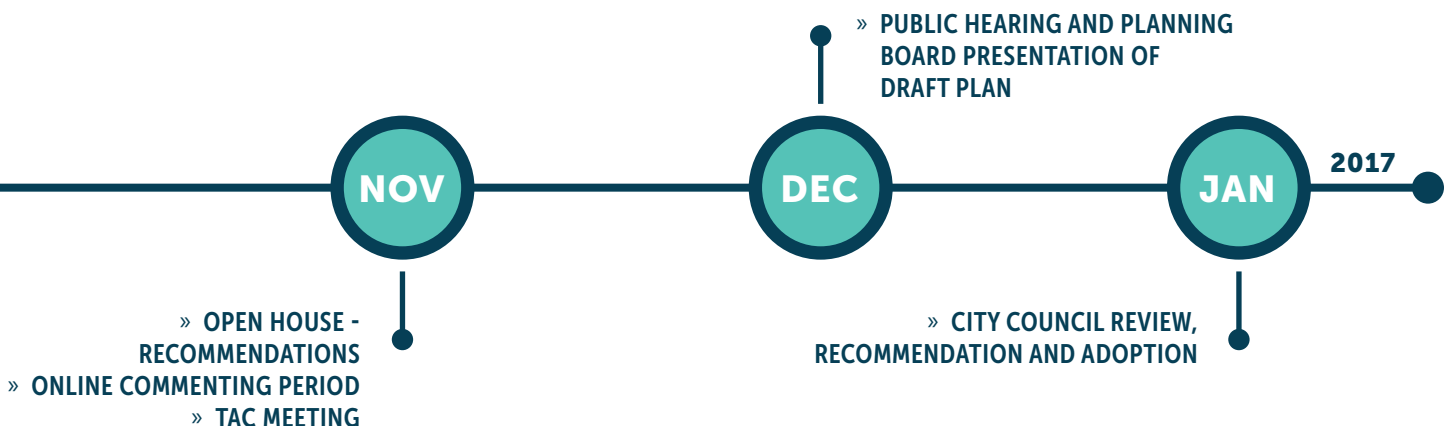
ONLINE SURVEY AND GENERAL COMMENTS

The polling questions presented at the visioning workshop were also available in a survey format on the project website. The survey collected 167 responses and asked participants to share their top

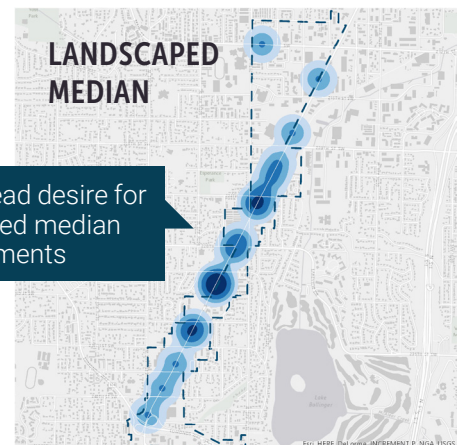
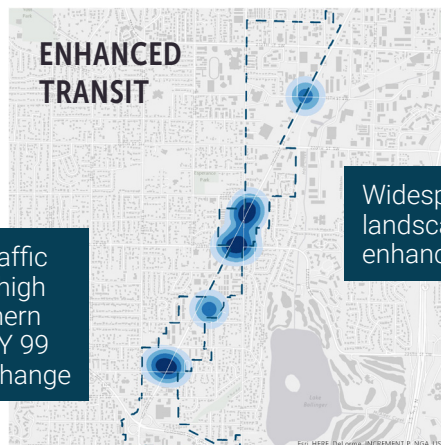
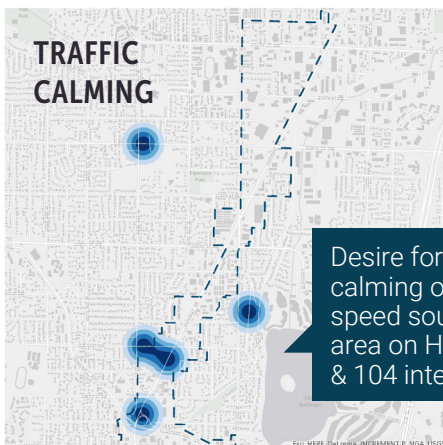
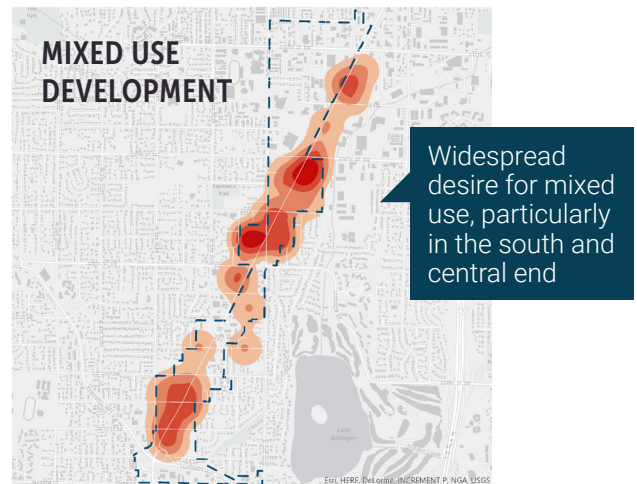
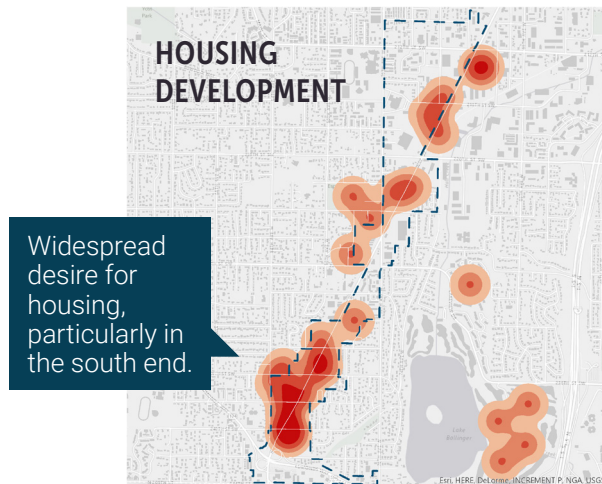
priority of housing, business, and infrastructure. See pages 27-28 for combined results from live polling and survey responses. The public also had opportunities throughout the planning process to share general comments about the near and long-term land use and transportation scenarios, the scope of the Planned Action EIS, and key elements of the implementation strategies and recommendations for the subarea.

WEB OUTREACH AND SOCIAL MEDIA

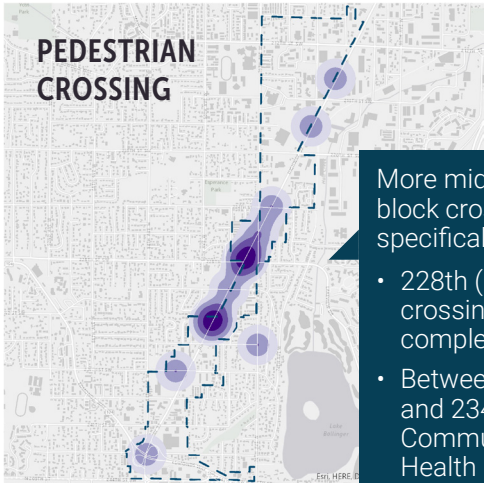
A dedicated project website was created to inform the public about the project and regularly updated with most recent project developments and event announcements throughout the planning process. The website provided an opportunity for those unable to attend the workshop or open houses to provide input and learn about the overall planning process, see results of workshops and surveys, and explore alternative scenarios and implementation strategies. Event announcements were also posted on the City of Edmonds Facebook page and on the City's official website including mailed announcements to over 2,100 addresses in the area.



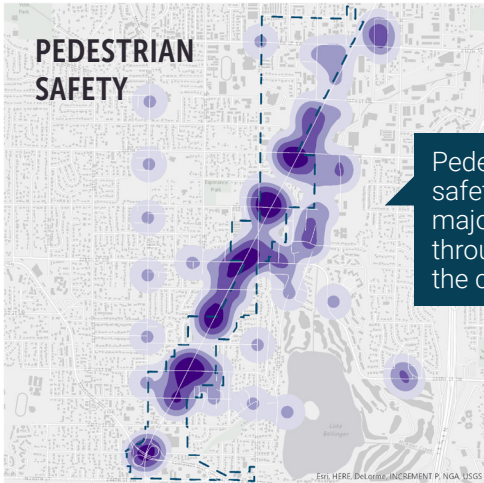
WE ASKED EDMONDS...



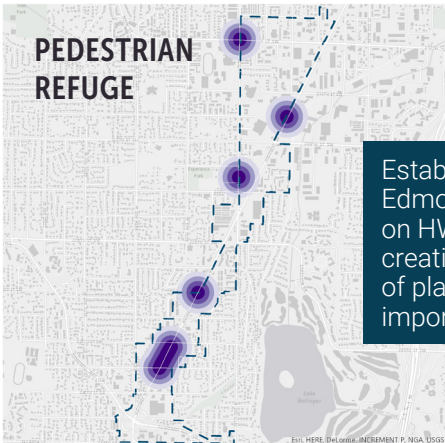
What types of improvements would you like to see happen and where?



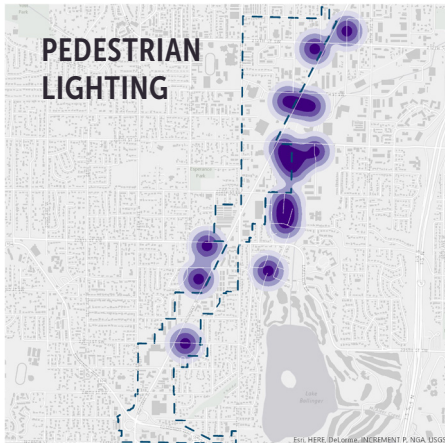
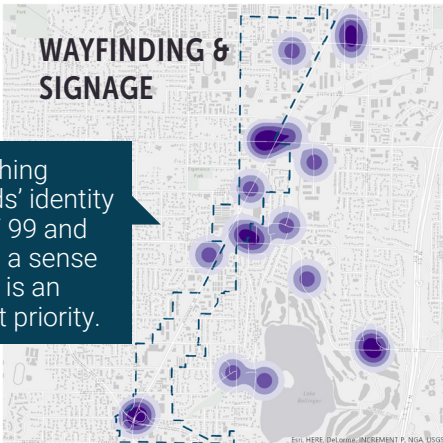
- More mid-block crossing specifically on:
- 228th (new crossing completed)
 - Between 230th and 234th near Community Health Center



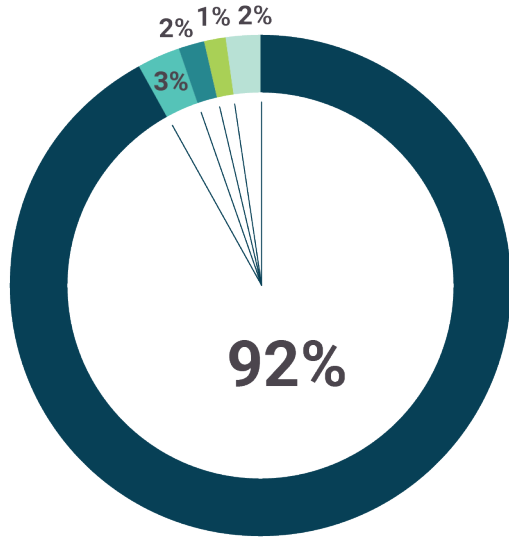
Pedestrian safety is a major concern throughout the corridor



Establishing Edmonds' identity on HWY 99 and creating a sense of place is an important priority.

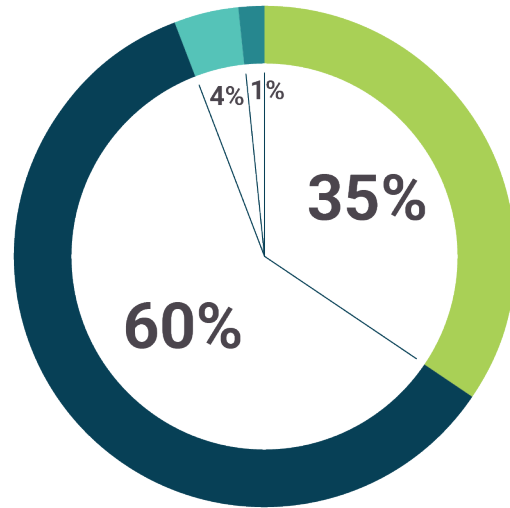


How do you typically travel to work and errands?



- PARK WITHIN SIGHT OF EACH DESTINATION
- WALK, BIKE OR BUS
- PARKING ONCE AND WALK BETWEEN SHOPS
- OTHER / NOT SURE

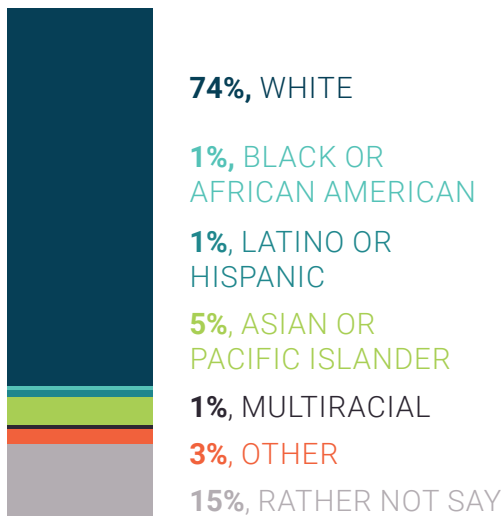
When shopping or running errands, I like to...



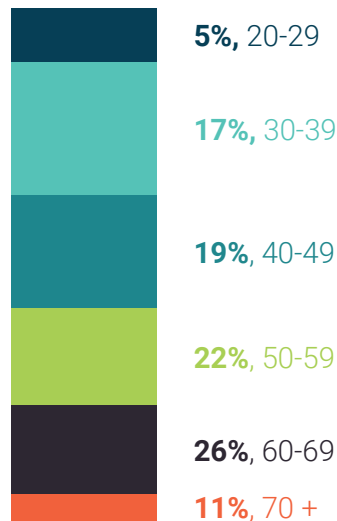
- DRIVE MYSELF
- BIKE
- BUS
- OTHER
- WALK

Demographics of Participants

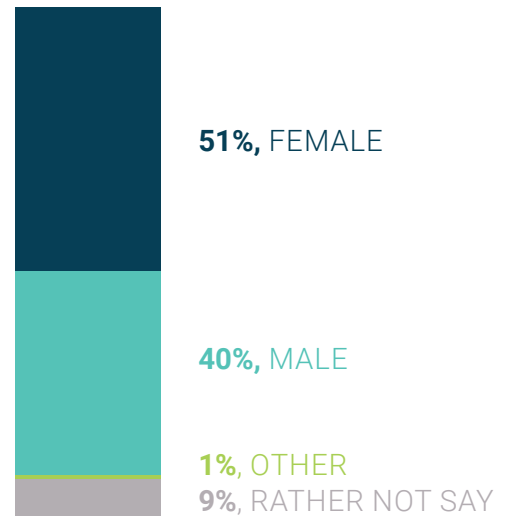
RACE



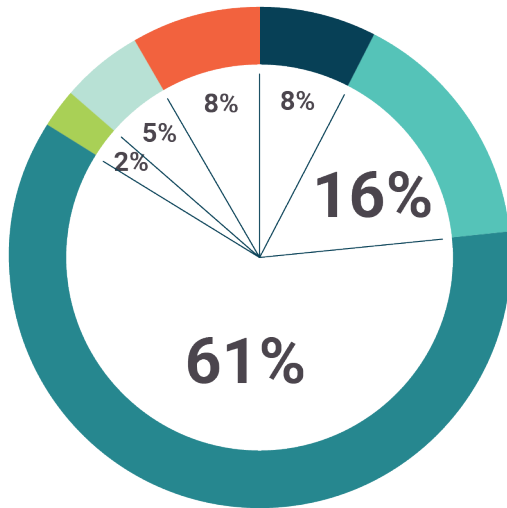
AGE



GENDER



If you could do just one thing for the corridor, what would be your top priority for...

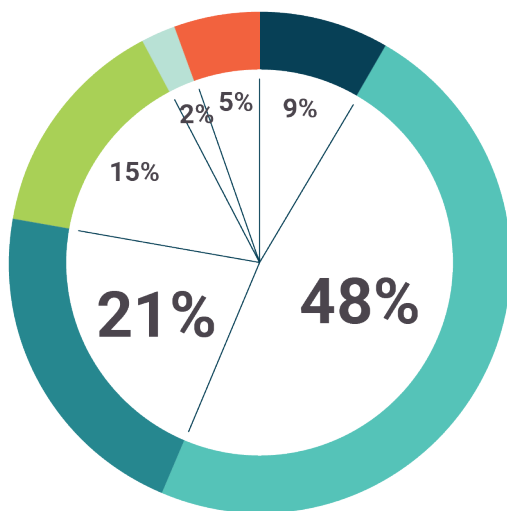
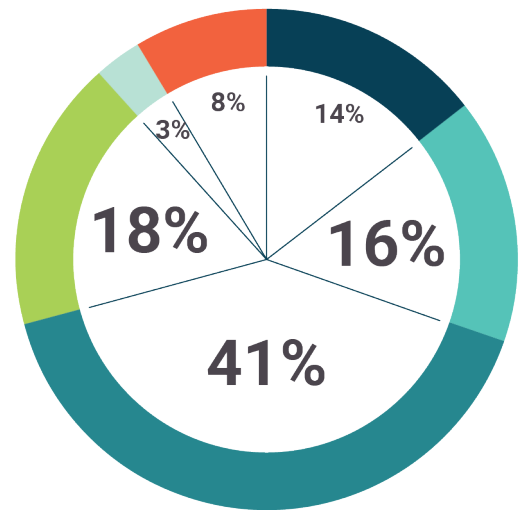


HOUSING

- ADD MORE HOUSING
- ADD MORE AFFORDABLE HOUSING UNITS
- BEAUTIFICATION, BLIGHT REMOVAL, CODE ENFORCEMENT
- HOMEOWNERS ASSISTANCE FOR REHABILITATION
- NO CHANGE
- SOMETHING ELSE

BUSINESS

- CATALYST DEVELOPMENT ON MAJOR SITES
- STOREFRONT IMPROVEMENTS
- BRING NEW BUSINESS AND JOBS TO THE AREA
- INVEST IN HOMEGROWN ENTREPRENEURS
- NO CHANGE
- SOMETHING ELSE



INFRASTRUCTURE

- ADDRESS PARKING - MANAGEMENT AND SHARE
- PUBLIC SPACE IMPROVEMENTS AND BEAUTIFICATION
- SAFETY IMPROVEMENTS FOR BIKES OR PEDESTRIANS
- BETTER CONNECTIONS FOR BIKES AND PEDESTRIANS
- NO CHANGE
- SOMETHING ELSE



CONSTRAINTS + CHALLENGES

LAND USE CONSTRAINTS & CHALLENGES

One issue the corridor needs to overcome is a public perception as being unsafe, unattractive and undesirable. It is sometimes viewed as a leftover area of the city and is not inspiring. Citizens often pass through the area on their way to somewhere else. There are misperceptions of what the corridor is and what it could eventually become.

Highway 99 is a wide auto-oriented regional thoroughfare; this type of road design can be challenging when attempting to create a more walkable and safe area. There are also many land owners in the area and some of the parcels are oddly shaped or have poor access to Highway 99. On top of that, compared with other communities in the region, potentially restrictive land use and parking regulations complicated future opportunities. These challenges can make it more difficult to design and build a viable development project.

Key Takeaways: Constraints + Challenges

Land Use

- » Public perception of Highway 99 as a "pass-through" area and an unsafe, unattractive, and undesirable area
- » Wide, auto-oriented thoroughfare
- » Oddly-shaped parcels with poor access
- » Restrictive land use and parking regulations

Economic

- » Limited retail trade area
- » Lack of diverse housing choice and commercial space

Transportation and Infrastructure

- » Commute patterns of resident and outside labor force can cause traffic and safety issues
- » Poor or lack of safe pedestrian crossings, traffic lights, and sidewalks

ECONOMIC CONSTRAINTS & CHALLENGES

One challenge for the area is the limited retail trade area due to geographical constraints with Puget Sound to the west and Interstate 5 a short distance to the east. Several distinct major regional retail centers and lifestyle centers in nearby communities also compete with this area.

Lack of housing choice can also be challenging. A wider variety of housing options and commercial space is available elsewhere in the region. The lack of housing variety has led to a shortage of affordable housing opportunities as well. The lack of commercial space may allow for retail “leakage,” which is the loss of potential local sales activity to areas outside of the corridor and city.

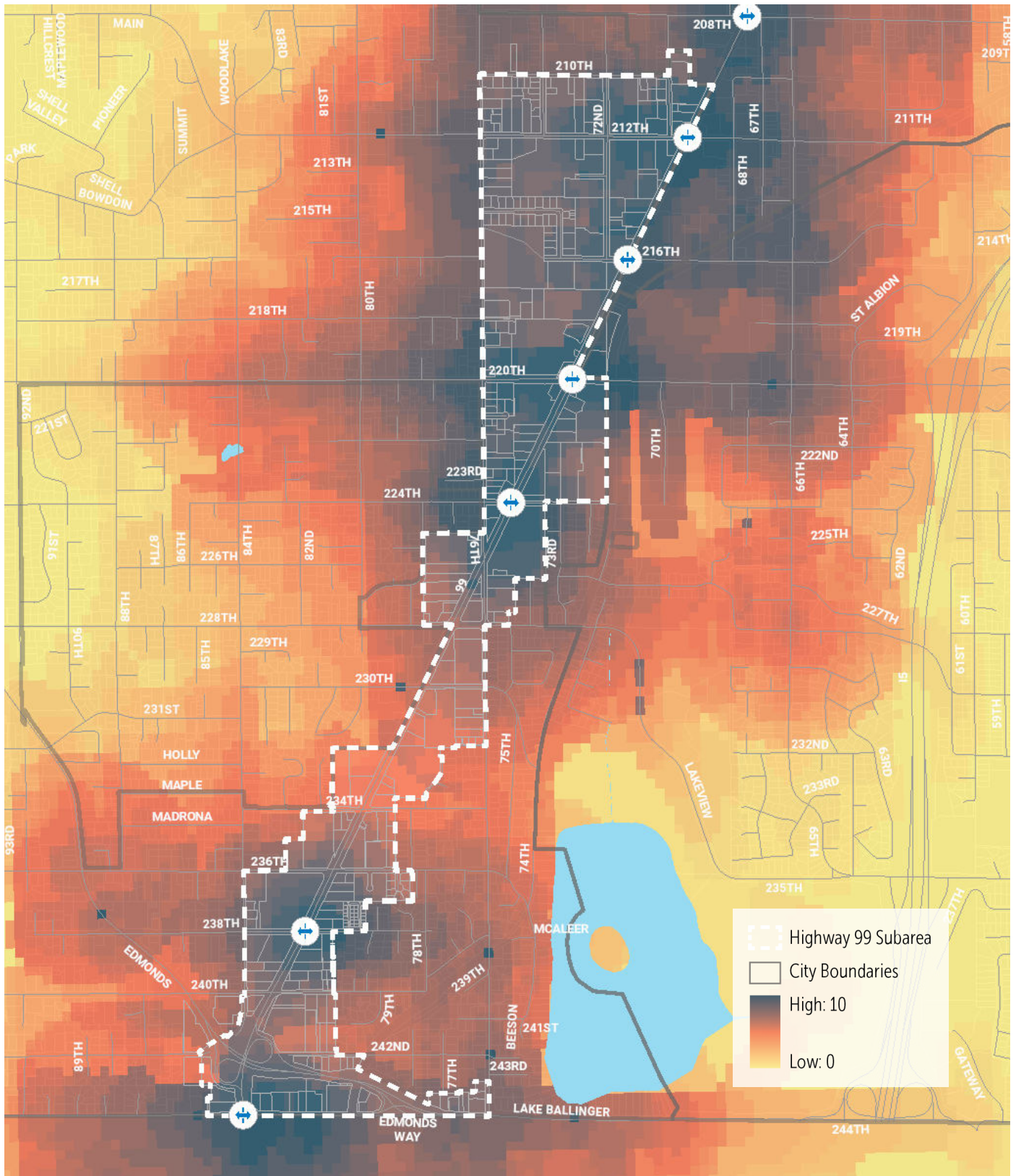
TRANSPORTATION AND INFRASTRUCTURE CONSTRAINTS & CHALLENGES

A substantial portion of the resident labor force commutes away for work, while large number of workers commute into town for work. This can create traffic and safety issues along the corridor.

Safe pedestrian crossings are presently inadequate. Many places where pedestrians want to cross the highway have no marked pedestrian crossing. This is particularly occurring in the southern section of the corridor. There are also long segments without any street crossings or traffic lights. For example, the central section of the corridor requires a 10-minute walk to find a safely marked crossing. Finally, sidewalks are limited along the stretch on the corridor and are not present everywhere.



FIGURE 11: SAFETY HEAT MAP WITHIN THE STUDY AREA





OPPORTUNITIES

LAND USE OPPORTUNITIES

Because of a long history of auto-oriented design along the Highway 99 corridor there are many opportunities for major land use changes.

Three specific locations on the corridor already have reasonably good urban form and include the area east of Highway 99 between 238th and 240th, the area east of Highway 99 at 228th and the area just to the north of the Swedish Medical Center along 212th. These three areas are considered to have good urban form because they currently have well-marked crossings, are near frequent transit service, have city block and lot sizes that are conducive to walking, and have a large amount of employment activity.

This area has a mix of uses, including retail, office, medical, and residential. It is an urban center of Edmonds and part of a larger regional hub.

Key Takeaways: Opportunities

Land Use

- » Good urban form and a mix of uses already exist on the corridor
- » Many opportunities exist for reinvestment, redevelopment, and increased density
- » Transit-oriented development, including affordable housing, can complement the area's transit system

Economic

- » Strong health services sector and International District
- » Existing and planned transit connections creates opportunities for transit-oriented development and a stronger business and employee base
- » Retail uses, including auto sales, that provide tax revenue for public services

Transportation and Infrastructure

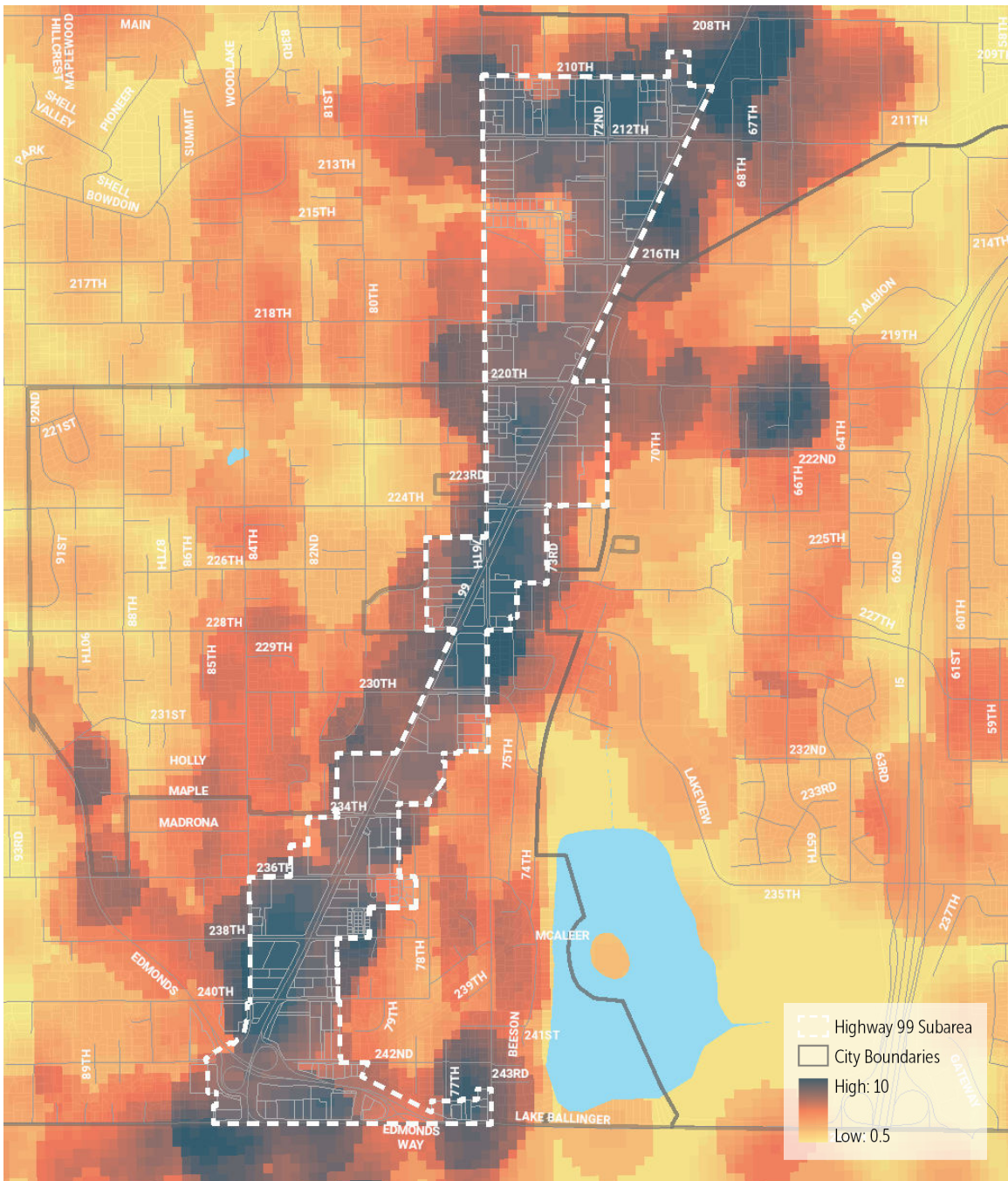
- » Providing a complete transportation system with efficient and cost-effective accessibility and mobility
- » Providing safe and convenient access all throughout the subarea especially for pedestrians

OPPORTUNITIES

Although some areas have good urban form, there are also opportunities for better integrated land uses along the corridor. Many parcels are less than 25% covered with buildings, presenting an opportunity for redevelopment and increased density in the area. In addition, much of the buildings are between 25 and 60 years old and are nearing the end of their functional lifespan. Just a

few new buildings or historic buildings along the corridor are expected to remain the same for the foreseeable future. Furthermore, there are many low-to-moderate value buildings and just a few new, higher value buildings in the area. This means that there is great potential for reinvestment and redevelopment along the corridor.

FIGURE 12: URBAN FORM WITHIN STUDY AREA



ECONOMIC OPPORTUNITIES

The service sector will continue to dominate the Edmonds employment base, with continued growth expected, especially in the health care sector. The burgeoning health services sector, anchored by Swedish-Edmonds Hospital is a perfect example. The Highway 99 corridor near the medical services node around the hospital offers opportunities for additional hospitality facilities.

Other opportunities include leveraging the “International District” to provide culturally-specific goods and services. The corridor already has a strong international business community, offering diverse array of goods and services.

A substantial number of jobs in Edmonds are occupied by residents of other outside communities. Additional housing variety in the area with a greater distribution along the affordability spectrum could help capture the latent housing demand of many Edmonds workers.

Existing and planned intermodal transit connections can leverage transit-oriented development. Also, additional population density in business districts can add market demand for goods and services and employee base for new and growing businesses.

New business and job opportunities are largely brought to the corridor through new development and redevelopment. Appropriately sited and sized development and redevelopment projects will increase:

- » Property tax receipts through the new construction provision that captures new construction value-based property tax for the first year a project is brought on line and adds that value to the city’s future property tax baseline.
- » Sales tax revenue from construction materials and activity.
- » Sales tax revenue from both personal and business spending accruing from new residents, workers and businesses within newly developed buildings.
- » Utility tax revenue from a greater number of utility customers.

FIGURE 13: LAND VALUE PER SQUARE FEET

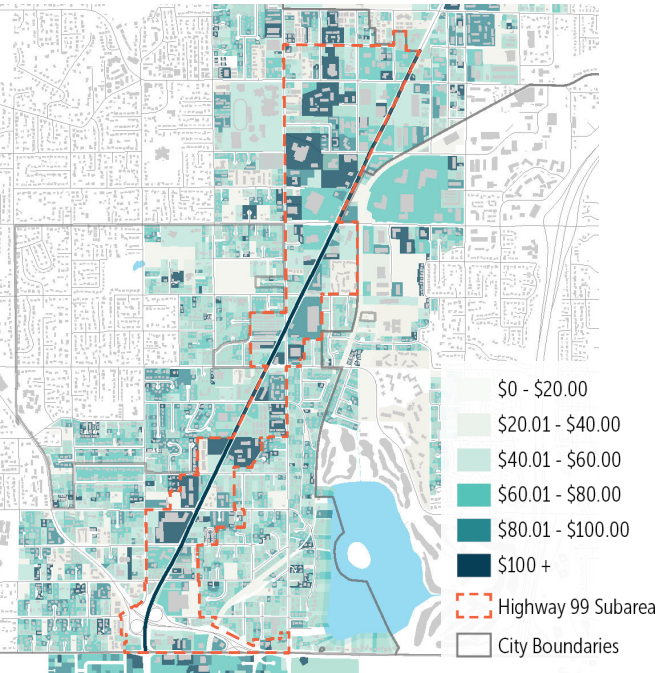
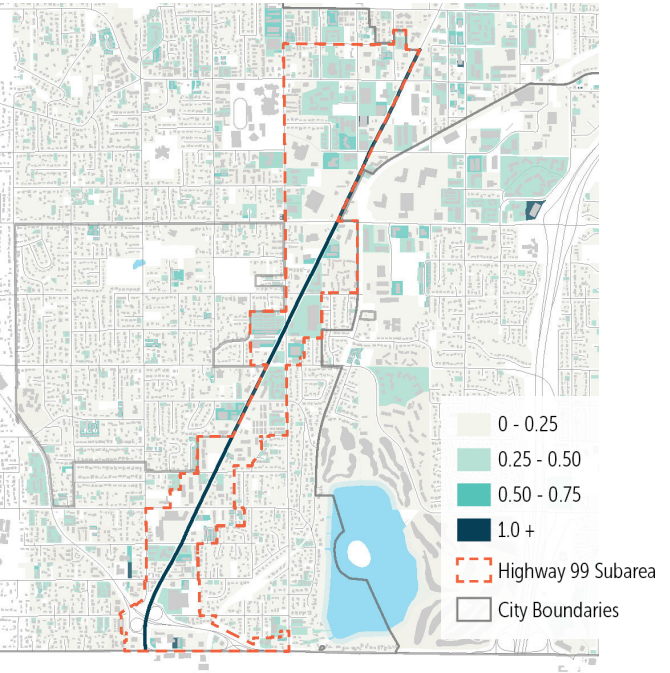


FIGURE 14: FLOOR AREA RATIO



TRANSPORTATION & INFRASTRUCTURE OPPORTUNITIES

The HWY 99 corridor is unique in that it is served by regional Bus Rapid Transit (BRT), local public transportation, and has access to commuter and express transit services to Seattle from the Mountlake Terrace I-5 Station which will offer light rail transit service with the Lynwood Link Extension planned to open in the year 2023.

The level of transit coverage within the Plan Area supports transit-oriented-development and attracts households with zero to low automobile ownership—a segment of the population that choose to reside near transit because they don't drive or don't want to encumber themselves with the cost of vehicle ownership. One of the characteristics of HWY 99 that helps BRT achieve its desirable rapidness—long distances between signal controlled intersections—is also one of the greatest impediments to pedestrian circulation. Despite the apparent conflict in functionality, the HWY 99 corridor presents opportunities to maintain the short travel times needed for an effective Swift rapid transit system while providing additional safe pedestrian crossings of HWY 99 and improving the overall multimodal connectivity of the Plan Area.

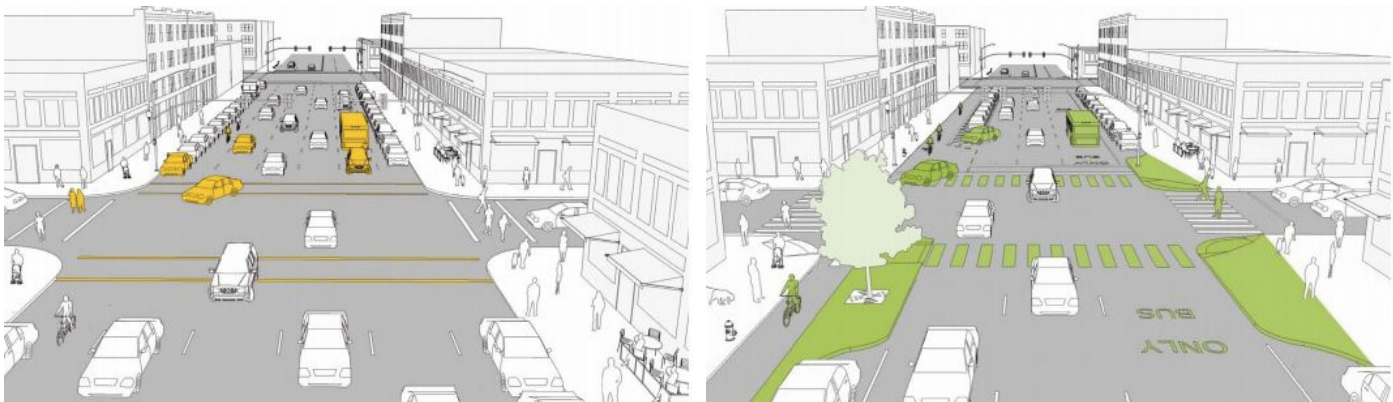
Opportunities for maintaining a rapid transit system include:

- » Implementing a Transit Signal Priority system that allow BRT vehicles to trigger a change in traffic signal phasing in favor of the buses approaching a signalized intersection.
- » Strongly enforcing the corridor's BAT lanes and improving their effectiveness through better access management and their use as queue jumping lanes.

Opportunities for improving pedestrian safety, circulation options, and access to transit include:

- » Transforming the approaches of streets intersecting HWY 99 into "Complete Streets" that improve the environment for all users to access and cross the corridor. Complete Street improvements might trade-off automobile travel lanes to create space for sidewalks and bike lanes, or might add a vehicular turning lane allowing for protected signal movements that eliminate conflicts with crossing pedestrians.
- » Strategic placement of new traffic signals and pedestrian crossings that break up the longest segments of HWY 99 without safe crosswalks.
- » Reconfigure high-speed corners and ramps to slow traffic and install pedestrian-activated flashing beacons at uncontrolled crossings.

FIGURE 15: BEFORE AND AFTER ILLUSTRATIONS OF EXAMPLE "COMPLETE STREET" IMPROVEMENTS



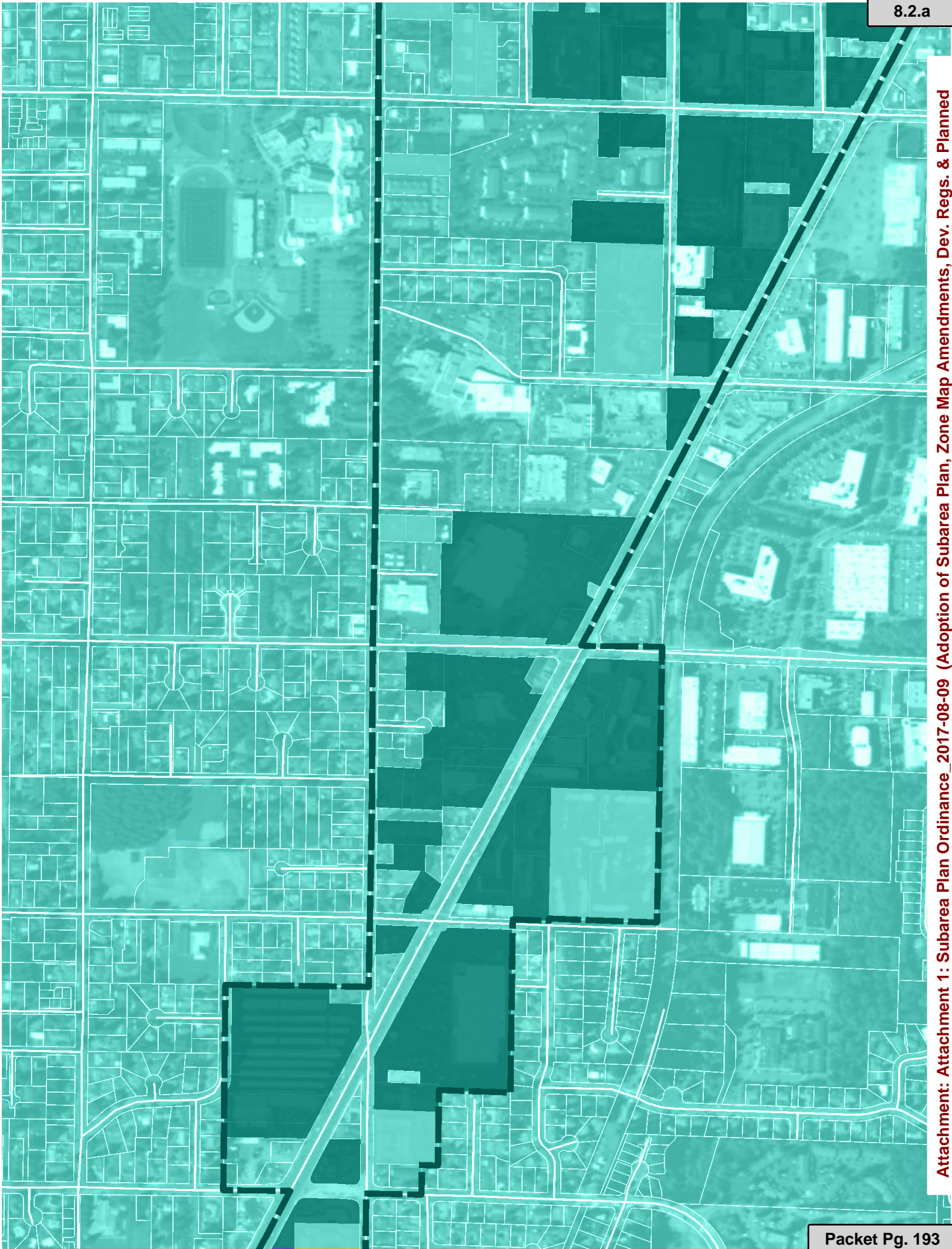
Source: CityLab, The Atlantic

Transportation improvements within the Plan Area should take advantage of the opportunities provided by key assets within the Plan Area.

- » The Interurban Trail is a key asset in the corridor. Improving access to the trail with short and strategically located pedestrian and bicycle-only connections from HWY 99 can increase the trail’s usefulness for shorter trips as well as longer distance travel.
- » Capitalize on the successful extension of 228th Street to break up large blocks and create more complete street connections to Edmond’s designated Priority Transit Corridors to provide better pedestrian access to transit stops and improve overall mobility.
- » Encourage transit use by enhancing transit stops in Priority Transit Corridors with amenities that make stops attractive, comfortable, and safe for waiting passengers.



The Interurban Trail



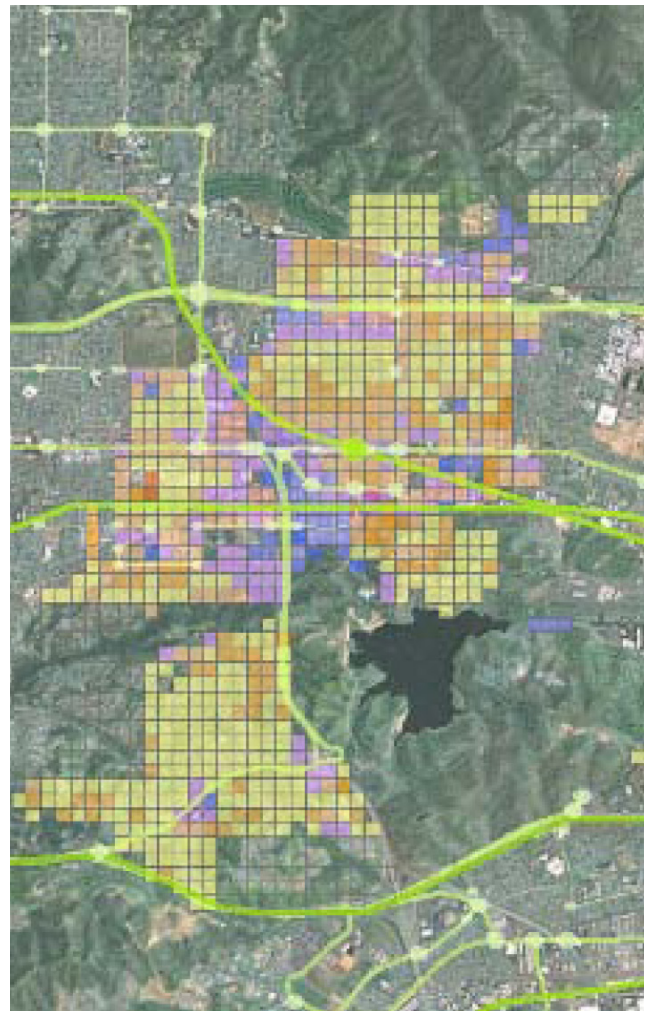
ALTERNATIVE SCENARIOS

LAND USE AND TRANSPORTATION SCENARIOS

Land use and transportation scenarios are an important part of the exploratory process in planning. Testing a range of policy options, development types and transportation improvements allows for a comparison of the relative strengths and weaknesses of virtual futures.

Two separate land use and transportation scenarios were evaluated within the HWY 99 corridor before landing on a final preferred alternative. The scenarios were tested using the open source scenario planning platform Envision Tomorrow.

Envision Tomorrow is a suite of planning tools that includes analysis and scenario design applications. The analysis tools allow users to analyze aspects of their current community using commonly accessible GIS data, such as tax assessor parcel data and Census data. The scenario design tools allow users to digitally map alternative future



Sample scenario

development scenarios on the landscape, and compare scenario outcomes in real time for a range of measures from public health, fiscal resiliency and environmental sustainability.

The location and styles of development that were tested came from public input through the workshop process and the existing conditions analysis of redevelopment potential. The transportation components of the scenarios were a combination of public input from the workshops, and existing projects in previous plans.

SCENARIO BUILDING BLOCKS

Each of the scenarios was constructed using a range of building types calibrated to the Edmonds market. Within a context such as the HWY 99 study area, a range of buildings could be anticipated. However, existing roadway conditions and regulatory requirements have precluded the

development of the mixed-use and residential building types desired by the public – predominantly three and four story apartments, and five and six story mixed-use buildings.

Within the current context of the area, land developers are unable to achieve rents high enough to make these buildings feasible. However, with investments into roadway improvements and regulatory changes, such as a reduction in parking requirements, the market becomes much more desirable. More pedestrian-oriented road conditions and development make the area more attractive to potential tenants, resulting in higher achievable rents, and therefore greater market feasibility for the type of development under consideration. This relationship between transportation, land use and the development market is well documented in Reid Ewing and Keith Bartholomew's research into Hedonic Price effects of Pedestrian- and Transit-Oriented Development (2011).

TABLE 5: BUILDING TYPE CHARACTERISTICS

BUILDING CHARACTERISTICS	THREE-STORY APARTMENTS	SIX-STORY MIXED-USE RESIDENTIAL	SIX STORY MIXED-USE OFFICE	TEN-STORY MIXED-USE OFFICE AND RESIDENTIAL
Parking ratios	0.75 spaces per dwelling unit	<ul style="list-style-type: none"> » 0.75 spaces per dwelling unit » No parking required for first 2,000 sq ft. commercial » 2.0 spaces per 1,000 sqft above 2,000 	<ul style="list-style-type: none"> » No parking required for first 2,000 sq ft. » 2.0 spaces per 1,000 sqft above 2,000 	<ul style="list-style-type: none"> » 0.75 spaces per dwelling unit » No parking required for first 2,000 sq ft. commercial » 2.0 spaces per 1,000 sqft above 2,000 commercial
Housing density (dwelling units per acre)	51.2	82.6	-	49.02
Employment density (jobs per acre)	-	33.6	208.5	208.98
Average dwelling unit size in square feet	850	759	-	759
Average rent	\$1,700 / unit	\$1,669 / unit	<ul style="list-style-type: none"> » \$22 / sqft retail » \$26 / sqft office 	<ul style="list-style-type: none"> » \$1,669 / unit » \$22 / sqft retail » \$26 / sqft office
Achievable land cost per square foot	\$41-\$49	\$80-\$94	\$36-\$43	\$70-\$79

Given the assumption that these investments and regulatory changes would take place, four primary building types came forward as the most likely to occur in the HWY 99 study area: three-story apartments, six-story mixed-use residential or office, six-story mixed-use office, and ten-story mixed-use office and residential in which the mixed-use buildings would include ground floor retail and service uses with either residential or office on the upper floors. These building types were used to construct the HWY 99 scenarios, and Table 5 summarizes the building characteristics of the four building types.

SCENARIO ALTERNATIVES

The power of scenario analysis lies in the ability to test out and compare different potential futures. The alternatives considered in the analysis include No Action (Alternative 1) and the Preferred Alternative (Alternative 2).

SCENARIO ALTERNATIVE 1: NO ACTION

Under Alternative 1, future growth would continue based on existing development regulations and past development trends. Operating under the assumption of existing roadway conditions and regulatory requirements, the development of higher intensity mixed-use and residential buildings proved unfeasible, resulting in the lowest potential for new housing and population growth in the corridor. Accordingly, commercial development would continue to be the primary use along the corridor.

SCENARIO ALTERNATIVE 2: PREFERRED ALTERNATIVE

Public feedback expressed a desire for a dual emphasis of both housing and employment, resulting in an area characterized by mixed-use development with an increase in residential development, greater intensity of development, and street-frontage and pedestrian amenities. Higher building intensity was focused in the high-rise



Three-story apartment building type



Six story mixed-use building type with ground floor retail and residential above



Ten-story mixed-use building type with ground floor office and residential above

node surrounding the Swedish Edmonds Campus, bringing a broader range of uses to the district, and the highest growth potential.

The Preferred Alternative assumes mixed use growth that is more balanced between residential, commercial, and office uses, an area-wide rezone, amendments to development regulations, and enhanced transportation improvements. Transportation improvements to the Highway 99 corridor and adjacent local streets would include measure to maintain level of service standards, increase east/west connectivity, provide greater bicycle and pedestrian mobility, and improve access to transit.

ALTERNATIVE SCENARIOS

TABLE 6: EXISTING AND PLANNED ACTIVITY UNITS

	HWY 99 CORRIDOR TOTAL ACRES	TOTAL ACTIVITY UNITS	ACTIVITY UNITS / ACRE	POPULATION	POP/ ACRE	JOBS	JOBS/ ACRE	HOUSING UNITS	HOUSING UNITS / ACRE
Existing Conditions	352.55	9669	27.4	5,872	16.65	3,797	10.77	1,579	4.47
Alternative 1 (No Action)	352.55	13,226	27.5	7,112	20.17	6,114	17.34	2,803	7.95
Alternative 2 (Preferred Alternative)	352.55	15,999	45.4	9,189	26.1	6,810	19.3	4,904	13.9

FIGURE 16: ALTERNATIVE 1 (NO ACTION)

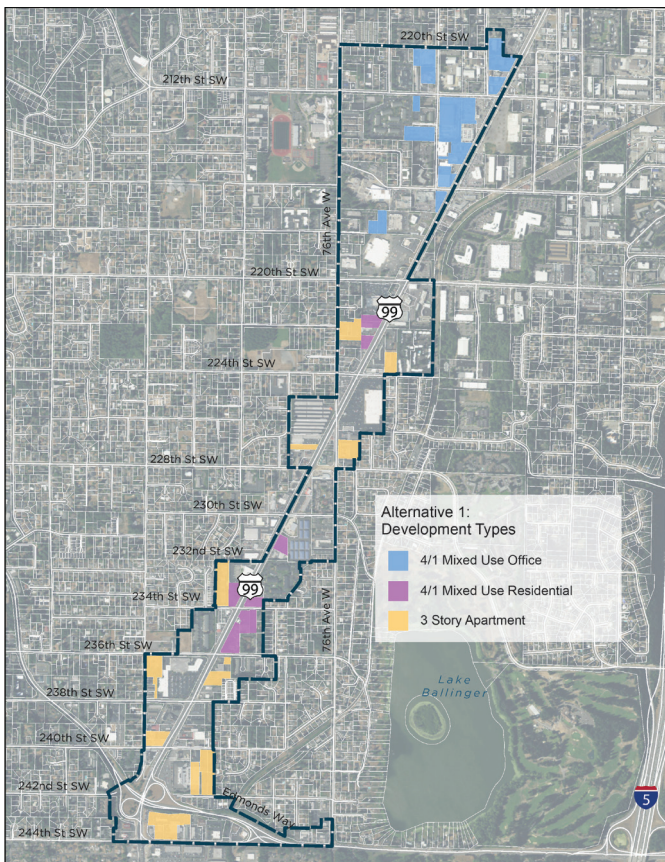
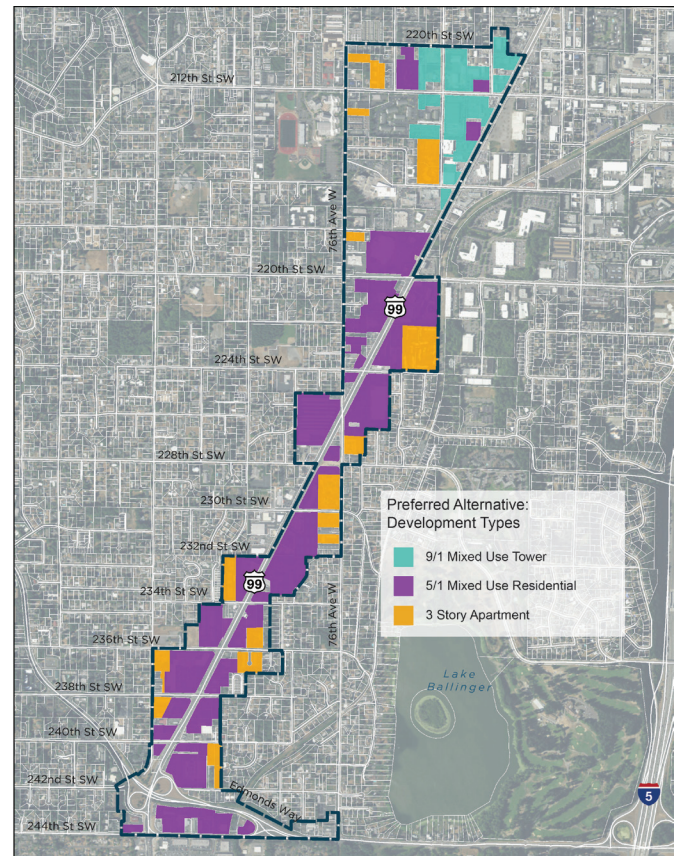


FIGURE 17: PREFERRED ALTERNATIVE (MIXED USE)



DEVELOPMENT CAPACITY ANALYSIS

As part of the subarea planning process, a maximum development capacity number was estimated by Fregonese Associates using the Envision Tomorrow model. In general, existing development does not fully utilize the development capacity available under current zoning, as much of the existing development in the study area is low-intensity and low-scale, with the medical uses surrounding the Swedish Edmonds Campus being the exception.

Because current zoning in the study area allows for a mix of uses, maximum development capacity was estimated for two alternatives. Under the first alternative, future growth would continue based on existing development regulations and past development trends. Alternative 2, which is the preferred alternative, assumes future mixed

use growth with a mix of residential, commercial, and office development, an area-wide rezone, amendments to development regulations, and transportation improvements. Table 7 shows the maximum development capacity numbers in comparison to existing development in the study area and the preferred alternative. Development capacity estimates are not a prediction that a certain amount of development will occur or when it may occur, but instead a measure of the maximum development that could occur in a given area. As Table 7 shows, estimated development capacity is significantly greater than the sum of existing and new growth planned under the alternatives and indicate sufficient development capacity in the study area to accommodate growth under the alternatives.

TABLE 7: DEVELOPMENT CAPACITY ANALYSIS

	HOUSING	JOBS
Maximum Development Capacity ¹	18,450	26,028
Existing Development	1,579	3,797
2035 GROWTH TARGETS (NEW JOBS & HOUSING)		
Alternative 1 (No Action)	1,224	2,317
Preferred Alternative (Mixed Use, High-Rise Node)	3,325	3,013
2035 TOTAL DEVELOPMENT ESTIMATE		
Alternative 1 (No Action)	2,803	6,114
Preferred Alternative (Mixed Use, High-Rise Node)	4,904	6,810
RATIO OF MAX. DEVELOPMENT CAPACITY TO 2035 TOTAL DEVELOPMENT ESTIMATE		
Alternative 1 (No Action)	6.6	4.3
Preferred Alternative (Mixed Use, High-Rise Node)	3.8	3.8

1: Assumes all parcels are developed to the maximum extent allowed under current zoning, with a relatively balanced mix of jobs and housing growth. It is not expected that the study area will completely redevelop to the maximum allowable extent.



IMPLEMENTATION STRATEGIES, POLICY RECOMMENDATIONS + ACTIONS

The most important part of a plan are the actions that are taken to achieve the vision identified by the plan.

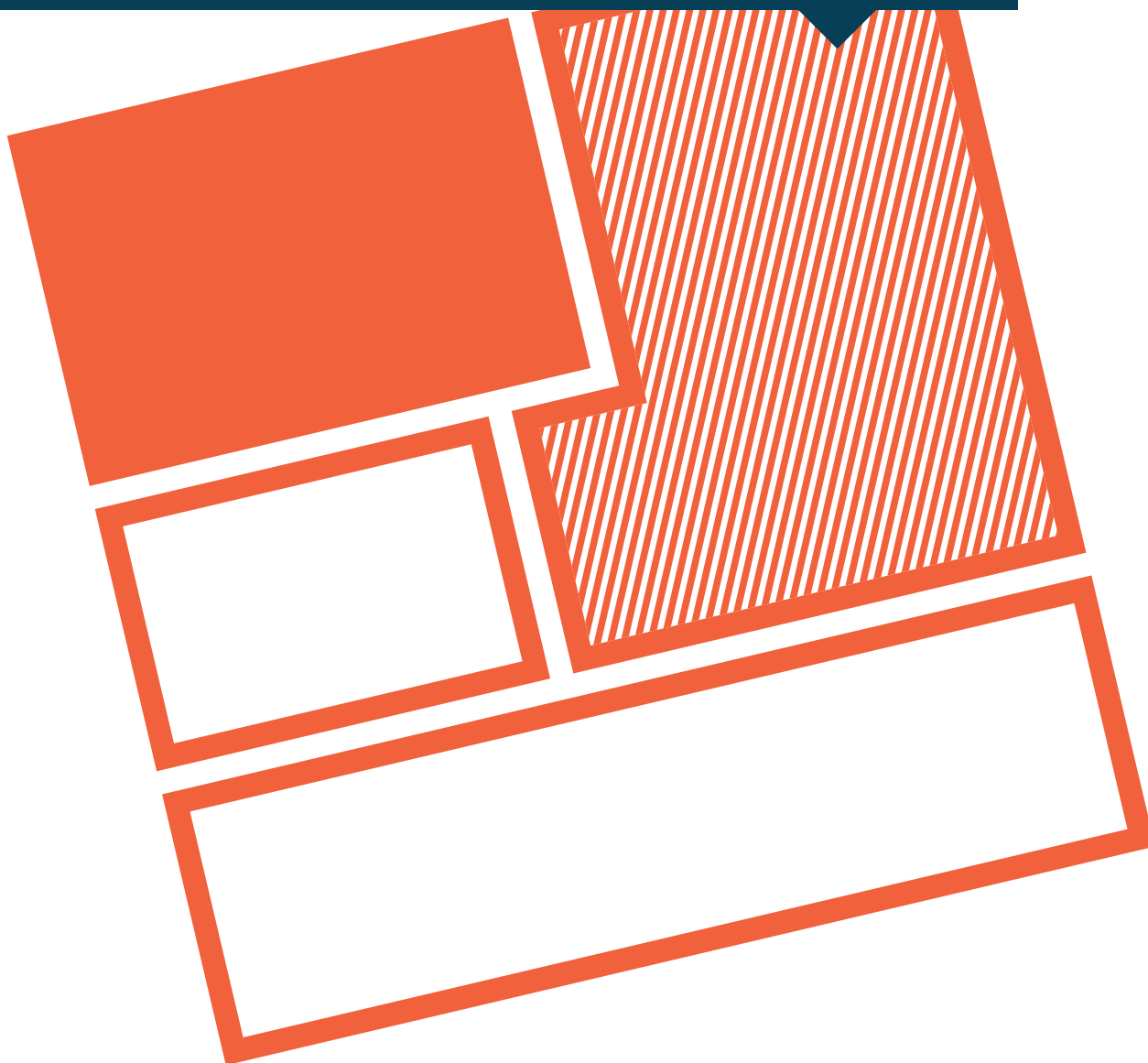
As this plan is very action-oriented and strategic, the implementation strategies focus generally on aligning the City's planning and regulatory structure to support the vision, and to identify those public and private investments that will lead to the vision's realization. The implementation strategies can be generally categorized as:

- » Identification of actions to support public and private investment, including recommendations to amend Edmonds land use and transportation policies and regulations for the entire 99 Subarea
- » Specific land use and transportation strategies, actions, policies and investments
- » Specific recommended changes to zoning and development standards
- » Timing and priority actions - organized into short, medium and long term action items
- » Matrix of potential capital improvement projects along with preliminary costing and relevant partner agencies



Artist's rendering of what future development could potentially look like on the northwest corner of Highway 99 and 234th intersection.

ZONING & DEVELOPMENT RECOMMENDATIONS



1

STRENGTHEN ECONOMIC OPPORTUNITY

RECOMMENDATION 1.1

Support the unique business clusters within the corridor such as the International District and the Health District which are major centers of employment and a regional draw.



RECOMMENDATION 1.2

Major auto sales facilities are also important to the local economy. Design standards, specifically the pedestrian activity zone, will allow auto sales to continue use of this zone.



RECOMMENDATION 1.3

The City should proactively work to strengthen and continue support for business organizations such as the Edmonds Chamber of Commerce, the Edmonds Community College Small Business Development Center (SBDC) and Business Training Center, Sustainable Edmonds, and other organizations in the county and state.

RECOMMENDATION 1.4

Continue to pursue expanded broadband internet within the corridor to make the location attractive to high-tech business investment.

RECOMMENDATION 1.5

Consider unique designs for streetscape improvements in the area, such as unique signage and lighting.

2

ENCOURAGE SUSTAINABLE BUILDING PRACTICES

RECOMMENDATION 2.1

Transit- and pedestrian-friendly development, with less reliance on individual automobile-driving, should be promoted through new design standards to increase sustainability. Recently adopted citywide requirements for new development to have greater energy-efficiency and more effective stormwater facilities will also contribute to sustainability.



RECOMMENDATION 2.2

In addition, the City should consider requiring electric vehicle charging facilities, especially for new development with residential uses, and bicycle facilities, along with options for car-sharing.



RECOMMENDATION 2.3

The use of solar panels and green building practices, even beyond current standards, should be encouraged and incentivized.



3

MODIFY MAP IN COMPREHENSIVE PLAN TO IDENTIFY DISTRICTS

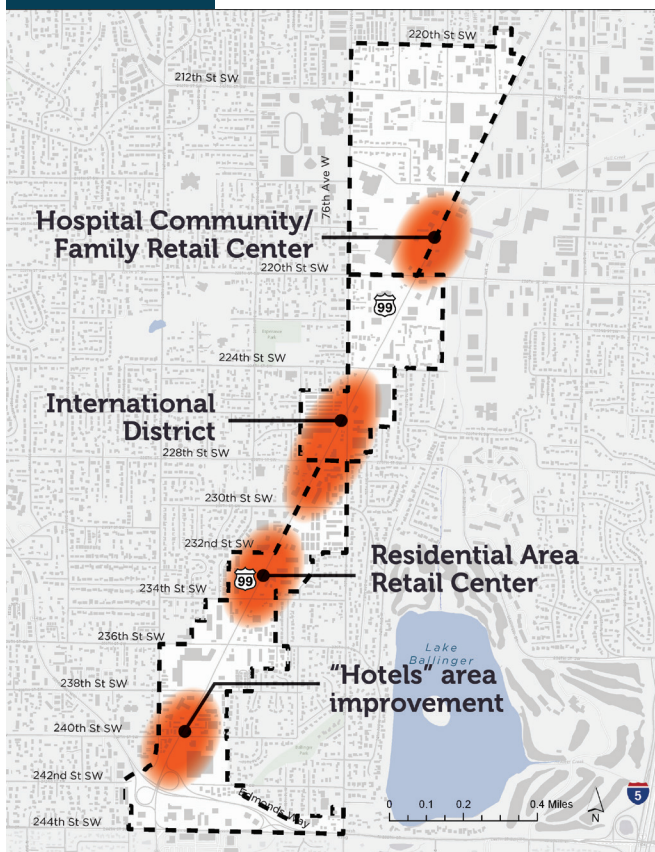
THE ISSUE TODAY:

The HWY 99 area has had several plans and studies in the past that have designated unique subdistricts within the HWY 99 area. This process has affirmed two of those subareas and changed one other. The subareas include a Hospital District at the north end, an International district in the center and a Gateway District in the south. The current Comprehensive Plan includes a subdistrict map that designates four focus areas, but does not reflect the community's desire for a southern "gateway" district that defines the entry into Edmonds.

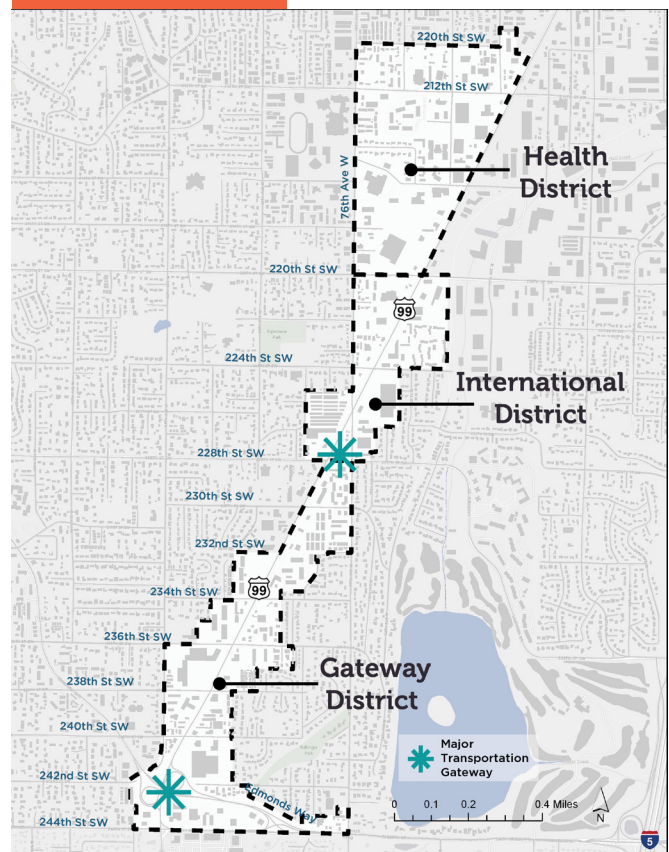
RECOMMENDATION 3.1

Replace the current Comprehensive Plan maps and text with updated materials that clearly identify the three distinct districts anchored around major transportation gateways and employment clusters, such as the hospital and international businesses.

CURRENT MAP



RECOMMENDATION



4

CONSOLIDATE CG AND CG-2 INTO A SINGLE CG ZONE

THE ISSUE TODAY:

The zoning in the planning area is unnecessarily complex and confusing. Most of the area is either zoned CG or CG2. The difference between them is a minor height difference of 15 feet. CG has a height allowance of 60 feet while CG2 has a height allowance of 75 feet.

RECOMMENDATION 4.1

Consolidate the existing CG and CG2 into a single CG zone with height limit at 75 feet. This allows for a cost-effective 6 story mixed-use building to be constructed with comfortable floor to ceiling heights. The construction type of 5 wood framed floors over a ground floor, concrete podium (also known as a “5-over-1 building”) is efficient and cost effective, and is also within the height capacity of fire truck ladders.

5

SIMPLIFY ZONING DESIGNATIONS AND ALIGN ZONING WITH COMPREHENSIVE PLAN

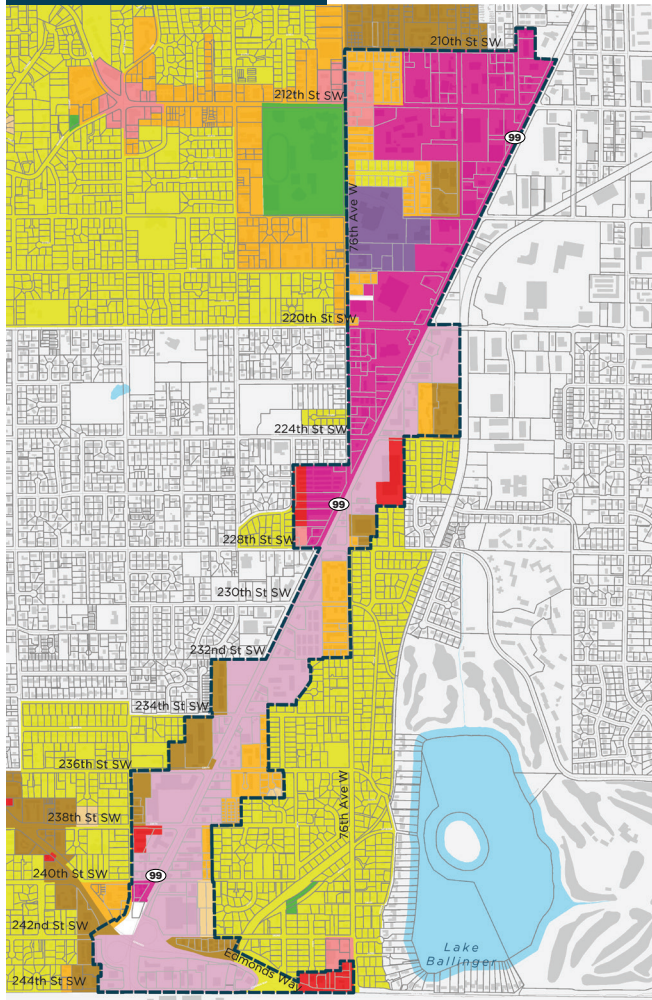
THE ISSUE TODAY:

Many of the current zones in the HWY 99 study area are remnants from the zones that were in place when this area of Edmonds was annexed from the County. The patchwork of zones is outdated and, in some cases, not consistent with parcel boundaries, meaning that some lots have more than one zone.

RECOMMENDATION 5.1

Instead of having 6 or more zones, it is recommended that the new, consolidated CG zone be applied to most of the study area. Additional recommendations below, as well as a change to other multifamily properties in the subarea when zoning map amendments are being considered, will ensure new buildings transition in scale into the surrounding single family neighborhoods. These changes will better align the zoning with the Comprehensive Plan map.

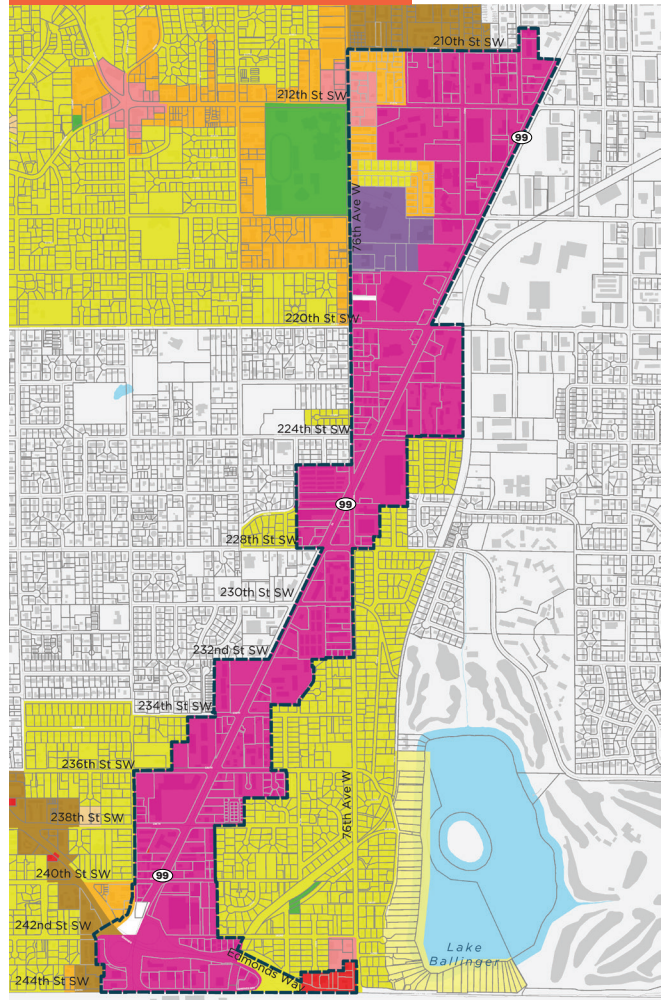
CURRENT ZONING MAP



CURRENT ZONING

- CG2 - General Commercial 2
- CG - General Commercial
- BN - Neighborhood Business
- BC - Community Business
- RS-8 - Single Family, 8,000 sq. ft.
- RM-3 - Multifamily, 3,000 sq. ft.
- RM-2.4 - Multifamily, 2,400 sq. ft.
- RM-1.5 - Multifamily, 1,500 sq. ft.
- MU - Medical Use
- P - Public Use

RECOMMENDED ZONING MAP



RECOMMENDED ZONING

- CG - General Commercial
- BN - Neighborhood Business
- BC - Community Business
- RS-8 - Single Family, 8,000 sq. ft.
- RM-3 - Multifamily, 3,000 sq. ft.
- RM-2.4 - Multifamily, 2,400 sq. ft.
- RM-1.5 - Multifamily, 1,500 sq. ft.
- MU - Medical Use
- P - Public Use

6

MODIFY CURRENT DESIGN STANDARDS

The design standard recommendations in this plan are not regulatory changes. Rather, they are proposed modifications to be considered when the design standards are written during the implementation of the plan. Code changes will only occur after the Subarea Plan has been adopted. During the implementation phase of the Plan when the design standards are modified, consideration of special circumstances within the corridor will be made to ensure the standards are feasible. For instance, large parcels that would have multiple buildings if redeveloped and parcels with unique access or transportation challenges may require a modified approach to the design standards.

ACCESS AND PARKING

WHAT ARE THE CURRENT DESIGN STANDARDS IN CG AND CG2 ZONES?

Current standards require that not more than 50 percent of a project's total parking spaces may be located between the building's front facade and the primary street. Parking lots may not be located on corner locations adjacent to public streets.

THE ISSUE TODAY:

This standard can allow too much parking on street fronts, which impacts pedestrian activity and hinders a vibrant urban street. This standard is attempting to encourage more parking in the rear of buildings, but it is regulating the location of a percentage of the parking rather than the amount of building front should be located towards the road. Depending on the project or lot size, the amount of parking in the front could still be very large with the existing standard. Regulating the percentage of the frontage that needs to be occupied by building instead of parking area is a more appropriate approach.



RECOMMENDATION 6.1

PEDESTRIAN-ORIENTED DESIGN

- » On a primary frontage, a minimum of 50% of the building facade should be within 20 feet of the property line where the primary frontage exists (at the edge of Pedestrian Activity Zone).



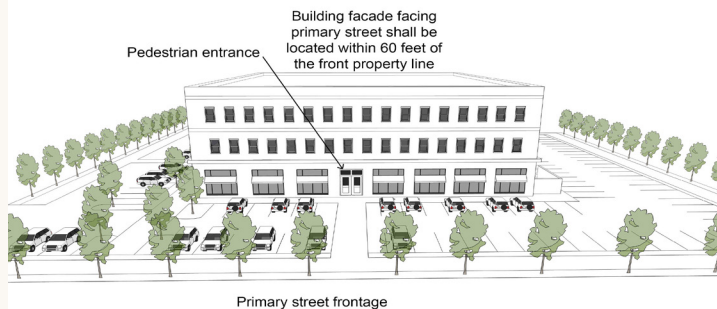
WHAT'S THE BENEFIT?

Pushing parking towards side and rear of buildings and bringing buildings up to the street allows more space for pedestrian activity on primary streets and more visibility for businesses.

RECOMMENDATION 6.3

ALTERNATIVE WALKABLE DESIGN AREA

- » As an alternative to the pedestrian-oriented design, a minimum 50% of the building facade should be within 60 feet of the front property line (at the edge of Pedestrian Activity Zone).
- » This alternative may be allowed if the City has found the site to have unique and significant constraints related to pedestrian access



RECOMMENDATION 6.2

GROUND FLOOR TRANSPARENCY

- » 50% of primary frontage building facade shall be made of transparent windows and doors.
- » All other street-facing facades within 30 feet of a public street require 30% transparency.



SCREENING, BUFFERING, AND AMENITY SPACE

WHAT ARE THE CURRENT DESIGN STANDARDS IN CG AND CG2 ZONES?

Currently the design standards require a dense Type IV landscaping buffer, a minimum of four feet wide, along all street frontages. Amenity space is not specifically required for development of a site.

THE ISSUE TODAY:

This standard creates landscaped barriers between pedestrians and buildings rather than enhancing a safe and comfortable pedestrian zone.



RECOMMENDATION 6.5

PEDESTRIAN ACTIVITY ZONE

- » Replace the 4-foot landscaped buffer with a required 10-foot Pedestrian Activity Zone setback on all primary frontages. Future design standards may consider special circumstances, such as auto dealer locations.



RECOMMENDATION 6.6

AMENITY SPACE

- » Outdoor amenity space, which may include landscaping, benches, or other amenities, should be required in conjunction with development.
- » A portion of the required amenity space should be provided as common space and may include pedestrian activity areas.

WHAT'S THE BENEFIT?

Allows for a range of active uses like sidewalk cafes and amenities such as public art, street furniture, street trees, bus shelters, pavement patterns, lighting, etc.



SETBACK AND BUFFERS FOR USES ADJACENT TO SINGLE-FAMILY ZONES

WHAT ARE THE CURRENT DESIGN STANDARDS IN CG AND CG2 ZONES?

Ground Floor Setback:

- » Current front setback in RM-1.5 (the current multifamily zone near Highway 99) is 15' (ECDC 16.30.030).
- » Where commercial, institutional, medical and multifamily uses are adjacent to residential zones, current standards require a 15' setback (ECDC 16.60.020).
- » Where commercial, institutional, medical uses are adjacent to residential zones, current standards require a dense 10' landscaping buffer (ECDC 16.60.030.A.1.f)
- » Where office and multifamily uses are adjacent to single-family zones, current standards require a minimum 4' wide and 10' tall landscaping buffer (ECDC 16.60.030.A.1.h)

Upper Floor Stepback:

- » No current standards exist for explicitly regulating the bulk and scale of buildings that are adjacent to single family zones.

- » The current Design Criteria seek to ensure "buildings do not display blank, unattractive walls to the abutting streets or residential properties, walls or portions of walls abutting streets or visible from residentially zoned properties" and suggests a variety of architectural treatments to mitigate impact, but does not explicitly require a stepback.
- » The Comprehensive Plan includes recommendations that should be considered when developing uses adjacent to single family areas.

- » From "City of Edmonds Comprehensive Plan (2015) – Medical/Highway 99 Activity Center and Highway 99 Corridor" section, page 64:

"New development should be allowed and encouraged to develop to the fullest extent possible while assuring that the design quality and amenities provided contribute to the overall character and quality of the corridor. Where intense development adjoins residential areas, site design (including buffers, landscaping, and the arrangement of uses) and building design should be used to minimize adverse impacts on residentially-zoned properties"

THE ISSUE TODAY:

Current design standards do not ensure proper transition of higher density buildings adjacent to single-family neighborhoods.



RECOMMENDATION 6.5

- » Establish stepback and setback standards for multifamily and/or commercial buildings adjacent to single family zones and include these standards in the zoning code.

RECOMMENDATION 6.7**GROUND FLOOR SETBACK**

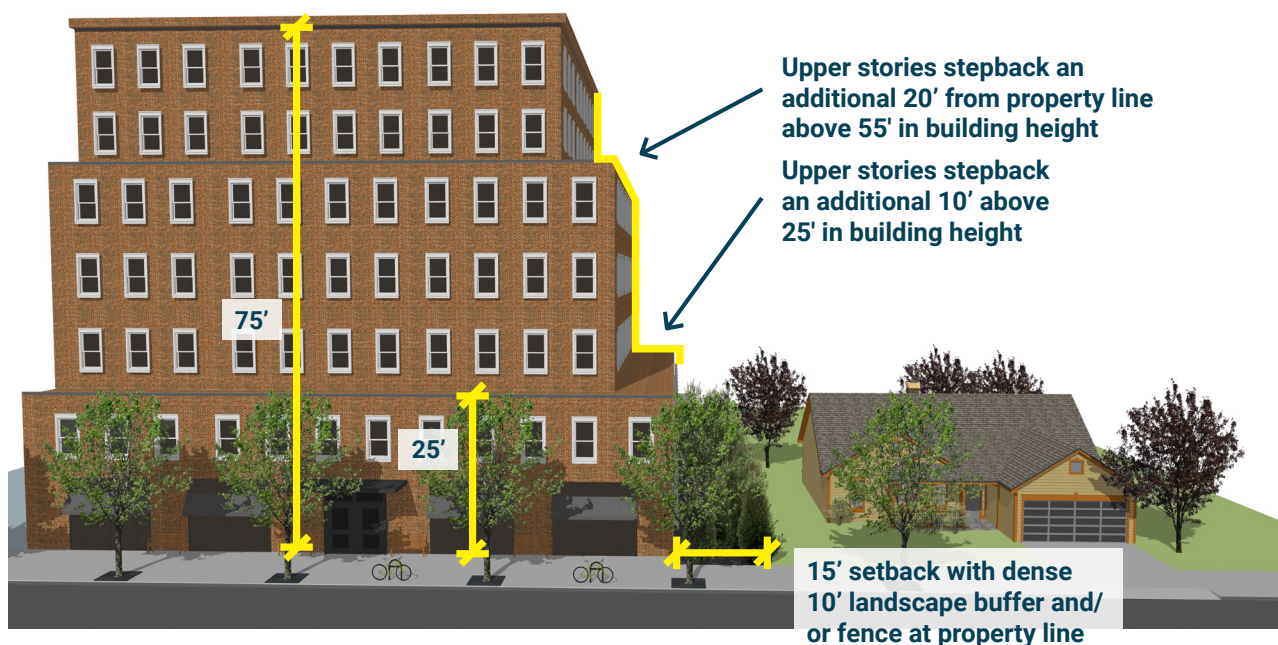
- » For frontages facing Highway 99, require a front setback of 10 feet from the property line to accommodate a Pedestrian Activity Zone.
- » Keep current 15 feet setback and 10' landscaping requirements for lot line adjacency with single family zones.

RECOMMENDATION 6.8**UPPER FLOOR STEPBACK**

- » Zero upper floor stepback up to 25 feet in height (30 feet is the maximum height in RM 1.5, which is the predominant zone surrounding the commercial zones on Highway 99).
- » Minimum 10 feet stepback above 25 feet in height on sides with lot line adjacency to single family zones. The portion of the building above 55 feet in height shall be stepped back at least 20 feet from a residential zone boundary.
- » Stepback areas can be used for active outdoor space such as balconies.

WHAT ABOUT THE CITY OF SHORELINE?

- » For developments consisting of three or more dwelling units located on a single parcel, the setback shall be 15 feet along any property line abutting R-4 or R-6 zones.
- » 10' stepback above 45' in mixed-use zone only





ADOPT TRANSIT-SUPPORTIVE PARKING STANDARDS

HWY 99 has many local and regional transit options as well as regional trails and bike routes, giving residents many travel options. In transit-rich areas, it is common for communities to reduce required on-site parking to encourage higher intensity and mixed-use development. Parking is also expensive and high parking requirements can raise costs, which results in

higher rents and reduced affordability. Current estimates for the cost of structured parking is anywhere from \$20,000-25,000 per space, and underground parking can exceed \$50,000 per space. Edmonds should adopt new, transit supportive parking standards for the Highway 99 area.

CURRENT PARKING STANDARDS FOR RESIDENTIAL AND COMMERCIAL

RESIDENTIAL

- » Studio apartment: 1.2 spaces
- » 1-Bedroom: 1.5 spaces
- » 2-Bedroom: 1.8 spaces
- » 3-Bedroom: 2 spaces

COMMERCIAL

- » 2.5 per 1,000 square feet (1 per 400 square feet)

RECOMMENDATION 7.1

RESIDENTIAL

- » Minimum average of 0.75 per unit for entire residential portion of each development. A different ratio may be approved if the City determines that development is near a transit station or is supported by a parking study.

WHAT'S THE BENEFIT?

Reducing parking minimums and following market demand for parking encourages people to ride transit or walk. This helps relieve congestion and improving environmental conditions.

RECOMMENDATION 7.2

COMMERCIAL

- » 2 per 1,000 square feet (1 per 500 square feet)
- » Exempt the first 3,000 square feet of commercial within mixed-use buildings that have a shared parking plan (parking study and management plan). This reduces the cost burden for small, local entrepreneurs. Compliance should be at the staff level to reduce administrative time and cost.
- » Allow for project-specific studies to reflect special situations.

AFFORDABLE HOUSING RECOMMENDATIONS



8

ENACT MULTIFAMILY TAX EXEMPTION (MFTE) PROGRAM

RECOMMENDATION 8.1

The City of Edmonds should pass an ordinance to define the HWY 99 area, which is an urban center, as a “target area” to allow MFTE projects. This would incentivize the construction of additional housing and mixed-use projects by enabling qualifying projects to take advantage of a tax exemption on the residential-portion of new buildings for 12 years in exchange for keeping 20% of units affordable during that period.



Anthem on 12th: An award-winning workforce housing development in Seattle financed through a Multifamily Tax Exemption Program.

WHAT IS A MFTE PROGRAM?

The MFTE Program is a state-authorized program that provides a property tax exemption for eight or 12 years on new multifamily buildings within urban centers. The 12-year exemption requires a minimum level at least 20% of the units to be affordable to households of moderate or lower income. The eight-year exemption leaves the public benefit requirement—in both type and size—to the jurisdiction’s discretion. The eight-year exemption carries no affordable housing requirement. Cities must pass an enabling ordinance to enact the MFTE and to allow applications for the exemption.

9

CONTINUE OR ENHANCE FEE WAIVER PROGRAM FOR AFFORDABLE HOUSING

RECOMMENDATION 9.1

The City should continue or enhance its program to allow the reduction of transportation and park impact fees for projects that include affordable housing.

10

FACILITATE A MIXED-USE, MIXED-INCOME DEMONSTRATION PROJECT

RECOMMENDATION 10.1

- » Identify a site with a willing owner/partner, or purchase or secure a transferrable option on a site.

RECOMMENDATION 10.2

- » The City can establish a special fund targeted at affordability and/or redevelopment, or make use of one or more of the tools listed on the next page to establish a special assessment district or direct state and federal funds towards a project.

RECOMMENDATION 10.3

- » Actively recruit developers, both non-profit affordable housing builders like the Korean Women's Association or other developers familiar with public-private partnerships. This recruitment can also be done by a specialized consultant.

RECOMMENDATION 10.4

- » Cultivate a champion who can motivate the development community and advocate for more affordable housing projects. This could be a local or state leader, such as an elected representative or a prominent local business leader.

RECOMMENDATION 10.5

- » Make this project the pilot project for the newly adopted MFTE and fee waiver program to ensure they function well and iron out any issues before broader adoption.

RECOMMENDATION 10.6

- » Consider using one or more of the special assessment districts, or programs listed in Recommendation 11.1 and locating this pilot project site within the Highway 99 area. This will enable the City to make use of special funds to assist with development and infrastructure costs or other subsidies. The first project or few projects will require more assistance than subsequent projects.

RECOMMENDATION 10.7

- » Assign special staff to the pilot project to ensure it remains a City priority and keeps moving forward. This staff person will also track what works well or what does not, and make final modification recommendations to the various programs before final broader adoption.



11

EXPAND USE OF FINANCING TOOLS

RECOMMENDATION 11.1

The City should actively seek to make use of local, state and federal funds and funding mechanisms to expand the opportunities for affordable housing, redevelopment and economic development within the HWY 99 area. Below is a list of some key tools and funding sources that should be considered.

- » City Fund for Redevelopment and Affordable Housing
- » Community Renewal Area (CRA) – used in Shoreline
- » Hospital Benefit Zone (HBZ) Financing Program
- » Local Infrastructure Project Area (LIPA) Financing
- » Landscape Conservation and Local Improvement Program (LCLIP)
- » Low Income Housing Tax Credits (LIHTC)
- » HUD HOME Program
- » HUD CDBG Program
- » Enterprise Community Partners Regional Equitable Development Initiative (REDI) Fund

WHAT'S THE BENEFIT?

- » Offers an alternative to the development community in which affordable housing can be a profitable endeavor.
- » Leverages public funds for private investment and demonstrates cost-effective ways to create more affordable housing in the Highway 99 area.



Lovejoy Station in Portland, Oregon is a five-story apartment community that serves residents with incomes between 40% and 80% area median income.

SIGNAGE + WAYFINDING RECOMMENDATIONS



12

**GATEWAY SIGNAGE AT HWY 99/
HWY 104 INTERCHANGE****RECOMMENDATION 12.1**

The public process identified the need to clearly establish the identify of Edmonds at the south end of the study area, through gateway features, such as signage and landscaping. The design treatments should clearly indicate an arrival into Edmonds and distinguish this stretch of HWY 99 from Shoreline. This could be accomplished in tandem with the realignment of the on and off ramps of HWY 104 proposed in the project list.



13

**TRANSIT GATEWAY SIGNAGE/STATION
AT HWY 99/228TH****RECOMMENDATION 13.1**

Regional commuter rail to the Mountlake Terrace transit center is scheduled for completion in 2023. The link from Edmonds to this new rail station is 228th. It is important to identify this transit gateway at the intersection of HWY 99 and 228th, and strengthen east-west connections for transit riders, bicyclists and commuters. Decorative and clear wayfinding signage at this intersection will establish the clear link for visitors and residents alike. A future transit linkage, either in the form of a reroute of existing local transit to connect Edmonds to the Mountlake Terrace transit station should be considered. In addition, an additional or moved BRT station and location at the intersection of 228th should also be examined in the future.

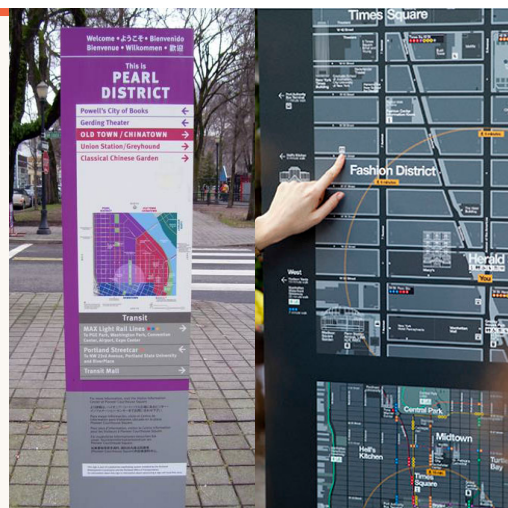


14

IMPROVE WAYFINDING SIGNAGE ALONG THE CORRIDOR

RECOMMENDATION 14.1

Many amenities and community destinations exist near HWY 99, but the public process revealed that finding these amenities can be difficult, particularly for visitors. Wayfinding signage with a uniquely Edmonds identity should point out safe auto, bicycle and pedestrian routes to surrounding amenities. The amenities and destinations identified include downtown Edmonds, Lake Ballinger, the Interurban Trail, new regional rail at Mountlake Terrace, the International District, the Health District and hospital.



15

DEVELOP A UNIQUE DISTRICT DESIGN IDENTITY

RECOMMENDATION 15.1

The subdistricts identified in this process and previous processes highlight the existing nodes of similar business activity, such as international businesses and health and hospital related uses. The City should invest in signage, lighting and art to improve the vitality of these areas, and support business development organizations that build capacity within the private sector. Some ideas the City should pursue are unique branding for each district, public and local art, street furniture, unique bus shelter designs, pavement patterns, special lighting fixtures, colored crosswalks, and banners.



16

PROHIBIT NEW POLE SIGNS

RECOMMENDATION 15.1

As the HWY 99 area transitions from an auto-oriented highway to a more dynamic and mixed-use environment, new tall pole signs designed to capture the attention of fast moving traffic are no longer compatible. The City should prohibit new pole signs within the study area.



TRANSIT RECOMMENDATIONS



17

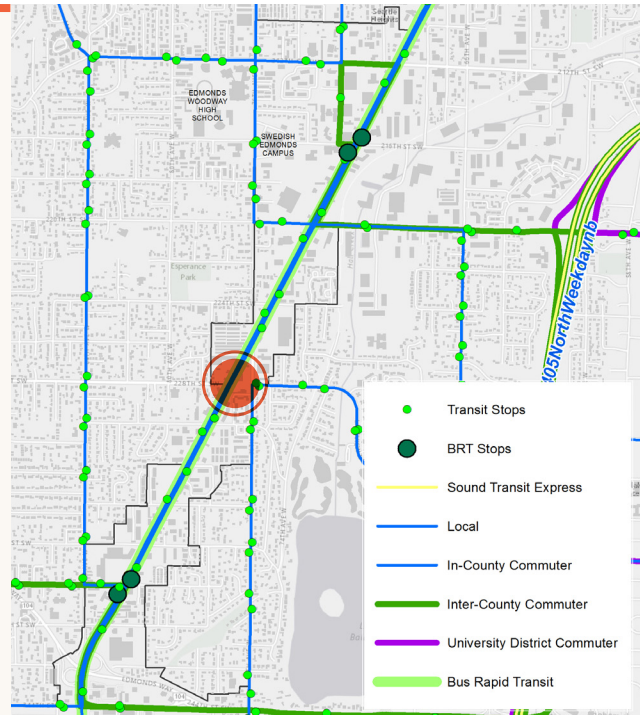
IMPROVE TRANSIT TRANSFERS

RECOMMENDATION 17.1

The public workshop and stakeholder engagement process revealed a few needed improvements in transit, particularly related to improving transit transfers. The City should work with Community Transit and other transit partners to ensure regional and local bus stops are close together and schedules are aligned to ensure convenient and efficient transfers. This can be accomplished by a consolidation or colocation of stations to reduce walking distances between routes.

Specific improvements related to emphasizing a new transit hub at 228th and HWY 99 include:

- » Consider a shuttle/transit service from HWY 99 to the Mountlake Terrace regional transit center
- » Consider a consolidated transit stop at 228th
- » Consider a new BRT station
- » Provide clear signage
- » Provide high-quality bike connection on 228th



Highway 99 and 228th will be a Key Intersection: The intersection improvements completed in May 2016 provide a key connection to the future regional rail in Mountlake Terrace and I-5. The new signalized intersection provides safer left turns, bike lanes, safe pedestrian crossings, lighting, sidewalks, and stormwater management.

18

INCENTIVIZE ALTERNATIVE TRANSPORTATION OPTIONS

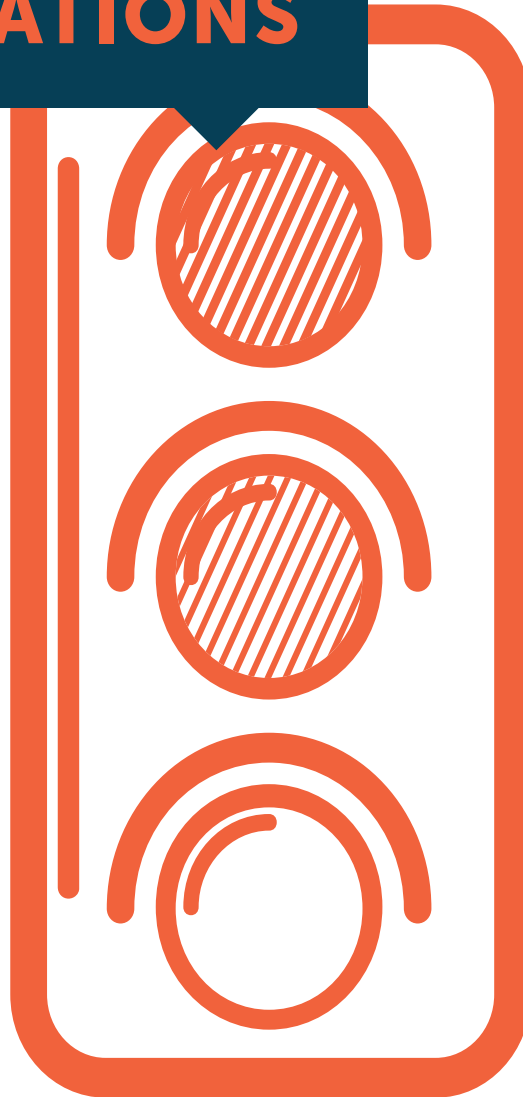
RECOMMENDATION 18.1

A robust and resilient transit system offers a wide range of options to commuters and the community. This includes convenient access to regional transit and transportation facilities, but also the finer grained connections that allow for quick, short connections to be made. The City should consider impact fee reductions and on-site parking reduction allowances for development project that offer or accommodate alternative transportation options on-site. Examples of on-site alternative transportation options include subsidized transit passes for residents, on-site car share parking, bicycle parking, electric car charging stations and temporary parking for private ride sharing services, such as Uber and Lyft.



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TRANSPORTATION INFRASTRUCTURE RECOMMENDATIONS



19

EXPAND USE OF GRANT PROGRAMS TO FUND SAFETY IMPROVEMENTS AND PEDESTRIAN FACILITIES

RECOMMENDATION 19.1

The City should actively seek to make use of local, state and federal funds and funding mechanisms to expand opportunities for safety improvements and pedestrian and bike-friendly enhancements within the HWY 99 area. Below is a list of some key grant programs that should be considered:

- » Safe Routes to School Program (Pedestrian & Bicycle projects within two miles of a school)
- » Pedestrian and Bicycle Program
- » Highway Safety Improvement Program (HSIP)
- » Congestion Mitigation & Air Quality Program (CMAQ)



The following includes a detailed list of short term and long term transportation improvements that include projects identified in previous plans and projects that have arisen as part of this process.

The projects are designed to accommodate a range of transportation goals, including:

- » Improve traffic flow and general safety and access management
- » Improve pedestrian safety and access to/from HWY 99 corridor
- » Improve pedestrian environment along HWY 99 corridor
- » Create safe pedestrian crossings of HWY 99 and access to transit
- » Improve transit mobility and transit stop environment

Further, the projects build upon or enhance the planned transportation improvements described earlier.

KEY TRANSPORTATION IMPROVEMENTS

CLOSE THE MOST SIGNIFICANT GAP IN PEDESTRIAN CROSSINGS WITHIN THE CORRIDOR

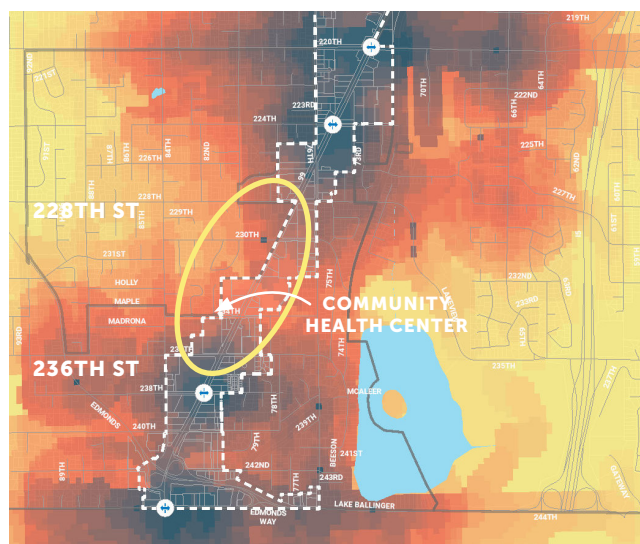
The longest segment of Highway 99 in the Plan Area without a controlled pedestrian crossing is between 238th Street SW and 228th Street SW—a distance of about 3,700 feet. Prior to the completion of the 228th Street connection between 76th Avenue W and the new traffic signal at Highway 99, the longest segment without a controlled crossing was nearly a mile. The short-term recommendation to improve this major obstacle to pedestrian travel is to install a traffic signal on HWY 99 with pedestrian crossings on all approaches.

The logical point within the gap for installation of a traffic signal and pedestrian crossings is at 234th Street SW. While not the midpoint of the segment, there are other factors that strengthen the need for a pedestrian crossing at this location. These include:

- » This location is within a node having strong redevelopment potential creating increased demand for pedestrian travel.
- » The Community Health Center is located 500 feet to the north of this intersection and Community Transit has bus stops on both sides of HWY 99 without a safe crossing to access the northbound bus stop.

The signal will need to be installed concurrently with new development at the 234th Street node in order to help meet signal warrants.

FIGURE 20: SAFETY HEAT MAP SHOWING GAP IN PEDESTRIAN CROSSING



IMPROVE PEDESTRIAN ACCESS FROM THE SOUTH AT THE SR 104 INTERCHANGE

The present design of the SR 104 interchange with HWY 99 is automobile dominated partial cloverleaf with on and off-ramps designed for moderate speeds. Sidewalks exist on both sides of HWY 99 through the interchange but require pedestrians to cross an on and off-ramp in either direction. Most of these crossings are unmarked and located on curves where traffic is accelerating or decelerating from freeway speeds. Further, bridge structure and trees restrict motorist sight lines of pedestrians crossing the ramps. The conditions are daunting for pedestrians and likely discourage people from traversing the interchange on foot.

The eastbound off-ramp at the southern end of the interchange is configured more favorably for pedestrians than the eastbound off-ramp at the northern end because the intersection is the terminus of a pedestrian pathway connecting the surrounding neighborhoods to HWY 99. The eastbound off-ramp is aligned at nearly a right angle to HWY 99, is controlled by a stop sign, and has a high visibility crosswalk crossing the ramp. In contrast, all of the remaining crossings are at uncontrolled and relatively high speed locations.

The long-term recommendation is to reconfigure the ramps as conventional 90-degree stop control intersections with marked crossings similar to the eastbound off-ramp configuration. Trucks may be accommodated through the use of low-angle slip ramps and channelizing islands to keep crossing distances short. Reconfiguration in this manner improves visibility and slows turning traffic.

As a short-term interim improvement, install pedestrian activated Rectangular Rapid Flashing Beacons (RRFB's) with high-visibility crosswalk markings at the pedestrian crossings of the

SR 104 on and off-ramps and provide new, or redirect existing, safety lighting to illuminate the crosswalks.

FIGURE 21: 90-DEGREE STOP CONTROL INTERSECTION TO AN OFF-RAMP CONFIGURATION

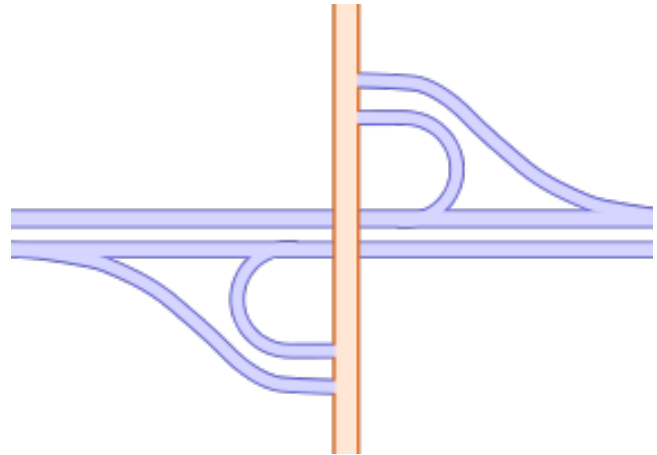


FIGURE 22: RECTANGULAR RAPID FLASHING BEACONS (RRFB'S)



ENCOURAGE WALKING AND BICYCLING TO ACCESS PLAN AREA FROM SURROUNDING NEIGHBORHOODS

During the public workshop residents of the Plan Area expressed concern regarding the safety of walking and bicycling to the HWY 99 corridor from their neighborhoods. Their concerns focused on the rural nature of connecting streets which lack sidewalks and lighting, and have overgrown vegetation restricting sight distance. These concerns extended onto HWY 99 where there was

a general consensus on the need for safety and street lighting on HWY 99 and on the residential streets feeding into the corridor, particularly pedestrian-scaled lighting. The conditions described above limit the resident's desire to access HWY 99 as a pedestrian or bicyclists particularly at night. Specific streets identified in the workshop include 240th Street SW between 84th Avenue W and 80th Way W and approach HWY 99 and 224th Street SW approaching 76th Avenue and HWY 99.

The City of Edmonds 2015 Comprehensive Plan identifies several "complete streets" projects on streets providing access to HWY 99. The improvements—as described in the section on Planned Improvements—include sidewalk construction, drainage improvements, lighting, and reallocation of the street's traveled way to improve safety for all users. Expanding on these projects to include additional streets feeding into the corridor can alleviate the resident's concerns about safety and dramatically increase active modes of transportation.

Improvements need not be extensive to create a more desirable pedestrian environment—sidewalks can be constructed on one side of narrow streets or paths of decomposed granite with asphalt berms may suffice to move pedestrians and bicyclists out of the street's traveled way.

Regardless of the extensiveness of the improvements, lighting should be a high priority in all cases. Safety lighting (lighting that illuminates intersection corners where pedestrians are expected to cross) and street lighting (overhead lighting that generally illuminates the width of the street) as well as pedestrian-scaled lighting (lighting on 12-17-foot tall standards that illuminate the pedestrian walkway) are fundamental prerequisites for walkable areas.

Streets that cannot be safely traveled by pedestrians and bicyclists at night, will experience limited travel during the day.

IMPROVE CONNECTIONS BETWEEN TRANSIT AND MAJOR EMPLOYMENT CENTERS

The extensive transit network serving the corridor is an opportunity to shift employee commute modes at the corridor's larger employment centers from driving to transit. Part of an effective strategy to change employee travel behavior is improving the physical connection between transit and the destination. The SWIFT stations at 216th Street SW serve the corridor's largest employment center—the Swedish Hospital campus and its associated medical offices. The SWIFT bus rapid transit system is an ideal opportunity for employees and patients/visitors to access the campus by transit. However, the connection between the stations and the various facilities in the campus require pedestrians to walk up steep grades and through parking lots to access building entrances. The lack of connections, direction and amenities discourages people from taking transit to the site.

This Plan recommends improving the connection between the Swift Stations at 216th Street SW and the Swedish Hospital Campus by implementing a pedestrian walkway system (potentially covered) internal to the campus with wayfinding to direct pedestrians to the various campus facilities including future land uses that support hospital expansion such as hotels and medical office buildings within the Health Care District.

FIGURE 23: PLANNED TRANSPORTATION IMPROVEMENTS

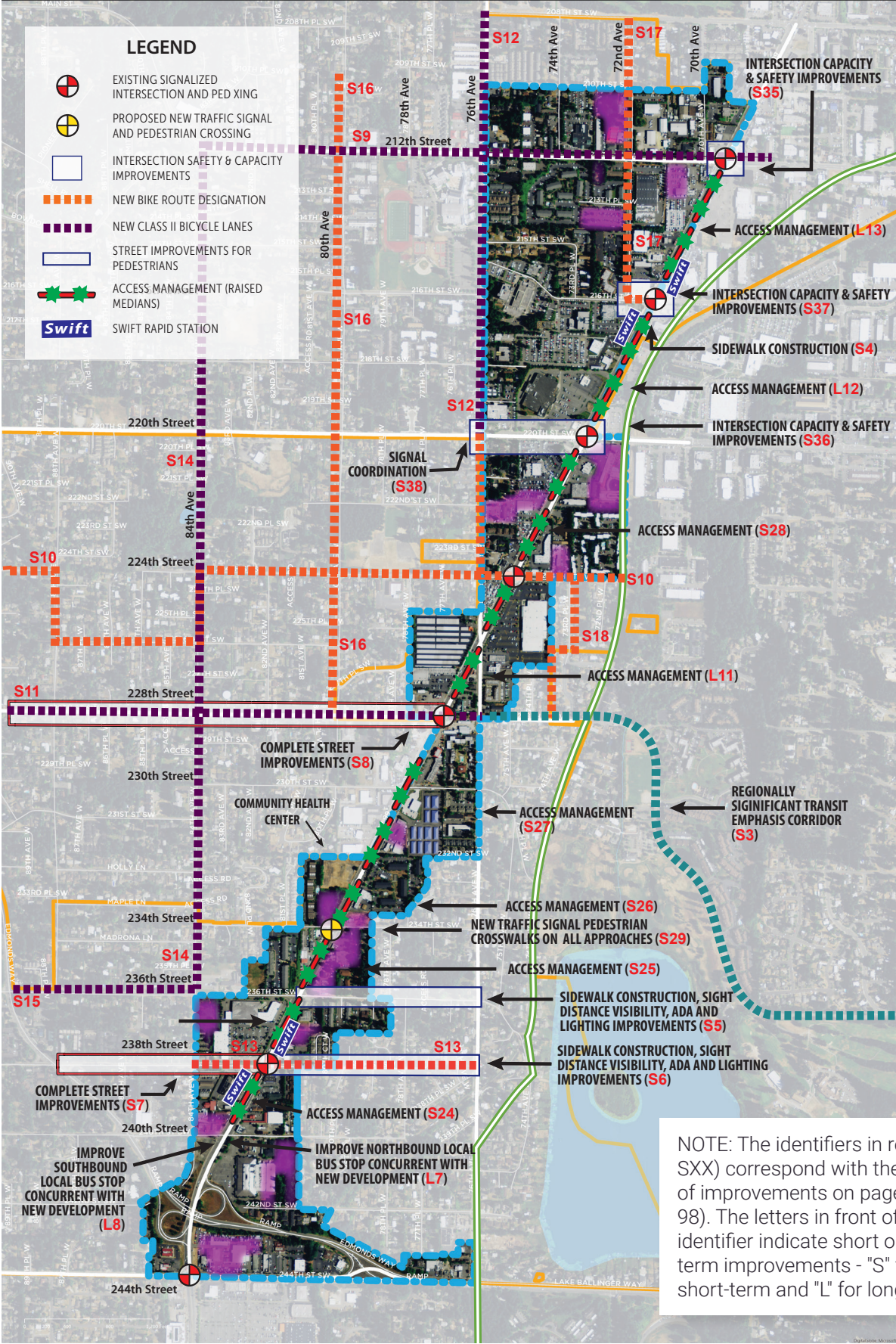
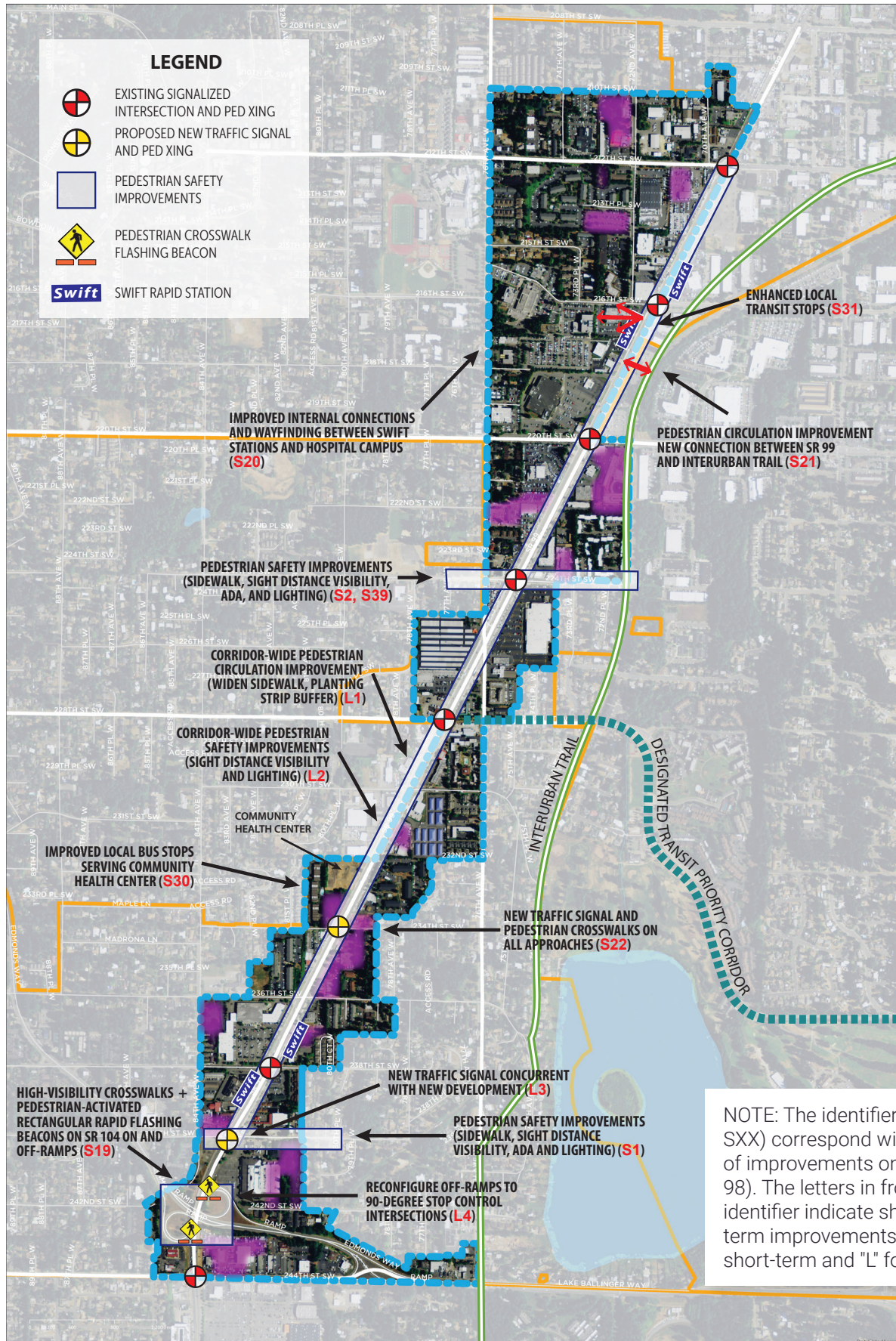


FIGURE 24: PROJECT SUPPORTIVE TRANSPORTATION IMPROVEMENTS



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PROPOSED SHORT-TERM TRANSPORTATION IMPROVEMENTS

IMPROVE PEDESTRIAN SAFETY AND ACCESS TO/FROM HIGHWAY 99 CORRIDOR		
MAP IDENTIFIER	IMPROVEMENT TYPE	DESCRIPTION OF IMPROVEMENT
S1	Pedestrian Safety	Improve sidewalks, sight distance visibility, street and safety lighting on 240th from 84th Ave W to 80th Way W (primarily along commercial frontages)
S2	Pedestrian Safety / Ped Circulation	Implement safety improvements at 224th and 76th Avenue W including constructing new or improving existing sidewalks on both sides of 224th approaching 76th Ave and SR 99.
Not Shown	General Safety	General need for safety and street lighting on residential streets surrounding SR 99, particularly pedestrian-scaled lighting.
S3	Regionally Significant Transit Emphasis Corridor	<p>Workshop participants identified the need to widen sidewalks on 228th east of SR 99. In the Summer of 2016 a number of pedestrian improvements were completed in this regionally significant multimodal corridor (see notes). Where narrow sidewalks still remain within the corridor or on connecting residential streets, the following pedestrian improvements may be considered in-lieu of widening sidewalks:</p> <ol style="list-style-type: none"> 1. Buffer pedestrians from moving traffic with street trees in tree wells constructed within parking lanes. 2. Consistent application of high visibility crosswalk markings at intersections. 3. Ensure street lighting illuminates entire width of street. Currently, street lights are located on one side of the street. Intersections with marked crosswalks should have safety lighting illuminating each end of crosswalks. 4. Install bus shelters at local bus stops with street lighting. Where right of way wont permit a shelter, use curb extensions to add width.
S4	Pedestrian and Vehicular Safety / Ped Circulation	Sidewalk construction projects: 216th St. SW from 72nd Ave. W to SR 99
S5	Pedestrian and Vehicular Safety / Ped Circulation	Sidewalk construction projects: 236th St. SW from HWY. 99 to 76th Ave. W
S6	Pedestrian and Vehicular Safety / Ped Circulation	Sidewalk construction projects: 238th St. SW from HWY. 99 to 76th Ave. W
S7	Complete Streets Impvts	238th Street SW, between SR 104 and SR 99. Widen to three lanes with curb, gutter, bike lanes, and sidewalk.
S8	Complete Streets Impvts	228th Street SW, between SR 99 and 95th Pl. W Widen to three lanes with curb, gutter, bike lanes and sidewalk, as well as intersection improvements at 228th @ 95th.

PROJECT SUPPORTIVE

PLANNED IMPROVEMENT

OTHER

IMPROVE PEDESTRIAN SAFETY AND ACCESS TO/FROM HIGHWAY 99 CORRIDOR	
NOTES	SOURCE
This type of corridorwide frontage improvement typically occurs as a condition of approval when the fronting property redevelops.	Workshop
224th Street, as a route, provides access to a few destination such as the interurban trail, but is not a primary route to major generators. However, many residential neighborhoods feed into 224th and it may serve as a lower volume and lower speed alternative for pedestrians and bicyclists. Near SR 99 224th lacks sidewalks on one or both sides of the street.	Workshop
Many of the workshop participants commented on the lack of street lighting on streets intersecting or paralleling SR 99. Safety lighting (lighting that illuminates intersection corners where pedestrians are expected to cross) and street lighting (overhead lighting that generally illuminates the width of the street) as well as pedestrian-scaled lighting (lighting on 12-17-foot tall standards that illuminate the pedestrian walkway) are fundamental prerequisites for walkable areas. Streets that cannot be safely traveled by pedestrians and bicyclists at night, will experience limited travel during the day.	Workshop
228th Street SW is one of the study area's only Complete Streets. It connects the SR 99 corridor to numerous destinations including Highway 104 into downtown, the interurban trail, parks and recreational facilities, and the Mountlake Terrace Transit Center where Sound Transit's extension of the Lynnwood LINK light rail will connect with local, commuter, and regional busses by the year 2023. 228th is also a local bus route. Class II bicycle lanes on 228th connect SR 99 to the interurban trail. The City recently extended 228th from 76th Avenue to complete its connection to SR 99. Other recent pedestrian improvements in the corridor include new ADA compliant ramps at corners, sidewalk repair, driveways moved to side streets, and an improved crosswalk at the Interurban Trail crossing with new curb extensions. The very narrow sidewalks on 228th that once connected to the Trail (two to three feet wide) have been augmented with a multi-use path parallel to the west side of the street extending to the Interstate 5. The pedestrian environment along some segments of 228th need improvement.	Workshop
Project identified as a high priority in the Comprehensive 2015 Transportation Element	Comprehensive Plan 2015 Transportation Element - Recommended Roadway Capital Projects
Project identified as a high priority in the Comprehensive 2015 Transportation Element	Comprehensive Plan 2015 Transportation Element - Recommended Roadway Capital Projects
Project identified as a high priority in the Comprehensive 2015 Transportation Element	Comprehensive Plan 2015 Transportation Element - Recommended Roadway Capital Projects
This project is recommended for inclusion in the City's Transportation Improvement Plan (TIP) for 2022-2035.	Comprehensive Plan 2015 Transportation Element - Recommended Roadway Capital Projects
This project is recommended for inclusion in the City's Transportation Improvement Plan (TIP) for 2022-2035.	Comprehensive Plan 2015 Transportation Element - Recommended Roadway Capital Projects

PROPOSED SHORT-TERM TRANSPORTATION IMPROVEMENTS

IMPROVE BICYCLE CIRCULATION ACROSS AND PARALLEL TO HIGHWAY 99 CORRIDOR		
MAP IDENTIFIER	IMPROVEMENT TYPE	DESCRIPTION OF IMPROVEMENT
S9	Bicycle Circulation	Class II bike lanes on 212th Street from Main Street to 68th Avenue crossing SR 99.
S10	Bicycle Circulation	Bike route designation on 224th Street SW from 84th Avenue W across SR 99 to interurban trail.
S11	Bicycle Circulation	Class II bike lanes on 228th Street SW from SR 104 across SR 99 to existing Class II lanes on 76th Avenue W.
S12	Bicycle Circulation	Class II bike lanes on 76th Avenue W from 208th to 220th and bike route designation to 224th Street SW.
S13	Bicycle Circulation	Bike route designation on 238th Street SW from 84th Avenue W across SR 99 to existing Class II bike lanes on 76th Avenue W.
S14	Bicycle Circulation	Class II bike lanes on 84th Avenue W from 212th Street SW to 236th Street SW and bike route designation on 84th Avenue W south to 238th Street SW.
S15	Bicycle Circulation	Class II bike lanes on 236th Street SW from SR 104 to 84th Avenue W.
S16	Bicycle Circulation	Bike route designation on 80th Avenue W from 206th Street SW to 228th Street SW
S17	Bicycle Circulation	Bike route designation on 72nd Avenue W from 208th Street SW to 216th Street SW and continuing on 216th Street SW to SR 99.
S18	Bicycle Circulation	Bike route designation on 73rd PI W from 224th Street SW to 226th PI SW.

PROJECT SUPPORTIVE

PLANNED IMPROVEMENT

OTHER

IMPROVE BICYCLE CIRCULATION ACROSS AND PARALLEL TO HIGHWAY 99 CORRIDOR	
NOTES	SOURCE
	City of Edmonds Bicycle Master Plan & Comprehensive Plan 2015 Transportation Element
	City of Edmonds Bicycle Master Plan & Comprehensive Plan 2015 Transportation Element
	City of Edmonds Bicycle Master Plan & Comprehensive Plan 2015 Transportation Element
	City of Edmonds Bicycle Master Plan & Comprehensive Plan 2015 Transportation Element
	City of Edmonds Bicycle Master Plan & Comprehensive Plan 2015 Transportation Element
	City of Edmonds Bicycle Master Plan & Comprehensive Plan 2015 Transportation Element
	City of Edmonds Bicycle Master Plan & Comprehensive Plan 2015 Transportation Element
72nd Avenue from 208th to 212th is a heavily utilized transit route. Because of the frequency of buses on this street, Community Transit recommends providing bicycle lanes instead of a route designation, or, if bicycle lanes cannot be provided, to relocate the route designation to an alternative parallel street.	City of Edmonds Bicycle Master Plan & Comprehensive Plan 2015 Transportation Element
	City of Edmonds Bicycle Master Plan & Comprehensive Plan 2015 Transportation Element

PROPOSED SHORT-TERM TRANSPORTATION IMPROVEMENTS

IMPROVE PEDESTRIAN ENVIRONMENT ALONG HIGHWAY 99 CORRIDOR		
MAP IDENTIFIER	IMPROVEMENT TYPE	DESCRIPTION OF IMPROVEMENT
S19	Pedestrian Safety	Install pedestrian activated Rectangular Rapid Flashing Beacons (RRFB's) with high-visibility crosswalk markings at the pedestrian crossings of the SR 104 on and off-ramps and provide safety lighting to illuminate the crosswalks.
Not Shown	Pedestrian and General Multimodal Circulation	Implement corridorwide wayfinding signage to local districts and major multimodal facilities

Attachment: Attachment 1: Subarea Plan Ordinance_2017-08-09 (Adoption of Subarea Plan, Zone Map Amendments, Dev. Regs. & Planned



PROJECT SUPPORTIVE



PLANNED IMPROVEMENT



OTHER

IMPROVE PEDESTRIAN ENVIRONMENT ALONG HIGHWAY 99 CORRIDOR	
NOTES	SOURCE
This is an interim improvement prior to the proposed reconfiguration of the ramp termini to eliminate high-speed movements.	DKS Associates
A corridorwide wayfinding system should be established providing signing at key intersections connecting to major destinations such as downtown, train station, SWIFT stops, nearest bicycle facilities, interurban trail access, parks and open space, local districts along the corridor, and freeway access.	Workshop and DKS Associates

PROPOSED SHORT-TERM TRANSPORTATION IMPROVEMENTS

PROVIDE SAFE PEDESTRIAN CROSSING OF HIGHWAY 99 AND ACCESS TO TRANSIT		
MAP IDENTIFIER	IMPROVEMENT TYPEW	DESCRIPTION OF IMPROVEMENT
S20	Access to/from Transit and Major Employment Center	Improve connection between the Swedish Hospital Campus and the Swift Stations at 216th Street SW by implementing a pedestrian walkway system (potentially covered) internal to the campus with wayfinding to direct pedestrians to the various campus facilities including future land uses that support hospital expansion such as hotels and medical office buildings within the Health Care District.
S21	Access to/from Transit and Interurban Trail	Provide pedestrian and bicycle links that connect the Interurban Trail to the SWIFT Station and Community Transit bus stops serving the Health Care District. When the property on the east side of SR 99 between 216th Street SW and 220th Street SW redevelops, require the development to dedicate an easement connecting the trail to SR 99.
S22	Pedestrian Safety / Access to Transit	"New pedestrian crossing at 234th Street which is the approximate mid-point of the large gap in crossings. This crossing may be achieved as shown in the notes column."
S24	"Traffic Safety Access Management / Safe Pedestrian Crossing"	Install raised median (with potential gateway features) between 240th and 238th. Channelize westbound traffic on 240th for right turns only. Allow u-turns at 238th. See not [1] below.
S25	"Traffic Safety Access Management / Safe Pedestrian Crossing"	236th Street to 234th Street. Install raised median and limited c-curb on SR 99. Restrict left turns from stop-controlled 236th Street.
S26	"Traffic Safety Access Management / Safe Pedestrian Crossing"	234th Street to 230th Street. Install raised median and limited c-curb on SR 99. Restrict left turns from stop-controlled 234th Street.
S27	"Traffic Safety Access Management / Safe Pedestrian Crossing"	230th Street to 228th Street. Install raised median and limited c-curb on SR 99. Restrict left turns from stop-controlled 234th Street.

PROJECT SUPPORTIVE

PLANNED IMPROVEMENT

OTHER

PROVIDE SAFE PEDESTRIAN CROSSING OF HIGHWAY 99 AND ACCESS TO TRANSIT

NOTES	SOURCE
The SWIFT stations at 216th Street SW serve corridor's largest employment center--the Swedish Hospital campus and its associated medical offices. The SWIFT bus rapid transit system and these stations are an ideal opportunity for employees and patients/visitors to access the campus by transit. However, the connection between the stations and the various facilities in the campus are by 216th Street SW and SR 99 driveways requiring pedestrians to walk up steep grades and through parking lots to access building entrances. The lack of connections, direction and amenities discourages people from taking transit to the site.	Community Transit and DKS Associates
The Interurban Trail generally parallels SR 99 and its alignment is closest to SR 99 between 216th Street SW and 220th Street SW--with only one parcel of land separation--an opportunity to provide a more direct link to the SWIFT stations (via crosswalks) and Community Transit bus stops located on the near and far sides of 216th Street SW. When the under-utilized land separating the trail from SR 99 redevelops, a condition of approval should require the property owner to dedicate an easement for this connection.	Community Transit and DKS Associates
<p>A. With significant high density development at 234th node, potentially intersection would warrant a traffic signal (see long-term improvements)</p> <p>B. Install a HAWK pedestrian activated signal</p> <p>C. Install a temporary two-stage unsignalized crossing in the interim timeframe before a signal is warranted.</p> <p>The crossing at this location should be a priority given it improves access to the community health facility from transit and closes a 3,500 foot gap in protected crossings within this segment of Highway 99</p>	DKS Associates
Improvements at 240th are geared towards obtaining a pedestrian crossing of SR 99 to close one of the crossing gaps in the corridor and to improve vehicular safety and access to the Burlington Coat Factory site. 240th Avenue at SR 99 is one of the highest vehicular collision locations in the corridor and it is not prudent to add an unsignalized crossing at this location. Current crossing demand is too low to warrant a short-term pedestrian crossing improvement. See long-term improvements.	SR 99 Access Management and Cross Section Focused Assessment
For long segments with numerous driveways, use intermittently placed medians to allow left turn in/out functions at key driveways. Access managed segments must permit u-turns at adjacent signalized intersections. Use median to restrict left turns from stop-controlled side streets with high collision histories unless the restriction conflicts with a short-term improvement or other proposed change in traffic control.	SR 99 Access Management and Cross Section Focused Assessment

PROPOSED SHORT-TERM TRANSPORTATION IMPROVEMENTS

PROVIDE SAFE PEDESTRIAN CROSSING OF HIGHWAY 99 AND ACCESS TO TRANSIT, CONT'D.

MAP IDENTIFIER	IMPROVEMENT TYPE	DESCRIPTION OF IMPROVEMENT
S28	"Traffic Safety Access Management / Safe Pedestrian Crossing"	224th Street to 220th Street. Install raised median and limited c-curb on SR 99.
S29	Safe Pedestrian Crossing / Development Access	Install traffic signal at SR 99 / 234th including pedestrian crosswalks on all approaches.

IMPROVE TRANSIT MOBILITY AND TRANSIT STOP ENVIRONMENT

MAP IDENTIFIER	IMPROVEMENT TYPE	DESCRIPTION OF IMPROVEMENT
Not Shown	Transit Mobility	Implement a Transit Signal Priority (TSP) system along SR 99 for the SWIFT Bus Rapid Transit system.
S30	Access to Transit / Transit Stop Environment	Improve local bus stops between 234th and 230th that serve Community Health Center and new development within the International and Gateway Districts.
S31	Access to Transit / Transit Stop Environment	Provide enhanced local transit stops near Swedish Hospital.
S32	Transit Service / Ped Amenities	Priority Transit Corridor: SR 99 from 208th to SR 104 (See Figure 10 on page 23 for Priority Transit Corridors designations in the subarea)
S33	Transit Service / Ped Amenities	Priority Transit Corridor: 228th Street SW from SR 104 to 76th Avenue W continuing to the Mountlake Terrace Transit Center. A new SWIFT station is proposed at SR 99 and 228th Street SW. (See Figure 10 on page 23 for Priority Transit Corridors designations in the subarea)

PROJECT SUPPORTIVE PLANNED IMPROVEMENT OTHER

PROVIDE SAFE PEDESTRIAN CROSSING OF HIGHWAY 99 AND ACCESS TO TRANSIT, CONT'D.

NOTES	SOURCE
For long segments with numerous driveways, use intermittently placed medians to allow left turn in/out functions at key driveways. Access managed segments must permit u-turns at adjacent signalized intersections. Use median to restrict left turns from stop-controlled side streets with high collision histories unless the restriction conflicts with a short-term improvement or other proposed change in traffic control.	SR 99 Access Management and Cross Section Focused Assessment
This improvement may only be feasible with significantly high density development at 234th Street node and with access from 234th Street to meet signal warrants. New development fronting SR 99 adjacent to the intersection should be required to dedicate land to provide width for wider sidewalks.	Workshop and DKS Associates

IMPROVE TRANSIT MOBILITY AND TRANSIT STOP ENVIRONMENT

NOTES	SOURCE
Transit Signal Priority (TSP) systems allow BRT vehicles to trigger a change in traffic signal phasing in favor of the buses approaching a signalized intersection. TSP can either expedite a green light for buses passing through an intersection, or expedite buses stopping at far-side bus stops and using the traffic signal's following cycle to load / unload passengers thus avoiding the delay waiting to cross to the far-side stop.	DKS Associates
Provide shelters, benches, lighting, and buffer the stop from moving traffic.	DKS Associates
The Swift Rapid stations are excellent examples of Bus Rapid Transit stations with ample rider amenities. The nearby local bus stops are established some distance away and most have only signs, some have a bench and waste receptacle. Enhancements should be considered at local stops near major employment centers, major retail concentrations, or institutions where employees may commute by transit. Local bus stops enhanced with wider waiting areas with shelters, benches, shade, lighting, bike racks, etc. may help encourage transit ridership, particularly in combination with employer trip reduction programs.	Workshop
These priority corridors would emphasize good daily transit service and bus stop amenities to make transit attractive.	Comprehensive Plan 2015 Transportation Element
These priority corridors would emphasize good daily transit service and bus stop amenities to make transit attractive. Although the LINK light rail extension to the Mountlake Terrace Transit Center is still six years out (2023) implementing incremental Priority Transit Corridor improvements on 228th is recommended in the short term to improve current bus service in the corridor as well as prepare the corridor for LRT service in 2023.	Comprehensive Plan 2015 Transportation Element

PROPOSED SHORT-TERM TRANSPORTATION IMPROVEMENTS

IMPROVE TRAFFIC FLOW AND GENERAL SAFETY WITH ACCESS MANAGEMENT		
MAP IDENTIFIER	IMPROVEMENT TYPE	DESCRIPTION OF IMPROVEMENT
S34	Pedestrian and Vehicular Safety / Ped Circulation	Safety improvements at the intersection of SR 99 / 224th Street SW and between 224th and 76th Avenues (particularly LT into Ranch 99 Market).
S35	Intersection Capacity & Safety / Ped Safety	SR 99 and 212th Street SW-widen 212th to add a westbound left turn lane for 200-foot storage length and an eastbound left turn lane for 300-foot storage length. Provide protected left turn phase for eastbound and westbound movements.
S36	Intersection Capacity & Safety / Ped Safety	SR 99 and 220th Street SW Widen 220th to add a 325-foot westbound right turn lane and a 300-foot eastbound right turn lane. Widen 220th to add a second westbound left turn lane. (This would add about 24 additional feet of crosswalk distance).
S37	Intersection Capacity & Safety / Ped Safety	SR 99 at 216th Street SW Widen to allow one left turn lane, one through lane and one right turn lane in eastbound and westbound directions, with 100-foot storage length for turn lanes. Add eastbound right turn overlap with northbound protected left turn.
S38	Intersection Capacity	Signal Coordination on 220th St. SW from 76th Ave. W to SR 99.
Not Shown	Access Management & Traffic Flow Improvements	"Consideration of operational strategies to help SR 99 traffic flow optimization, including: - U-turns (recommended at intersections in the access management memorandum). - Jug handle movement accommodations at intersections. - Use of adaptive traffic signals."

NOTES:

- From the southern border at the county line (MP 43.50) until approximately 1/4 mile to the north, just past the interchange area but before 240th Street SW (MP 43.72), SR 99 is designated as **Limited Access Partially Controlled**. This is defined as "At-grade intersections are allowed for selected public roads and approaches for existing private driveways, no commercial approaches allowed, and no direct access if alternate public road access is available".
- From the point just north of the SR 104 interchange (MP 43.72) northward through both Edmonds and Lynnwood (MP 43.72 - 48.92), the designation of SR 99 is **Managed Access Class 4**.



PROJECT SUPPORTIVE



PLANNED IMPROVEMENT



OTHER

IMPROVE TRAFFIC FLOW AND GENERAL SAFETY WITH ACCESS MANAGEMENT

NOTES	SOURCE
Revision of the side street channelization at 224th to avoid the need for a split phase. WSDOT suggests changing the shared thru/left and right-only lanes to a left-only and shared thru/right configuration.	Workshop and WSDOT
This project is recommended for inclusion in the City's Transportation Improvement Plan (TIP) for 2016-2021.	Comprehensive Plan 2015 Transportation Element - Recommended Roadway Capital Projects
This project is recommended for inclusion in the City's Transportation Improvement Plan (TIP) for 2016-2021.	Comprehensive Plan 2015 Transportation Element - Recommended Roadway Capital Projects
This project is recommended for inclusion in the City's Transportation Improvement Plan (TIP) for 2016-2021.	Comprehensive Plan 2015 Transportation Element - Recommended Roadway Capital Projects
Should be implemented in coordination with a Transit Signal Priority system (see S37).	Comprehensive Plan 2015 Transportation Element
U-turns will be extremely important with implementation of median access management. Thus, use of various and innovative measures to accommodate changes in traffic patterns is encouraged and might come as a result of site redevelopment. Note that 48-50 feet is the minimum U-turn diameter accepted in the past by the State, with justification.	WSDOT

- 3. State of Washington Legal Requirements of M4 Highways.** Class 4 Managed Access highways are designed to have a posted speed limit of 30 to 35 mph in urbanized areas and 35 to 45 mph in rural areas. In urban areas and developing areas where higher volumes are present or growth that will require a change in intersection control is expected in the foreseeable future, it is imperative that the location of any public access point be planned carefully to ensure adequate traffic progression. Where feasible, major intersecting roadways that might ultimately require intersection control changes are planned with a minimum of ½-mile spacing. Private access connections to the highway are spaced 250 feet apart, only a single access for individual or contiguous parcels under the same owner, and variance permits may be allowed.
- 4. Within Incorporated Cities.** Under RCW 35.78.030 and RCW 47.50, incorporated cities and towns have jurisdiction over access permitting on streets designated as state highways. Accesses located within incorporated cities and towns are regulated by the city or town and no deviation by WSDOT will be required. Document decisions made on these accesses in the DDP.

PROPOSED LONG-TERM TRANSPORTATION IMPROVEMENTS

IMPROVE PEDESTRIAN ENVIRONMENT ALONG HIGHWAY 99 CORRIDOR		
MAP IDENTIFIER	IMPROVEMENT TYPE	DESCRIPTION OF IMPROVEMENT
L1	Pedestrian Circulation	Widen sidewalks on SR 99 from 212th street to 240th Street to include a minimum 4-foot wide planting strip to buffer pedestrians from moving traffic. Use space for placement of appropriate street trees.
L2	Vehicular / Pedestrian Safety	Install street lighting on SR 99 corridor to close gaps and to achieve uniform spacing and illumination. Install safety lighting at intersections as part of this improvement.

SAFE PEDESTRIAN CROSSING OF HIGHWAY 99 AND ACCESS TO TRANSIT		
MAP IDENTIFIER	IMPROVEMENT TYPE	DESCRIPTION OF IMPROVEMENT
L3	Pedestrian Circulation	Install new signal at 240th (with crosswalks on all four legs) concurrent with new development at 240th node. Consolidate and relocate driveways to 240th (helps meet signal warrants)
L4	Vehicular / Pedestrian Safety	Reconfigure off-ramps as conventional 90-degree stop control intersections. The Rectangular Rapid Flashing Beacons (RRFB's) recommended as an interim short-term improvements (S19) may continue to be used with the reconfigured ramps.



PROJECT SUPPORTIVE



PLANNED IMPROVEMENT



OTHER

IMPROVE PEDESTRIAN ENVIRONMENT ALONG HIGHWAY 99 CORRIDOR	
NOTES	SOURCE
This improvement requires approximately 6-feet of right of way. The City should require dedication of right of way (or a permanent public access easement) from private property when redeveloped. Dedication or easement may also be used when properties adjacent to local bus stops redevelop in order to obtain width for ADA compliance and bus shelters.	"Highway 99 Traffic Safety and Circulation Study (2007) DKS Associates"
This improvement may also be implemented in conjunction with district identity, streetscape, or themed urban design projects.	"Highway 99 Traffic Safety and Circulation Study (2007) DKS Associates"

SAFE PEDESTRIAN CROSSING OF HIGHWAY 99 AND ACCESS TO TRANSIT	
NOTES	SOURCE
This improvement requires approximately 6-feet of right of way. The City should require dedication of right of way (or a permanent public access easement) from private property when redeveloped. Dedication or easement may also be used when properties adjacent to local bus stops redevelop in order to obtain width for ADA compliance and bus shelters.	"Highway 99 Traffic Safety and Circulation Study (2007) DKS Associates"
This improvement may also be implemented in conjunction with district identity, streetscape, or themed urban design projects.	"Highway 99 Traffic Safety and Circulation Study (2007) DKS Associates"

PROPOSED LONG-TERM TRANSPORTATION IMPROVEMENTS

IMPROVE TRANSIT MOBILITY AND TRANSIT STOP ENVIRONMENT

MAP IDENTIFIER	IMPROVEMENT TYPE	DESCRIPTION OF IMPROVEMENT
----------------	------------------	----------------------------

The following Priority Transit Corridor improvement designations are included in Edmond's Comprehensive Plan. Improvements specific to Priority Transit Corridors are not specifically defined but generally include frequent and reliable service, and bus stop amenities which can include wider waiting areas, shelters, seating, shade, good illumination, accessibility for the disabled, and buffers from moving traffic.

L5	Transit Service / Ped Amenities	Priority Transit Corridor: 220th Street SW from 76th Avenue W to proposed light rail transit station at I-5 interchange. (See Figure 10 on page 23 for Priority Transit Corridors designations in the subarea)
L6	Transit Service / Ped Amenities	Priority Transit Corridor: 238th Street SW from SR 104 to SR 99. (See Figure 10 on page 23 for Priority Transit Corridors designations in the subarea)
L7	Transit Service / Ped Amenities	Improve local bus stop northbound at 240th (provide seating, shelter, refuge can, lighting, etc.) concurrent with new development at 240th Street node.
L8	Transit Service / Ped Amenities	Improve local bus stop southbound at 240th (provide seating, shelter, refuge can, lighting, etc.) concurrent with new development at 240th Street node.
L9	Transit Service Efficiency	Extend BAT lanes on SR 99 onto overcrossing of SR 104 and continuing to 244th Street.

IMPROVE TRAFFIC FLOW AND GENERAL SAFETY WITH ACCESS MANAGEMENT

MAP IDENTIFIER	IMPROVEMENT TYPE	DESCRIPTION OF IMPROVEMENT
L10	Access Management	238th Street to 236th Street. Install raised median and limited c-curb on SR 99.
L11	Access Management	228th Street to 224th Street. Install raised median and limited c-curb on SR 99.
L12	Access Management	220th Street to 216th Street. Install raised median and limited c-curb on SR 99.
L13	Access Management	216th Street to 212th Street. Install raised median and limited c-curb on SR 99.



PROJECT SUPPORTIVE



PLANNED IMPROVEMENT



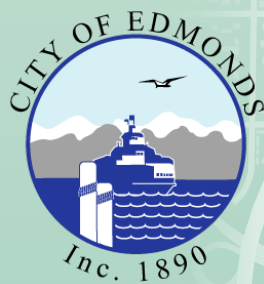
OTHER

IMPROVE TRANSIT MOBILITY AND TRANSIT STOP ENVIRONMENT	
NOTES	SOURCE
These priority corridors would emphasize good daily transit service and bus stop amenities to make transit attractive.	Comprehensive Plan 2015 Transportation Element
These priority corridors would emphasize good daily transit service and bus stop amenities to make transit attractive.	Comprehensive Plan 2015 Transportation Element
This improvement most likely needed when higher intensity redevelopment occurs within the 240th node.	Workshop
This improvement would best benefit from the installation of a traffic signal at SR 99 / 240th Street.	Workshop and DKS Associates
Extension of the BAT lanes through the bottleneck created by the four-lane overcrossing provides additional efficiency for SWIFT service which has experienced a drop in reliability due to congestion within the corridor.	Community Transit

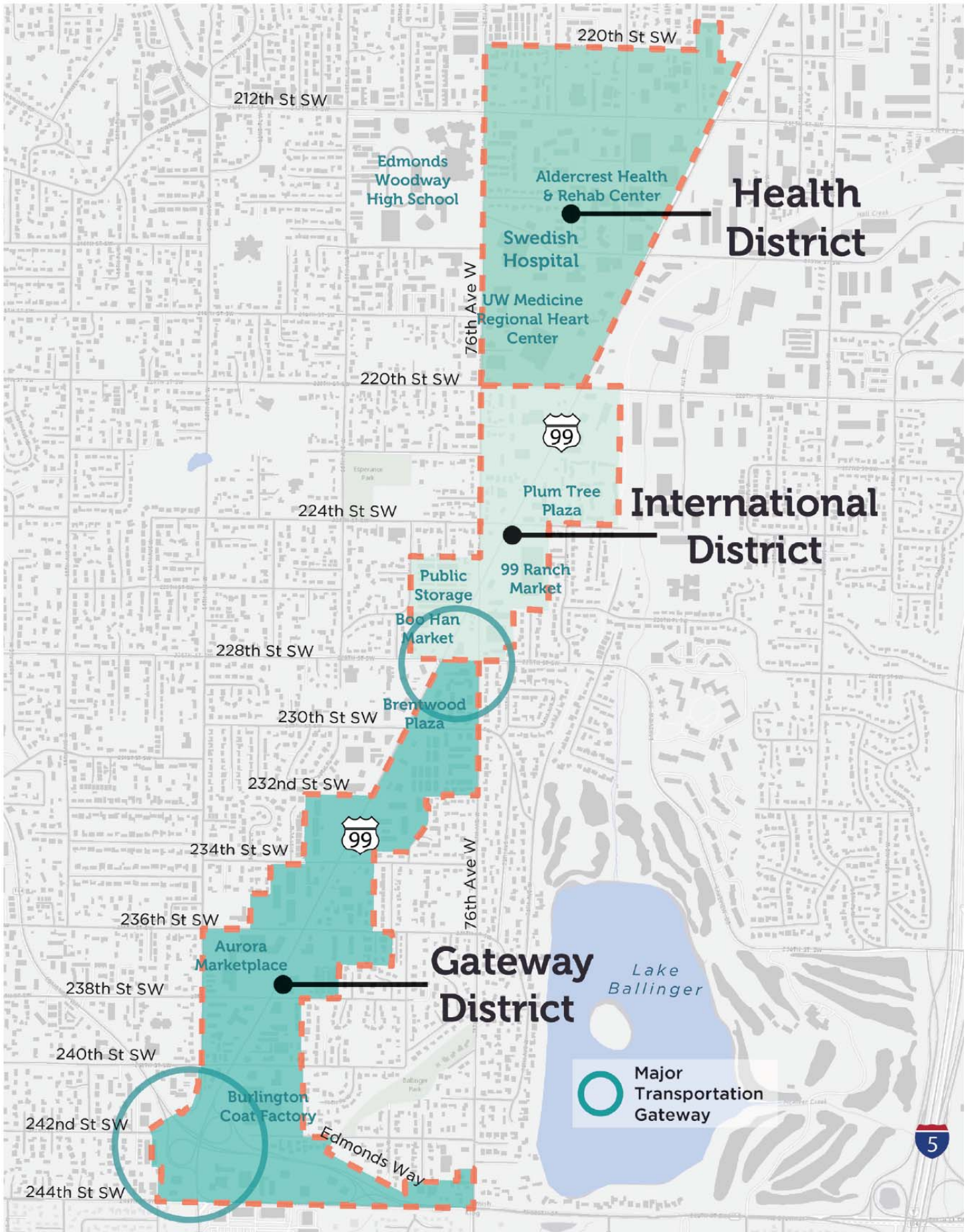
IMPROVE TRAFFIC FLOW AND GENERAL SAFETY WITH ACCESS MANAGEMENT	
NOTES	SOURCE
For long segments with numerous driveways, use intermittently placed medians to allow left turn in/out functions at key driveways. Access managed segments must permit u-turns at adjacent signalized intersections. Use median to restrict left turns from stop-controlled side streets with high collision histories unless the restriction conflicts with a short-term improvement or other proposed change in traffic control.	SR 99 Access Management and Cross Section Focused Assessment (2015) and Workshop

APPENDIX A: PLANNED ACTION ENVIRONMENTAL IMPACT STATEMENT

**Edmonds Highway 99 Subarea Plan
JUNE 2017**



Attachment B



ORDINANCE NO. _____

AN ORDINANCE OF THE CITY OF EDMONDS, WASHINGTON, ADOPTING AMENDMENTS TO THE EDMONDS COMMUNITY DEVELOPMENT CODE, AMENDING CHAPTER 16.60 ENTITLED "CG – GENERAL COMMERCIAL ZONE," AND ECDC 20.60.045 ENTITLED "FREESTANDING SIGNS – REGULATIONS," REZONING CERTAIN PROPERTY TO CG – GENERAL COMMERCIAL ZONE, AND FIXING A TIME WHEN THE SAME SHALL BECOME EFFECTIVE.

WHEREAS, on May 10, 2017, the Planning Board held a public hearing on the draft regulations and, after discussion, recommended their adoption by the City Council; and

WHEREAS, the Board also held a supplemental public hearing on June 14, 2017 and, after formally considering the factors required for zoning map (rezone) amendments, recommended approval by the Council; and

WHEREAS, the City Council reviewed the recommended development regulations and zoning map changes on June 6, 2017; and

WHEREAS, on June 20, 2017 the City Council held a public hearing on the Highway 99 Area development regulations and zoning map amendments; and

WHEREAS, the City Council again reviewed the draft development regulations on July 18, 2017 and July 31, 2017; and

WHEREAS, this proposal constitutes an area-wide rezone due to the size of the affected area and the number of different parcels and ownerships involved; and

WHEREAS, the proposed development regulations for the CG chapter of the city code (*see Attachment 1*) implement the Highway 99 Subarea Plan and establish new standards, including new requirements to require electric vehicle charging stations, encourage transit use

and pedestrian activity, and add clear design requirements, including for site layout, landscaping, amenity space, and building design; and

WHEREAS, pursuant to ECDC 20.40.010, at least the following factors shall be considered in reviewing a proposed rezone:

A. Comprehensive Plan. Whether the proposal is consistent with the comprehensive plan;

B. Zoning Ordinance. Whether the proposal is consistent with the purposes of the zoning ordinance, and whether the proposal is consistent with the purposes of the proposed zone district;

C. Surrounding Area. The relationship of the proposed zoning change to the existing land uses and zoning of surrounding or nearby property;

D. Changes. Whether there has been sufficient change in the character of the immediate or surrounding area or in city policy to justify the rezone;

E. Suitability. Whether the property is economically and physically suitable for the uses allowed under the existing zoning, and under the proposed zoning. One factor could be the length of time the property has remained undeveloped compared to the surrounding area, and parcels elsewhere with the same zoning;

F. Value. The relative gain to the public health, safety and welfare compared to the potential increase or decrease in value to the property owners; and

WHEREAS, the city council finds, after considering the above factors and the Planning Board's recommendation, particularly including the proposed findings that were accepted by the Planning Board on June 14, 2017, that the proposal should be approved; NOW, THEREFORE,

THE CITY COUNCIL OF THE CITY OF EDMONDS, WASHINGTON, DO
ORDAIN AS FOLLOWS:

Section 1. Chapter 16.60 of the Edmonds Community Development Code, entitled “CG – GENERAL COMMERCIAL ZONE,” is hereby amended to read as set forth in **Attachment 1**, which is attached hereto and incorporated herein by this reference as if set forth in full.

Section 2. Section 20.60.045 of the Edmonds Community Development Code, entitled “Freestanding signs -- Regulations,” is hereby amended to read as set forth in **Exhibit 2**, which is attached hereto and incorporated herein by this reference as if set forth in full.

Section 3. That certain real property depicted on **Exhibit 3**, which is attached hereto and incorporated herein by this reference as if set forth in full, is hereby rezoned to CG – GENERAL COMMERCIAL ZONE.

Section 4. The Development Services Director or her designee is hereby authorized and directed to make appropriate amendments to the Edmonds Zoning Map in order to properly designate the rezoned property as “CG – GENERAL COMMERCIAL ZONE” pursuant to Section 3 of this ordinance.

Section 5. Effective Date. This ordinance, being an exercise of a power specifically delegated to the City legislative body, is not subject to referendum, and shall take effect five (5) days after passage and publication of an approved summary thereof consisting of the chapter.

APPROVED:

MAYOR DAVID O. EARLING

ATTEST/AUTHENTICATED:

CITY CLERK, SCOTT PASSEY

APPROVED AS TO FORM:
OFFICE OF THE CITY ATTORNEY:

BY _____
JEFFREY B. TARADAY

FILED WITH THE CITY CLERK:
PASSED BY THE CITY COUNCIL:
PUBLISHED:
EFFECTIVE DATE:
ORDINANCE NO. _____

SUMMARY OF ORDINANCE NO. _____

of the City of Edmonds, Washington

On the ____ day of _____, 2017, the City Council of the City of Edmonds, passed Ordinance No. _____. A summary of the content of said ordinance, consisting of the title, provides as follows:

AN ORDINANCE OF THE CITY OF EDMONDS, WASHINGTON, ADOPTING AMENDMENTS TO THE EDMONDS COMMUNITY DEVELOPMENT CODE, AMENDING CHAPTER 16.60 ENTITLED "CG – GENERAL COMMERCIAL ZONE," AND ECDC 20.60.045 ENTITLED "FREESTANDING SIGNS – REGULATIONS," REZONING CERTAIN PROPERTY TO CG – GENERAL COMMERCIAL ZONE, AND FIXING A TIME WHEN THE SAME SHALL BECOME EFFECTIVE.

The full text of this Ordinance will be mailed upon request.

DATED this ____ day of _____, 2017.

CITY CLERK, SCOTT PASSEY

Chapter 16.60
CG – GENERAL COMMERCIAL ZONE

EXHIBIT 1

Sections:

- 16.60.000 CG zone.
- 16.60.005 Purposes.
- 16.60.010 Uses.
- 16.60.015 Location standards for sexually oriented businesses.
- 16.60.020 Site development standards – General.
- 16.60.030 Site development standards – Design.
- 16.60.040 Operating restrictions.

16.60.000 CG zone.

- A. This chapter establishes the general commercial zoning district.
- B. Definitions. For purposes of this chapter, the following definitions apply.
 - 1. Amenity space: outdoor space for uses that are considered to provide an amenity or benefit to people
 - 2. Auto sales use: facilities for the commercial sale of motor vehicles, including buildings and areas typically associated with auto sales use, such as areas for the display and storage of automobiles that are sold or serviced as part of the overall auto sales use.
 - 3. Frontage: the front part of a property or building adjacent to a street
 - 4. Primary frontage (or “primary street frontage”): the frontage for a property that is adjacent to only one street or, for a property that is adjacent to more than one street, the frontage that is adjacent to the street that is considered primary over any other streets to which the property is adjacent.
 - 5. Stepback: The upper portion of a building that is required to be set (or stepped) further back than the minimum setback otherwise required by ECDC 16.60.020.A.
- C. Where this chapter conflicts with any other, this chapter shall prevail for the general commercial district.

16.60.005 Purposes.

The CG zone has the following specific purposes in addition to the general purposes for business and commercial zones listed in Chapter 16.40 ECDC:

- A. Encourage economic vitality through businesses, investment, redevelopment, and efficient use of land;
- B. Encourage safe and comfortable access for pedestrians, transit, and motorists;

- C. Encourage attractive mixed use development, affordable housing, and a variety of commercial uses; and
- D. Recognize the district's evolving identity and sense of place, including distinctions between different parts of the district, and be sensitive to adjacent residential zones.

16.60.010 Uses.

A. Permitted Primary Uses.

1. All permitted or conditional uses in any other zone in this title, except as specifically prohibited by subsection (C) of this section or limited by subsections (B) and (D) of this section;
2. Halfway houses;
3. Sexually oriented businesses, which shall comply with the location standards set forth in ECDC 16.60.015, the development regulations set forth in Chapter 17.50 ECDC, and the licensing regulations set forth in Chapter 4.52 ECC.

B. Permitted Secondary Uses.

1. Off street parking and loading areas to serve a permitted use.
2. Indoor storage facilities that either comprise less than 40% of a permitted primary use of the building in which they are located or are in a separate accessory building or buildings comprising less than 40% of the total leasable building space used for the parcel's permitted primary use(s).
3. Outdoor storage areas that are integral to a permitted primary use, such as storage or display areas for automobile sales, building materials or building supply sales, or garden/nursery sales, provided that such outdoor uses are screened from adjacent residential zoning districts.

C. Prohibited Uses.

1. Mobile home parks.
2. Storage facilities or outdoor storage areas intended as a primary use, not secondary to a permitted use. Automobile wrecking yards, junk yards, or businesses primarily devoted to storage or mini storage are examples of this type of prohibited use.

D. Uses Requiring a Conditional Use Permit.

1. Aircraft landings as regulated by Chapter 4.80 ECC. [Ord. 3981 § 1 (Att. A), 2014; Ord. 3635 § 1, 2007].

16.60.015 Location standards for sexually oriented businesses: All sexually oriented businesses shall comply with the requirements of this section, the development regulations set forth in Chapter 17.50 ECDC, and Chapter 4.52 ECC. The standards established in this section shall not be construed to restrict or prohibit the following activities or products: (1) expressive dance; (2) plays, operas, musicals, or other dramatic works; (3) classes, seminars, or lectures conducted for a scientific or educational purpose; (4) printed materials or visual representations intended for educational or scientific purposes; (5) nudity within a locker room or other similar facility used for changing clothing in connection with athletic or exercise activities; (6) nudity within a hospital, clinic, or other similar medical facility for health-related

purposes; and (7) all movies and videos that are rated G, PG, PG13, R, and NC17 by the Motion Picture Association of America.

A. Separation Requirements. A sexually oriented business shall only be allowed to locate where specifically permitted and only if the following separation requirements are met:

1. No sexually oriented business shall be located closer than 300 feet to any of the following protected zones, whether such protected zone is located within or outside the city limits:

- a. A residential zone as defined in Chapter 16.10 ECDC;
- b. A public use zone as defined in Chapter 16.80 ECDC.

2. No sexually oriented business shall be located closer than 300 feet to any of the following protected uses, whether such protected use is located within or outside the city limits:

- a. A public park;
- b. A public library;
- c. A nursery school or preschool;
- d. A public or private primary or secondary school;
- e. A church, temple, mosque, synagogue, or other similar facility used primarily for religious worship;
- f. A community center such as an amusement park, public swimming pool, public playground, or other facility of similar size and scope used primarily by children and families for recreational or entertainment purposes;
- g. A permitted residential use located in a commercial zone;
- h. A museum; and
- i. A public hospital or hospital district.

3. No sexually oriented business shall be located closer than 500 feet to any bar or tavern within or outside the city limits.

B. Measurement. The separation requirements shall be measured by following a straight line from the nearest boundary line of a protected zone specified in subsection (A) of this section or nearest physical point of the structure housing a protected use specified in subsection (A) of this section to the nearest physical point of the tenant space occupied by a sexually oriented business.

C. Variance from Separation Requirements. Variances may be granted from the separation requirements in subsection (A) of this section if the applicant demonstrates that the following criteria are met:

- 1. The natural physical features of the land would result in an effective separation between the proposed sexually oriented business and the protected zone or use in terms of visibility and access;
- 2. The proposed sexually oriented business complies with the goals and policies of the community development code;

3. The proposed sexually oriented business is otherwise compatible with adjacent and surrounding land uses;
4. There is a lack of alternative locations for the proposed sexually oriented business; and
5. The applicant has proposed conditions which would minimize the adverse secondary effects of the proposed sexually oriented business.

D. Application of Separation Requirements to Existing Sexually Oriented Businesses. The separation requirements of this section shall not apply to a sexually oriented business once it has located within the city in accordance with the requirements of this section. [Ord. 3981 § 1 (Att. A), 2014; Ord. 3635 § 1, 2007].

16.60.020 Site development standards – General.

A. Table. Except as hereinafter provided, development requirements shall be as follows:

Dimensional Requirements Table						
	Minimum Lot Area	Minimum Lot Width	Minimum Street Setback	Minimum Side/Rear Setback	Maximum Height	Maximum Floor Area
CG	None	None	5'/10' ²	0/15 ¹	75' ³	None

¹ Fifteen feet from all lot lines adjacent to RM or RS zoned property; otherwise no setback is required by this subsection..

² The 5' minimum width applies only to permitted outdoor auto sales use; otherwise the minimum is 10'.

³ None for structures located within an area designated as a high-rise node on the comprehensive plan map.

B. Maximum height for purposes of this chapter need not include railings, chimneys, mechanical equipment or other exterior building appurtenances that do not provide interior livable space. In no case shall building appurtenances together comprise more than 20 percent of the building surface area above the maximum height.

C. Pedestrian area.

1. For purposes of this chapter, the pedestrian area described herein is the area adjacent to the street that encompasses the public right of way from the edge of the curb (or, if no curb, from the edge of pavement) and the street setback area, as identified in Table A of this section.

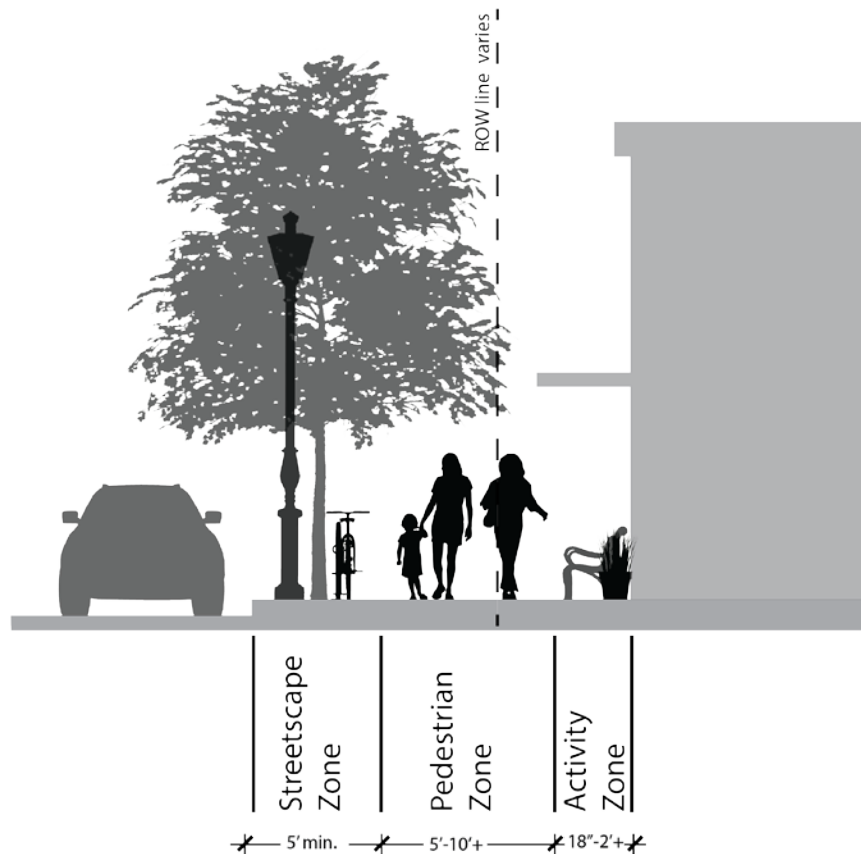
2. The pedestrian area is composed of three zones: the activity zone, the pedestrian zone, and the streetscape zone. Providing improvements to the pedestrian area, as needed to be consistent with this subsection on at least the primary street, is required as part of development projects, excluding development that would not add a new building or that consists of building improvements that do not add floor area equaling more than 10% of the building's existing floor area or that consists of additional parking stalls that comprise less than 10% of the existing parking stalls or that consists of development otherwise exempted under this chapter.

- a. Activity Zone. The activity zone shall be the open-air pedestrian area from the building front to the edge of the pedestrian zone. The activity zone is the section of the pedestrian area that is reserved for activities that commonly occur immediately adjacent to the building facade. Typical amenities or activities included in the activity

zone include, but are not limited to, sidewalks, benches, potted plants, outdoor dining and shopping. The area shall be paved to connect with the pedestrian zone in an ADA-accessible manner. Stairs, stoops and raised decks or porches may be constructed in a portion of the activity zone.

b. Pedestrian Zone. The pedestrian zone is located between the activity zone and the streetscape zone. The pedestrian zone consists of a minimum 5-foot clear and unobstructed path for safe and efficient through-traffic for pedestrians. Architectural projections and outdoor dining may be permitted to encroach into the pedestrian zone only where a minimum 5-foot clear path and 7-foot vertical clearance is maintained within the pedestrian zone.

c. Streetscape Zone. The streetscape zone is located between the curb or pavement edge to the edge of the pedestrian zone and shall be a minimum of 5 feet wide. The streetscape zone is the section that is reserved for pedestrian use and for amenities and facilities that commonly occur between the adjacent curb or pavement edge and pedestrian through-traffic. Typical amenities and facilities in the streetscape zone include, but are not limited to, street trees, street lights, benches, bus stops, and bike racks. Street trees shall be required in conformance with the Edmonds Street Tree Plan.

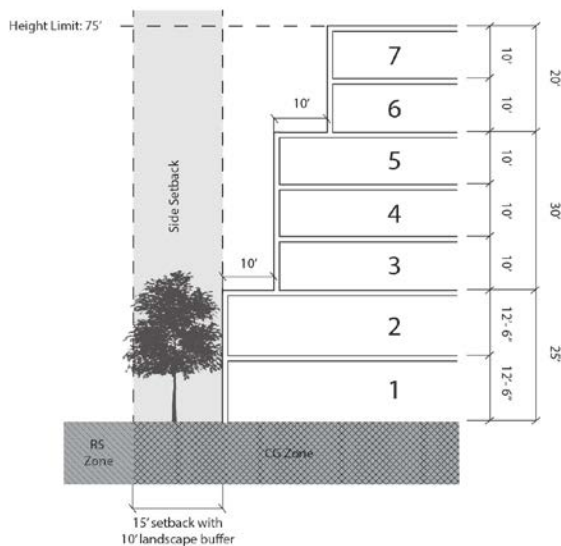


Note: Numerical Ranges for the Pedestrian Zone and the Activity Zone are typical but do not control over other requirements of this chapter.

(Illustration: Pedestrian Area)

B. Building setback when adjacent to RS Zones

1. The portion of the buildings above 25 feet in height shall step back no less than 10 feet from the required setback to an adjacent RS zone. That portion of the building over 55 feet in height shall be step back no less than 20 feet from the required setback to an adjacent RS Zone.
2. Balconies, railings, parapets and similar features that do not enclose an interior space may extend into the setback area in order to encourage more human activity and architectural features.



(Illustration: Setback and “Stepback” of building adjacent to RS Zones)

16.60.030 Site development standards – Design.

Design review by the architectural design board is required for any project that includes buildings exceeding 75 feet in height as identified in ECDC 16.70.020. Projects not exceeding this height may be reviewed by staff as a Type I decision. Regardless of what review process is required, all projects proposed in the CG zone must meet the design standards contained in this section.

A. Screening and Buffering.

1. General.

- a. Retaining walls facing adjacent property or public rights of way shall not exceed 7 feet in height. A minimum of 4 feet of planted terrace is required between stepped wall segments.
- b. Tree landscaping may be clustered to soften the view of a building or parking lot, yet allow visibility to signage and building entry.

- c. Stormwater facilities shall be designed to minimize visual impacts and integrate landscaping into the design.
- d. All parking lots are required to provide Type V interior landscaping, consistent with Chapter 20.13 ECDC
- e. Type I landscaping is required for commercial, institutional and medical uses adjacent to single family or multifamily zones. The buffer shall be a minimum of 10 feet in width and continuous in length.
- f. Type I landscaping is required for residential parking areas adjacent to single family zones. The buffer shall be a minimum of 4 feet in width and continuous in length.
- g. Type I landscaping is required for commercial and multifamily uses adjacent to single family zones. The buffer shall be a minimum of four feet in width and 10 feet in height and continuous in length.
- h. If there is a loading zone and/or trash compactor area next to a single family or multifamily zone, there shall be a minimum of a six-foot-high masonry wall plus a minimum width of 5 feet of Type I landscaping. Trash and utility storage elements shall not be permitted to encroach within street setbacks or within setbacks adjacent to single family zones. Mechanical equipment, including heat pumps and other mechanical elements, shall not be placed in the setbacks.
- i. Landscape buffers, Type I, shall be used along the edge of parking areas adjacent to single family zones.
- j. Outdoor storage areas for commercial uses must be screened from adjacent RS zones.

2. Parking Lots Abutting Streets.

- a. Type IV landscaping, minimum 5 feet wide, is required along all street frontages where parking lots, excluding for auto sales use, abut the street right-of-way.
- b. For parking lots where auto sales uses are located, the minimum setback area must be landscaped to include a combination of vegetation and paved pedestrian areas.
- c. All parking located under the building shall be completely screened from the public street by one of the following methods:
 - i. Walls that have architectural treatment meeting at least three of the elements listed in ECDC 16.60.030.D.2.e;
 - ii. Type III planting and a grill that is 25 percent opaque; or
 - iii. Grill work that is at least 80 percent opaque.

B. Parking, Access, and Bicycle Storage Standards.

1. Parking requirements. Vehicle parking shall be provided as follows:

- a. Nonresidential uses, one space per 500 square feet of leasable building space; and
- b. Residential uses, an average of 0.75 space per unit that is less than 700 square feet, an average of 1.25 parking spaces per unit that is between 700 and 1,100 square feet, and otherwise 1.75 spaces per unit.
- c. In addition, guest parking for residential uses at a minimum ratio of one guest space for every twenty required parking spaces.
- d. For mixed use development, a portion of the parking spaces may be shared between residential and commercial uses provided the director finds that the proposal is supported by a parking study and/or nationally recognized parking standards and that the site plan assures access for all shared parking uses.

- e. Parking meeting the non-residential parking requirements shall be open to the public throughout business operating hours.
- 2. The first 3,000 square feet of commercial space in a mixed-use development with a shared parking plan is exempt from off street parking requirements.
- 3. The development services director may approve a different ratio for the vehicle parking required by standards of subsection B.1 of this section when an applicant submits parking data illustrating that the standards do not accurately apply to a specific development. The data submitted for an alternative parking ratio shall include, at a minimum, the size and type of the proposed development, and the anticipated peak and average parking loads of all uses. The director may approve a parking ratio that is based on the specific type of development and its primary users in relationship to:
 - a. An analysis conducted using nationally recognized standards or methodology, such as is contained in the Urban Land Institute's most recent version of the publication "Shared Parking" or the latest version of the Institute of Transportation Engineers publication "Parking Generation"; or
 - b. A site-specific parking study that includes data and analysis for one or more of the following:
 - i. 1/4 mile proximity to a bus rapid transit station and methodology that takes into account transit-oriented development;
 - ii. Use of transportation demand management policies, including but not limited to free or subsidized transit passes for residents and workers;
 - iii. On-site car-share and bike-share facilities; or
 - iv. Uses that serve patients, clients, or tenants who do not have the same vehicle parking needs as the general population.
 - v. Other methods that reduce the need for vehicle parking.
- 4. All off-street surface parking shall be located to the side or rear of the primary building, except as otherwise allowed by this chapter, and shall be screened from the sidewalk by a wall or plantings between 2 to 4 feet in height. Outdoor parking areas shall comprise 40% or less of the public street frontage area within 100 feet of the primary street for the lot or tract and, on corner lots, may not be located at the corner. The requirements of this subsection do not apply to permitted auto sales uses.
- 5. Electric vehicle charging stations: One or more electric vehicle charging stations must be provided for all new development that includes housing. Required charging stations shall be installed to serve at least 10% of the required residential parking stalls. In addition, either additional stations or planned capacity (or a combination thereof) that can double the amount of initially required stations shall be provided. For this subsection, "planned capacity" means site design and construction that includes electrical wiring connection and ventilation, compliant with the City's building codes, to support potential or actual future electric vehicle charging stations.
- 6. Bicycle storage spaces: Bicycle storage spaces for multifamily housing, excluding housing for assisted living or other specialized facilities where the development services director finds that the targeted population is not likely to use bicycles, shall be provided for residents at a ratio of 1 bicycle storage space for each residential unit under 700 square feet and 2 bicycle storage spaces for each residential unit greater than 700 square feet. Bicycle storage spaces shall consist of storage racks, lockers, or other secure space to accommodate sheltered, safe, and

convenient bicycle storage for building residents. Such space may be in a vehicle parking garage or another appropriate location but shall not be provided as open storage on a deck or balcony. Where sheltered bicycle storage is being provided within a dedicated common space of the building, the total number of required bicycle storage spaces may be reduced by up to 50% from that which is otherwise required, provided that one or more secure bicycle racks, useable by visitors, for at least four bicycles is provided within the front setback of the property.

7. Driveways accessing Highway 99: All driveway connections to Highway 99 must meet the applicable requirements of the Washington State Department of Transportation, including minimum requirements for distance between driveway access connections, which may be up to 250 feet to help promote traffic safety and minimize pedestrian-vehicle conflicts.

8. Paths within Parking Lots.

- a. Pedestrian paths in parking lots shall be delineated by separate paved routes that meet federal accessibility requirements and that use a variation in textures and/or colors and may include landscape barriers and landscape islands.
- b. Pedestrian paths shall be provided at least every 180 feet within parking lots. These shall be designed to provide access to onsite buildings as well as to pedestrian walkways that border the development.
- c. Pedestrian paths shall be a minimum of six feet in width and shall be separated from the parking area either horizontally or vertically (e.g. with curbs). Where paths cross vehicular lanes, raised traffic tables should be considered if feasible.
- d. Parking lots shall have pedestrian connections to the main sidewalk at a minimum of every 100 feet.

9. Bonus for Parking Below or Above Ground Floor.

- a. For projects where at least 50 percent of the parking is below or above the ground floor of the building, the following standards may be applied regardless of any ECDC standards that otherwise conflict:
 - i. The minimum drive aisle width may be reduced to 22 feet.
 - ii. The maximum ramp slope may be increased to 20 percent.
 - iii. A mixture of full and reduced width parking stalls may be provided without demonstrating the stalls could also be provided at full width dimensions.

10. Drive –through facilities. Drive-through facilities such as, but not limited to, banks, cleaners, fast food, drug stores, and espresso stands, shall comply with the following:

- a. Drive-through windows and stacking lanes shall not be located along the facades of the building that face a street.
- b. No more than one direct entrance or exit from the drive-through shall be allowed as a separate curb cut onto an adjoining street.

11. Pedestrian and Transit Access.

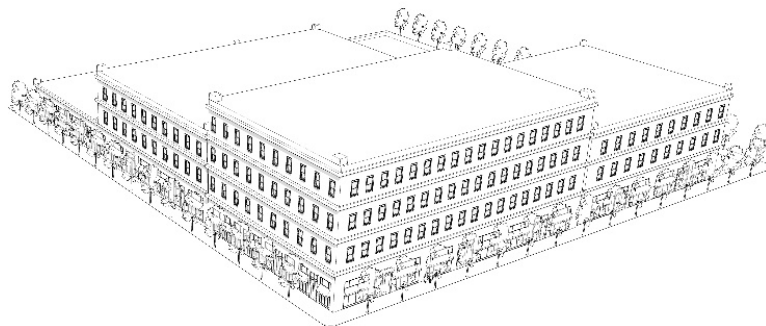
- a. Pedestrian building entries must connect directly to the public sidewalk and to

- adjacent developments if feasible.
- b. Internal pedestrian routes shall extend to the property line and connect to existing pedestrian routes where applicable. Potential future connections shall also be identified such that pedestrian access between developments can occur without walking in the parking or access areas.
- c. Where a transit station or bus stop is located in front of or adjacent to a parcel, pedestrian connections linking the station or stop directly to the development are required.
- d. Pedestrian routes shall connect buildings on the same site to each other.

C. Site Design and Layout: Overall, the design and use of each site shall be based on the building/street relationship and on the integration of pedestrian features. This will take the form of either a Pedestrian Oriented Design Area or an Alternative Walkable Design Area, as described below in subsections 1 and 2 of this section, provided that an exceptions process, pursuant to Subsection 3 below, may be allowed under the provisions of this section. Additional site design and layout standards in this section must also be met.

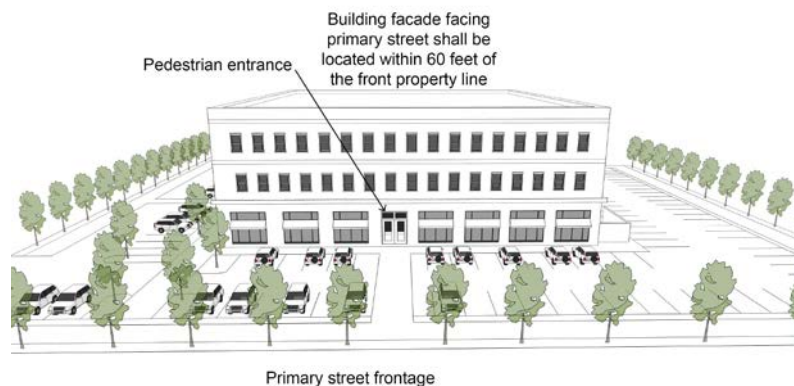
1. Pedestrian Oriented Design Area: Unless otherwise permitted under subsections 2 or 3 of this section, development must meet the requirements of this subsection for a pedestrian-oriented design area.

- a. Primary Frontage. At least 50% of a building's façade facing the primary public street shall be located within 20 feet of the property line where the primary street frontage exists. The illustration below provides an example of this concept. The requirement does not apply to buildings that are behind another building on the same lot when the other building has a footprint of at least 3000 square feet and has met the requirement. Where site constraints preclude strict compliance with the requirement, the building line shall be measured one foot behind the line created by that constraint. On a corner lot or a lot with frontages on multiple streets, the development services director shall determine the primary street frontage considering the following:
 - i. The street classification of the adjacent streets;
 - ii. The prevailing orientation of other buildings in the area;
 - iii. The length of the block face on which the building is located; or
 - iv. Unique characteristics of the lot or street.
- b. The building must include a prominent pedestrian entry on the primary frontage. Vehicle parking, other than where permitted for vehicle sales use, shall not be located within the first 20 feet of the primary street frontage. The first 20 feet of the primary street frontage may include building space, landscaping, artwork, seating areas, outdoor displays, and pedestrian and bicycle facilities.



2. Alternative Walkable Design Area Option: An alternative to the pedestrian-oriented design area requirements of subsection 1 in this section may be allowed by the development services director only for sites that the director has found to have unique and significant constraints related to pedestrian access and for which a phased design plan to increase pedestrian access and connectivity has been submitted to the development services department. While they currently may be largely auto-oriented, Walkable Design Areas have a high potential for walking, bicycling and transit service. If a development is allowed to use this standard, it shall be subject to the requirements of this subsection.

- a. Building Placement: For any new building permitted on a property after August 1, 2017, a minimum of 50% of the building's façade facing the primary street shall be located within 60 feet of the front property line or within 65 feet where a 5-foot landscaping area is provided between the parking lot and the sidewalk. When site constraints preclude strict compliance with this requirement, the building line shall be measured one foot behind the line created by that constraint.
- b. On a corner lot or a lot with frontages on multiple streets, the development services director shall determine the primary street frontage considering the following:
 - i. The street classification of the adjacent streets;
 - ii. The prevailing orientation of other buildings in the area;
 - iii. The length of the block face on which the building is located;
 - iv. The location of any alley or parking areas; or
 - v. Unique characteristics of the lot or street.
- c. No more than one double-sided row of parking spaces shall be allowed in the front of a building on its primary frontage.
- d. A pedestrian entrance must be located on the primary frontage.



- e. Required Amenity spaces, under subsection 4 below, shall be located to connect the building to the street as much as practicable, provided that amenity space may also be located between buildings where the space will be used in common.

3. Exceptions Process for Pedestrian or Walkable Design: An exception to the exact requirements of subsections 1 or 2 of this section may be allowed by the hearing examiner under a Type IIIA decision process to provide for design flexibility that still encourages pedestrian orientation and efficient land uses when the following criteria are met:

- a. The property is located within 300 feet of a highway interchange and has unique pedestrian access constraints or is primarily used for motor vehicle sales;
- b. The development provides business and pedestrian areas that are near the primary street frontage and likely to be active throughout the day and evening;
- c. The development features a prominent building entry for pedestrian use that is highly visible and connected by a well-lit walkway from the primary street frontage;
- d. At least 25% of the required amenity space shall be located to connect the building to the street in a manner that encourages pedestrian use and include seating, landscaping, and artwork
- e. Where a site has multiple buildings (excluding accessory utility buildings), 50% or more of the required amenity space shall be located between buildings to allow for shared use.
- f. No more than 50% of vehicle parking, other than that associated with a permitted vehicle sales facility use, may be located within 20 feet of the front property line.
- g. One or more buildings on the site must have at least two stories of useable space.

4. Amenity space. Amenity space is intended to provide residents, employees, and visitors with places for a variety of outdoor activities.

- a. An area equivalent to at least 5% of the building footprint shall be provided as amenity space. If a vehicle parking area is being added to the site without the concurrent development of a building of at least 2,000 square feet, amenity space must be provided to equal at least 5% of the additional parking area.
- b. The amenity space shall be outdoor space that incorporates pedestrian-oriented features, such as, but not limited to, seating, paths, gazebos, dining tables, pedestrian-scale lighting, and artwork. A minimum of 10% of the required amenity space shall be comprised of plantings, which may include tree canopy areas and other shade or screening features. Native vegetation is encouraged.
- c. The majority of the required amenity space must be provided in one or more of the following forms:
 - i. Recreation areas: an open space available for recreation. The area may be spatially defined by landscaping rather than building frontages. Its surface shall consist primarily of hardy groundcover or a material conducive to playground or recreational use. Decorative landscape features, such as flower

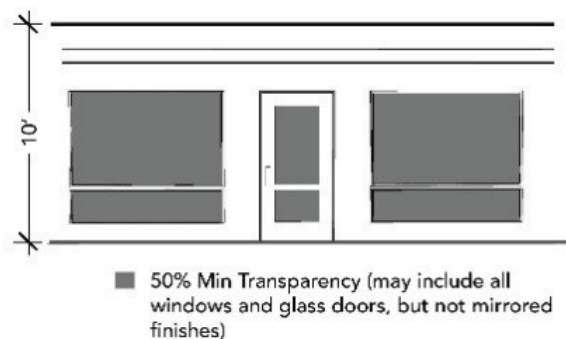
- beds, shall not comprise more than 15% of the total area.
- ii. Plazas: an open space available for community gathering and commercial activities. A plaza shall be spatially defined primarily by either building facades, with strong connections to interior uses, or close proximity to the public sidewalk, especially at the intersection of streets. Its surface shall be primarily hardscape, provided that trees, shade canopies, and other landscaping, as well as water features and artwork, may add visual or environmental features to the space.
 - iii. Squares or courtyards: an open space available for unstructured recreation or community gathering purposes. A square is spatially defined by building facades with strong connections to interior uses. Its surface shall be primarily hardscape, supplemented by trees and other landscaping. Water features and artwork are optional.
 - iv. Exception: A community garden may comprise a portion of any amenity space, provided that it:
 - 1) is located more than 20 feet from a primary street frontage;
 - 2) is dedicated to ongoing use by residents of the site, including for growing edible produce; and
 - 3) includes facilities for watering the garden and storing garden supplies.
5. Lighting: All lighting shall be shielded and directed downward and away from adjacent parcels. This may be achieved through lower poles at the property lines and/or full “cut off” fixtures.
- a. Parking lots shall have lighting poles that are a maximum of 25 feet in height. Pedestrian paths or walkways and outdoor steps shall have pedestrian-scaled lighting focused on the travel path. Pole height shall be a maximum of 14 feet, although lighting bollards are preferred.
 - b. For pedestrian paths and walkways on internal portions of the site, solar-powered lighting may be sufficient.
 - c. Entries shall have lighting for safety and visibility integrated with the building/canopy.

D. Building Design Standards

1. General. To provide variety and interest in appearance, the following design elements should be considered, and a project shall demonstrate how at least four of the elements will be used to vary the design of the site:
- a. Building massing and unit layout,
 - b. Placement of structures and setbacks,
 - c. Location of pedestrian and vehicular facilities,
 - d. Composition and character of open space, plant materials and street trees,
 - e. Variety in architectural elements, façade articulation, and/or building materials,
 - f. Roof variation in slope, height and/or materials.

2. Building Design and Massing.

- a. Buildings shall convey a visually distinct “base” and “top, which may be achieved through differences in massing elements and/or architectural details.
- b. The bulk and scale of buildings of over 3,000 square feet in footprint shall be mitigated through the use of massing and design elements such as façade articulation and modulation, setbacks, step-backs, distinctive roof lines or forms, and other design details.
- c. Primary Frontage: On the primary frontage, to provide visual connection between activities inside and outside the building, 50% of the building façade between 2 and 10 feet in height, as measured from the adjacent sidewalk, shall be comprised of



windows or doors that are transparent, the bottom of which may not be more than 4 feet above the adjacent sidewalk. A departure from this standard may be approved when the façade will not be visible from the public street due to the placement of other buildings on the site, provided that the requirements of subsection “e” in this section shall apply.

- i. On the primary frontage, no vehicle parking shall be located within the first 20 feet of the first level of a building facing the street except where such parking is underground.
- d. All Other Building Frontages: All street-facing facades within 30 feet of a public street, other than for the primary frontage or those facing an alley or the last block of a dead-end street, shall comply with the standard below.
 - i. Thirty percent (30%) of the building façade between 2 and 10 feet in height shall be made of windows or doors that are transparent, the bottom of which may not be more than four feet above the adjacent sidewalk. Windows shall not be mirrored or have glass tinted darker than 40% in order to meet this requirement.
- e. Wall treatment: Building facades not subject to all requirements of ECDC 16.60.030.D.2.c or ECDC 16.60.030.D.2.d are intended to not display blank, unattractive walls to the public or to other building tenants. To accomplish this, walls greater than 30 feet in length shall have architectural treatment that incorporates at least four of the following elements into the design of the facade:
 - i. Masonry (except for flat concrete block).
 - ii. Concrete or masonry plinth at the base of the wall.

- iii. Belt courses of a different texture and color.
- iv. Projecting cornice.
- v. Projecting metal or wood canopy.
- vi. Decorative tilework.
- vii. Trellis containing planting.
- viii. Medallions.
- ix. Artwork or wall graphics.
- x. Vertical differentiation.
- xi. Decorative lighting fixtures.
- xii. Glazing
- xiii. An architectural element not listed above that is approved by the director to meet the intent of this subsection.

16.60.040 Operating restrictions.

A. Enclosed Building. All uses shall be carried on entirely within a completely enclosed building, except the following:

- 1. Public utilities;
- 2. Off street parking and loading areas;
- 3. Drive-in business;
- 4. Secondary uses permitted under ECDC 16.60.010(B);
- 5. Limited outdoor display of merchandise meeting the criteria of Chapter 17.65 ECDC;
- 6. Public markets; provided, that when located next to a single family residential zone, the market shall be entirely within a completely enclosed building;
- 7. Outdoor dining meeting the criteria of Chapter 17.75 ECDC;
- 8. Motorized and nonmotorized mobile vending units meeting the criteria of Chapter 4.12 ECC.

B. Interim Use Status – Public Markets.

1. Unless a public market is identified on a business license as a year-round market within the city of Edmonds, a premise licensed as a public market shall be considered a temporary use. As a temporary activity, any signs or structures used in accordance with the market do not require design review. When a location is utilized for a business use in addition to a public market, the public market use shall not decrease the required available parking for the other business use below the standards established in this chapter.

C. Ongoing Uses.

- 1. Audio equipment at drive-through facilities shall not be audible off site.
- 2. Development subject to the standards of this chapter shall continue to meet the standards of this chapter except as specifically permitted otherwise.

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20.60.045 Freestanding signs – Regulations.

A. Regulation. Permanent freestanding signs are discouraged. Freestanding signs shall be approved only where the applicant demonstrates by substantial evidence that there are no reasonable and feasible alternative signage methods to provide for adequate identification and/or advertisement.

B. Maximum Area. The maximum area of a freestanding sign shall be as follows:

<u>Zone</u>	<u>Maximum Area of Sign</u>
<u>RS, RM</u>	<u>10 square feet (subdivision, PRD, multifamily)</u> <u>4 square feet (individual residence sign)</u>
<u>BN, BP</u>	<u>24 square feet (single)</u> <u>48 square feet (group)</u>
<u>BC, BD,</u> <u>WMU, FVMU</u>	<u>32 square feet (single)</u> <u>48 square feet (group)</u>
<u>CW</u>	<u>32 square feet (single)</u> <u>48 square feet (group)</u>
<u>CG</u>	<u>Sign area shall be governed by subsection (C) of this section</u>

C. Allowable Sign Area for Freestanding Signs – CG Zone. The total allowable sign area for freestanding signs on general commercial sites shall be 56 square feet or one-half square foot of sign area for each lineal foot of street frontage, whichever is greater, up to a maximum of 160 square feet of freestanding sign area. Freestanding signs count against the overall allowable permanent sign area. Multiple business or tenant sites shall further be allowed an additional 24 square feet of freestanding sign area for each commercial tenant or

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occupant in excess of one up to a maximum sign area of 160 square feet. Corner lots choosing to accumulate sign area under the provisions of subsection (E) of this section shall be limited to 160 square feet.

D. Maximum Height. The maximum sign height of freestanding signs shall be as follows:

<u>Zone</u>	<u>Maximum Height of Sign</u>
<u>RS, RM</u>	<u>6 feet</u>
<u>BN, BP, BC, BD, CG, CW, WMU, FVMU</u>	<u>14 feet</u>

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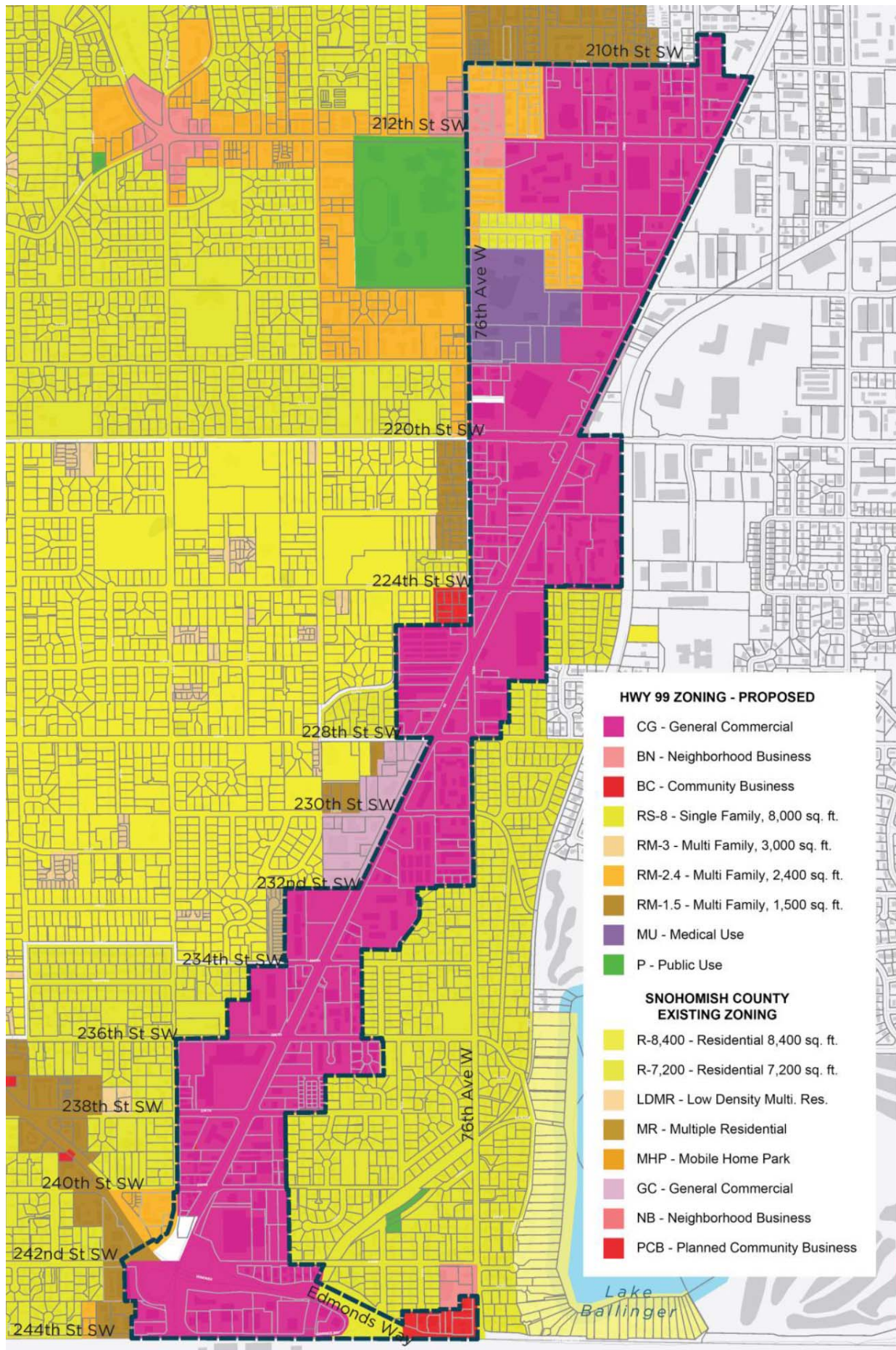
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Exhibit 3



ORDINANCE NO. _____

AN ORDINANCE OF THE CITY OF EDMONDS, WASHINGTON, ADOPTING AMENDMENTS TO THE EDMONDS COMMUNITY DEVELOPMENT CODE, AMENDING CHAPTER 16.60 ENTITLED "CG – GENERAL COMMERCIAL ZONE," AND ECDC 20.60.045 ENTITLED "FREESTANDING SIGNS – REGULATIONS," REZONING CERTAIN PROPERTY TO CG – GENERAL COMMERCIAL ZONE, AND FIXING A TIME WHEN THE SAME SHALL BECOME EFFECTIVE.

WHEREAS, on May 10, 2017, the Planning Board held a public hearing on the draft regulations and, after discussion, recommended their adoption by the City Council; and

WHEREAS, the Board also held a supplemental public hearing on June 14, 2017 and, after formally considering the factors required for zoning map (rezone) amendments, recommended approval by the Council; and

WHEREAS, the City Council reviewed the recommended development regulations and zoning map changes on June 6, 2017; and

WHEREAS, on June 20, 2017 the City Council held a public hearing on the Highway 99 Area development regulations and zoning map amendments; and

WHEREAS, the City Council again reviewed the draft development regulations on July 18, 2017 and July 31, 2017; and

WHEREAS, this proposal constitutes an area-wide rezone due to the size of the affected area and the number of different parcels and ownerships involved; and

WHEREAS, the proposed development regulations for the CG chapter of the city code (*see Attachment 1*) implement the Highway 99 Subarea Plan and establish new standards, including new requirements to require electric vehicle charging stations, encourage transit use

and pedestrian activity, and add clear design requirements, including for site layout, landscaping, amenity space, and building design; and

WHEREAS, pursuant to ECDC 20.40.010, at least the following factors shall be considered in reviewing a proposed rezone:

A. Comprehensive Plan. Whether the proposal is consistent with the comprehensive plan;

B. Zoning Ordinance. Whether the proposal is consistent with the purposes of the zoning ordinance, and whether the proposal is consistent with the purposes of the proposed zone district;

C. Surrounding Area. The relationship of the proposed zoning change to the existing land uses and zoning of surrounding or nearby property;

D. Changes. Whether there has been sufficient change in the character of the immediate or surrounding area or in city policy to justify the rezone;

E. Suitability. Whether the property is economically and physically suitable for the uses allowed under the existing zoning, and under the proposed zoning. One factor could be the length of time the property has remained undeveloped compared to the surrounding area, and parcels elsewhere with the same zoning;

F. Value. The relative gain to the public health, safety and welfare compared to the potential increase or decrease in value to the property owners; and

WHEREAS, the city council finds, after considering the above factors and the Planning Board's recommendation, particularly including the proposed findings that were accepted by the Planning Board on June 14, 2017, that the proposal should be approved; NOW, THEREFORE,

THE CITY COUNCIL OF THE CITY OF EDMONDS, WASHINGTON, DO
ORDAIN AS FOLLOWS:

Section 1. Chapter 16.60 of the Edmonds Community Development Code, entitled “CG – GENERAL COMMERCIAL ZONE,” is hereby amended to read as set forth in **Attachment 1**, which is attached hereto and incorporated herein by this reference as if set forth in full.

Section 2. Section 20.60.045 of the Edmonds Community Development Code, entitled “Freestanding signs -- Regulations,” is hereby amended to read as set forth in **Exhibit 2**, which is attached hereto and incorporated herein by this reference as if set forth in full.

Section 3. That certain real property depicted on **Exhibit 3**, which is attached hereto and incorporated herein by this reference as if set forth in full, is hereby rezoned to CG – GENERAL COMMERCIAL ZONE.

Section 4. The Development Services Director or her designee is hereby authorized and directed to make appropriate amendments to the Edmonds Zoning Map in order to properly designate the rezoned property as “CG – GENERAL COMMERCIAL ZONE” pursuant to Section 3 of this ordinance.

Section 5. Applicability. This ordinance shall not apply to applications for building permits on property within the area shown on **Exhibit 3** where the building permit applicant obtained a design review approval for the same property between July 1, 2017 and August 15, 2017, PROVIDED THAT after September 30, 2017, this ordinance shall apply to all properties regardless of when a building permit applicant obtained design review approval.

Section 6. Effective Date. This ordinance, being an exercise of a power specifically delegated to the City legislative body, is not subject to referendum, and shall take effect

five (5) days after passage and publication of an approved summary thereof consisting of the chapter.

APPROVED:

MAYOR DAVID O. EARLING

ATTEST/AUTHENTICATED:

CITY CLERK, SCOTT PASSEY

APPROVED AS TO FORM:
OFFICE OF THE CITY ATTORNEY:

BY _____
JEFFREY B. TARADAY

FILED WITH THE CITY CLERK:
PASSED BY THE CITY COUNCIL:
PUBLISHED:
EFFECTIVE DATE:
ORDINANCE NO. _____

SUMMARY OF ORDINANCE NO. _____

of the City of Edmonds, Washington

On the ____ day of _____, 2017, the City Council of the City of Edmonds, passed Ordinance No. _____. A summary of the content of said ordinance, consisting of the title, provides as follows:

AN ORDINANCE OF THE CITY OF EDMONDS, WASHINGTON, ADOPTING AMENDMENTS TO THE EDMONDS COMMUNITY DEVELOPMENT CODE, AMENDING CHAPTER 16.60 ENTITLED "CG – GENERAL COMMERCIAL ZONE," AND ECDC 20.60.045 ENTITLED "FREESTANDING SIGNS – REGULATIONS," REZONING CERTAIN PROPERTY TO CG – GENERAL COMMERCIAL ZONE, AND FIXING A TIME WHEN THE SAME SHALL BECOME EFFECTIVE.

The full text of this Ordinance will be mailed upon request.

DATED this ____ day of _____, 2017.

CITY CLERK, SCOTT PASSEY

Chapter 16.60
CG – GENERAL COMMERCIAL ZONE

EXHIBIT 1

Sections:

- 16.60.000 CG zone.
- 16.60.005 Purposes.
- 16.60.010 Uses.
- 16.60.015 Location standards for sexually oriented businesses.
- 16.60.020 Site development standards – General.
- 16.60.030 Site development standards – Design.
- 16.60.040 Operating restrictions.

16.60.000 CG zone.

- A. This chapter establishes the general commercial zoning district.
- B. Definitions. For purposes of this chapter, the following definitions apply.
 - 1. Amenity space: outdoor space for uses that are considered to provide an amenity or benefit to people
 - 2. Auto sales use: facilities for the commercial sale of motor vehicles, including buildings and areas typically associated with auto sales use, such as areas for the display and storage of automobiles that are sold or serviced as part of the overall auto sales use.
 - 3. Frontage: the front part of a property or building adjacent to a street
 - 4. Primary frontage (or “primary street frontage”): the frontage for a property that is adjacent to only one street or, for a property that is adjacent to more than one street, the frontage that is adjacent to the street that is considered primary over any other streets to which the property is adjacent.
 - 5. Stepback: The upper portion of a building that is required to be set (or stepped) further back than the minimum setback otherwise required by ECDC 16.60.020.A.
- C. Where this chapter conflicts with any other, this chapter shall prevail for the general commercial district.

16.60.005 Purposes.

The CG zone has the following specific purposes in addition to the general purposes for business and commercial zones listed in Chapter 16.40 ECDC:

- A. Encourage economic vitality through businesses, investment, redevelopment, and efficient use of land;
- B. Encourage safe and comfortable access for pedestrians, transit, and motorists;

- C. Encourage attractive mixed use development, affordable housing, and a variety of commercial uses; and
- D. Recognize the district's evolving identity and sense of place, including distinctions between different parts of the district, and be sensitive to adjacent residential zones.

16.60.010 Uses.

A. Permitted Primary Uses.

1. All permitted or conditional uses in any other zone in this title, except as specifically prohibited by subsection (C) of this section or limited by subsections (B) and (D) of this section;
2. Halfway houses;
3. Sexually oriented businesses, which shall comply with the location standards set forth in ECDC 16.60.015, the development regulations set forth in Chapter 17.50 ECDC, and the licensing regulations set forth in Chapter 4.52 ECC.

B. Permitted Secondary Uses.

1. Off street parking and loading areas to serve a permitted use.
2. Indoor storage facilities that either comprise less than 40% of a permitted primary use of the building in which they are located or are in a separate accessory building or buildings comprising less than 40% of the total leasable building space used for the parcel's permitted primary use(s).
3. Outdoor storage areas that are integral to a permitted primary use, such as storage or display areas for automobile sales, building materials or building supply sales, or garden/nursery sales, provided that such outdoor uses are screened from adjacent residential zoning districts.

C. Prohibited Uses.

1. Mobile home parks.
2. Storage facilities or outdoor storage areas intended as a primary use, not secondary to a permitted use. Automobile wrecking yards, junk yards, or businesses primarily devoted to storage or mini storage are examples of this type of prohibited use.

D. Uses Requiring a Conditional Use Permit.

1. Aircraft landings as regulated by Chapter 4.80 ECC. [Ord. 3981 § 1 (Att. A), 2014; Ord. 3635 § 1, 2007].

16.60.015 Location standards for sexually oriented businesses: All sexually oriented businesses shall comply with the requirements of this section, the development regulations set forth in Chapter 17.50 ECDC, and Chapter 4.52 ECC. The standards established in this section shall not be construed to restrict or prohibit the following activities or products: (1) expressive dance; (2) plays, operas, musicals, or other dramatic works; (3) classes, seminars, or lectures conducted for a scientific or educational purpose; (4) printed materials or visual representations intended for educational or scientific purposes; (5) nudity within a locker room or other similar facility used for changing clothing in connection with athletic or exercise activities; (6) nudity within a hospital, clinic, or other similar medical facility for health-related

purposes; and (7) all movies and videos that are rated G, PG, PG13, R, and NC17 by the Motion Picture Association of America.

A. Separation Requirements. A sexually oriented business shall only be allowed to locate where specifically permitted and only if the following separation requirements are met:

1. No sexually oriented business shall be located closer than 300 feet to any of the following protected zones, whether such protected zone is located within or outside the city limits:

- a. A residential zone as defined in Chapter 16.10 ECDC;
- b. A public use zone as defined in Chapter 16.80 ECDC.

2. No sexually oriented business shall be located closer than 300 feet to any of the following protected uses, whether such protected use is located within or outside the city limits:

- a. A public park;
- b. A public library;
- c. A nursery school or preschool;
- d. A public or private primary or secondary school;
- e. A church, temple, mosque, synagogue, or other similar facility used primarily for religious worship;
- f. A community center such as an amusement park, public swimming pool, public playground, or other facility of similar size and scope used primarily by children and families for recreational or entertainment purposes;
- g. A permitted residential use located in a commercial zone;
- h. A museum; and
- i. A public hospital or hospital district.

3. No sexually oriented business shall be located closer than 500 feet to any bar or tavern within or outside the city limits.

B. Measurement. The separation requirements shall be measured by following a straight line from the nearest boundary line of a protected zone specified in subsection (A) of this section or nearest physical point of the structure housing a protected use specified in subsection (A) of this section to the nearest physical point of the tenant space occupied by a sexually oriented business.

C. Variance from Separation Requirements. Variances may be granted from the separation requirements in subsection (A) of this section if the applicant demonstrates that the following criteria are met:

- 1. The natural physical features of the land would result in an effective separation between the proposed sexually oriented business and the protected zone or use in terms of visibility and access;
- 2. The proposed sexually oriented business complies with the goals and policies of the community development code;

3. The proposed sexually oriented business is otherwise compatible with adjacent and surrounding land uses;
4. There is a lack of alternative locations for the proposed sexually oriented business; and
5. The applicant has proposed conditions which would minimize the adverse secondary effects of the proposed sexually oriented business.

D. Application of Separation Requirements to Existing Sexually Oriented Businesses. The separation requirements of this section shall not apply to a sexually oriented business once it has located within the city in accordance with the requirements of this section. [Ord. 3981 § 1 (Att. A), 2014; Ord. 3635 § 1, 2007].

16.60.020 Site development standards – General.

A. Table. Except as hereinafter provided, development requirements shall be as follows:

Dimensional Requirements Table						
	Minimum Lot Area	Minimum Lot Width	Minimum Street Setback	Minimum Side/Rear Setback	Maximum Height	Maximum Floor Area
CG	None	None	5'/10' ²	0/15 ¹	75' ³	None

¹ Fifteen feet from all lot lines adjacent to RM or RS zoned property; otherwise no setback is required by this subsection..

² The 5' minimum width applies only to permitted outdoor auto sales use; otherwise the minimum is 10'.

³ None for structures located within an area designated as a high-rise node on the comprehensive plan map.

B. Maximum height for purposes of this chapter need not include railings, chimneys, mechanical equipment or other exterior building appurtenances that do not provide interior livable space. In no case shall building appurtenances together comprise more than 20 percent of the building surface area above the maximum height.

C. Pedestrian area.

1. For purposes of this chapter, the pedestrian area described herein is the area adjacent to the street that encompasses the public right of way from the edge of the curb (or, if no curb, from the edge of pavement) and the street setback area, as identified in Table A of this section.

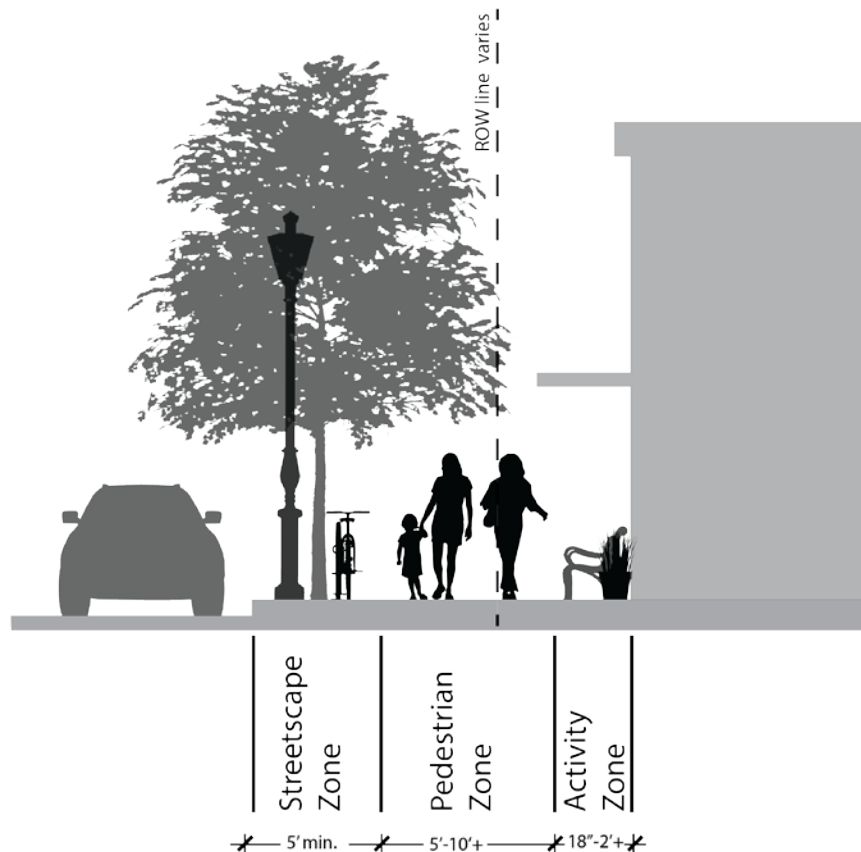
2. The pedestrian area is composed of three zones: the activity zone, the pedestrian zone, and the streetscape zone. Providing improvements to the pedestrian area, as needed to be consistent with this subsection on at least the primary street, is required as part of development projects, excluding development that would not add a new building or that consists of building improvements that do not add floor area equaling more than 10% of the building's existing floor area or that consists of additional parking stalls that comprise less than 10% of the existing parking stalls or that consists of development otherwise exempted under this chapter.

- a. Activity Zone. The activity zone shall be the open-air pedestrian area from the building front to the edge of the pedestrian zone. The activity zone is the section of the pedestrian area that is reserved for activities that commonly occur immediately adjacent to the building facade. Typical amenities or activities included in the activity

zone include, but are not limited to, sidewalks, benches, potted plants, outdoor dining and shopping. The area shall be paved to connect with the pedestrian zone in an ADA-accessible manner. Stairs, stoops and raised decks or porches may be constructed in a portion of the activity zone.

b. Pedestrian Zone. The pedestrian zone is located between the activity zone and the streetscape zone. The pedestrian zone consists of a minimum 5-foot clear and unobstructed path for safe and efficient through-traffic for pedestrians. Architectural projections and outdoor dining may be permitted to encroach into the pedestrian zone only where a minimum 5-foot clear path and 7-foot vertical clearance is maintained within the pedestrian zone.

c. Streetscape Zone. The streetscape zone is located between the curb or pavement edge to the edge of the pedestrian zone and shall be a minimum of 5 feet wide. The streetscape zone is the section that is reserved for pedestrian use and for amenities and facilities that commonly occur between the adjacent curb or pavement edge and pedestrian through-traffic. Typical amenities and facilities in the streetscape zone include, but are not limited to, street trees, street lights, benches, bus stops, and bike racks. Street trees shall be required in conformance with the Edmonds Street Tree Plan.

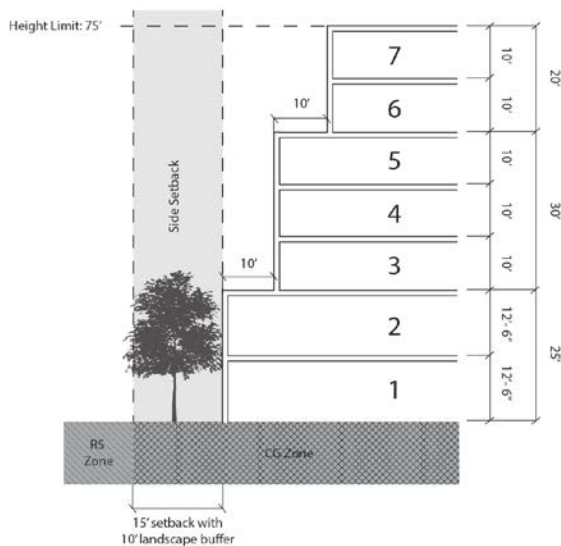


Note: Numerical Ranges for the Pedestrian Zone and the Activity Zone are typical but do not control over other requirements of this chapter.

(Illustration: Pedestrian Area)

B. Building setback when adjacent to RS Zones

1. The portion of the buildings above 25 feet in height shall step back no less than 10 feet from the required setback to an adjacent RS zone. That portion of the building over 55 feet in height shall be step back no less than 20 feet from the required setback to an adjacent RS Zone.
2. Balconies, railings, parapets and similar features that do not enclose an interior space may extend into the setback area in order to encourage more human activity and architectural features.



(Illustration: Setback and "Stepback" of building adjacent to RS Zones)

16.60.030 Site development standards – Design.

Design review by the architectural design board is required for any project that includes buildings exceeding 75 feet in height as identified in ECDC 16.70.020. Projects not exceeding this height may be reviewed by staff as a Type I decision. Regardless of what review process is required, all projects proposed in the CG zone must meet the design standards contained in this section.

A. Screening and Buffering.

1. General.

- a. Retaining walls facing adjacent property or public rights of way shall not exceed 7 feet in height. A minimum of 4 feet of planted terrace is required between stepped wall segments.
- b. Tree landscaping may be clustered to soften the view of a building or parking lot, yet allow visibility to signage and building entry.

- c. Stormwater facilities shall be designed to minimize visual impacts and integrate landscaping into the design.
- d. All parking lots are required to provide Type V interior landscaping, consistent with Chapter 20.13 ECDC
- e. Type I landscaping is required for commercial, institutional and medical uses adjacent to single family or multifamily zones. The buffer shall be a minimum of 10 feet in width and continuous in length.
- f. Type I landscaping is required for residential parking areas adjacent to single family zones. The buffer shall be a minimum of 4 feet in width and continuous in length.
- g. Type I landscaping is required for commercial and multifamily uses adjacent to single family zones. The buffer shall be a minimum of four feet in width and 10 feet in height and continuous in length.
- h. If there is a loading zone and/or trash compactor area next to a single family or multifamily zone, there shall be a minimum of a six-foot-high masonry wall plus a minimum width of 5 feet of Type I landscaping. Trash and utility storage elements shall not be permitted to encroach within street setbacks or within setbacks adjacent to single family zones. Mechanical equipment, including heat pumps and other mechanical elements, shall not be placed in the setbacks.
- i. Landscape buffers, Type I, shall be used along the edge of parking areas adjacent to single family zones.
- j. Outdoor storage areas for commercial uses must be screened from adjacent RS zones.

2. Parking Lots Abutting Streets.

- a. Type IV landscaping, minimum 5 feet wide, is required along all street frontages where parking lots, excluding for auto sales use, abut the street right-of-way.
- b. For parking lots where auto sales uses are located, the minimum setback area must be landscaped to include a combination of vegetation and paved pedestrian areas.
- c. All parking located under the building shall be completely screened from the public street by one of the following methods:
 - i. Walls that have architectural treatment meeting at least three of the elements listed in ECDC 16.60.030.D.2.e;
 - ii. Type III planting and a grill that is 25 percent opaque; or
 - iii. Grill work that is at least 80 percent opaque.

B. Parking, Access, and Bicycle Storage Standards.

1. Parking requirements. Vehicle parking shall be provided as follows:

- a. Nonresidential uses, one space per 500 square feet of leasable building space; and
- b. Residential uses, an average of 0.75 space per unit that is less than 700 square feet, an average of 1.25 parking spaces per unit that is between 700 and 1,100 square feet, and otherwise 1.75 spaces per unit.
- c. In addition, guest parking for residential uses at a minimum ratio of one guest space for every twenty required parking spaces.
- d. For mixed use development, a portion of the parking spaces may be shared between residential and commercial uses provided the director finds that the proposal is supported by a parking study and/or nationally recognized parking standards and that the site plan assures access for all shared parking uses.

- e. Parking meeting the non-residential parking requirements shall be open to the public throughout business operating hours.
- 2. The first 3,000 square feet of commercial space in a mixed-use development with a shared parking plan is exempt from off street parking requirements.
- 3. The development services director may approve a different ratio for the vehicle parking required by standards of subsection B.1 of this section when an applicant submits parking data illustrating that the standards do not accurately apply to a specific development. The data submitted for an alternative parking ratio shall include, at a minimum, the size and type of the proposed development, and the anticipated peak and average parking loads of all uses. The director may approve a parking ratio that is based on the specific type of development and its primary users in relationship to:
 - a. An analysis conducted using nationally recognized standards or methodology, such as is contained in the Urban Land Institute's most recent version of the publication "Shared Parking" or the latest version of the Institute of Transportation Engineers publication "Parking Generation"; or
 - b. A site-specific parking study that includes data and analysis for one or more of the following:
 - i. 1/4 mile proximity to a bus rapid transit station and methodology that takes into account transit-oriented development;
 - ii. Use of transportation demand management policies, including but not limited to free or subsidized transit passes for residents and workers;
 - iii. On-site car-share and bike-share facilities; or
 - iv. Uses that serve patients, clients, or tenants who do not have the same vehicle parking needs as the general population.
 - v. Other methods that reduce the need for vehicle parking.
- 4. All off-street surface parking shall be located to the side or rear of the primary building, except as otherwise allowed by this chapter, and shall be screened from the sidewalk by a wall or plantings between 2 to 4 feet in height. Outdoor parking areas shall comprise 40% or less of the public street frontage area within 100 feet of the primary street for the lot or tract and, on corner lots, may not be located at the corner. The requirements of this subsection do not apply to permitted auto sales uses.
- 5. Electric vehicle charging stations: One or more electric vehicle charging stations must be provided for all new development that includes housing. Required charging stations shall be installed to serve at least 10% of the required residential parking stalls. In addition, either additional stations or planned capacity (or a combination thereof) that can double the amount of initially required stations shall be provided. For this subsection, "planned capacity" means site design and construction that includes electrical wiring connection and ventilation, compliant with the City's building codes, to support potential or actual future electric vehicle charging stations.
- 6. Bicycle storage spaces: Bicycle storage spaces for multifamily housing, excluding housing for assisted living or other specialized facilities where the development services director finds that the targeted population is not likely to use bicycles, shall be provided for residents at a ratio of 1 bicycle storage space for each residential unit under 700 square feet and 2 bicycle storage spaces for each residential unit greater than 700 square feet. Bicycle storage spaces shall consist of storage racks, lockers, or other secure space to accommodate sheltered, safe, and

convenient bicycle storage for building residents. Such space may be in a vehicle parking garage or another appropriate location but shall not be provided as open storage on a deck or balcony. Where sheltered bicycle storage is being provided within a dedicated common space of the building, the total number of required bicycle storage spaces may be reduced by up to 50% from that which is otherwise required, provided that one or more secure bicycle racks, useable by visitors, for at least four bicycles is provided within the front setback of the property.

7. Driveways accessing Highway 99: All driveway connections to Highway 99 must meet the applicable requirements of the Washington State Department of Transportation, including minimum requirements for distance between driveway access connections, which may be up to 250 feet to help promote traffic safety and minimize pedestrian-vehicle conflicts.

8. Paths within Parking Lots.

- a. Pedestrian paths in parking lots shall be delineated by separate paved routes that meet federal accessibility requirements and that use a variation in textures and/or colors and may include landscape barriers and landscape islands.
- b. Pedestrian paths shall be provided at least every 180 feet within parking lots. These shall be designed to provide access to onsite buildings as well as to pedestrian walkways that border the development.
- c. Pedestrian paths shall be a minimum of six feet in width and shall be separated from the parking area either horizontally or vertically (e.g. with curbs). Where paths cross vehicular lanes, raised traffic tables should be considered if feasible.
- d. Parking lots shall have pedestrian connections to the main sidewalk at a minimum of every 100 feet.

9. Bonus for Parking Below or Above Ground Floor.

- a. For projects where at least 50 percent of the parking is below or above the ground floor of the building, the following standards may be applied regardless of any ECDC standards that otherwise conflict:
 - i. The minimum drive aisle width may be reduced to 22 feet.
 - ii. The maximum ramp slope may be increased to 20 percent.
 - iii. A mixture of full and reduced width parking stalls may be provided without demonstrating the stalls could also be provided at full width dimensions.

10. Drive –through facilities. Drive-through facilities such as, but not limited to, banks, cleaners, fast food, drug stores, and espresso stands, shall comply with the following:

- a. Drive-through windows and stacking lanes shall not be located along the facades of the building that face a street.
- b. No more than one direct entrance or exit from the drive-through shall be allowed as a separate curb cut onto an adjoining street.

11. Pedestrian and Transit Access.

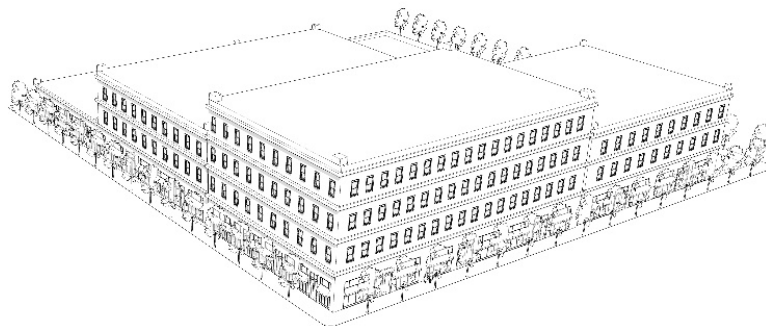
- a. Pedestrian building entries must connect directly to the public sidewalk and to

- adjacent developments if feasible.
- b. Internal pedestrian routes shall extend to the property line and connect to existing pedestrian routes where applicable. Potential future connections shall also be identified such that pedestrian access between developments can occur without walking in the parking or access areas.
- c. Where a transit station or bus stop is located in front of or adjacent to a parcel, pedestrian connections linking the station or stop directly to the development are required.
- d. Pedestrian routes shall connect buildings on the same site to each other.

C. Site Design and Layout: Overall, the design and use of each site shall be based on the building/street relationship and on the integration of pedestrian features. This will take the form of either a Pedestrian Oriented Design Area or an Alternative Walkable Design Area, as described below in subsections 1 and 2 of this section, provided that an exceptions process, pursuant to Subsection 3 below, may be allowed under the provisions of this section. Additional site design and layout standards in this section must also be met.

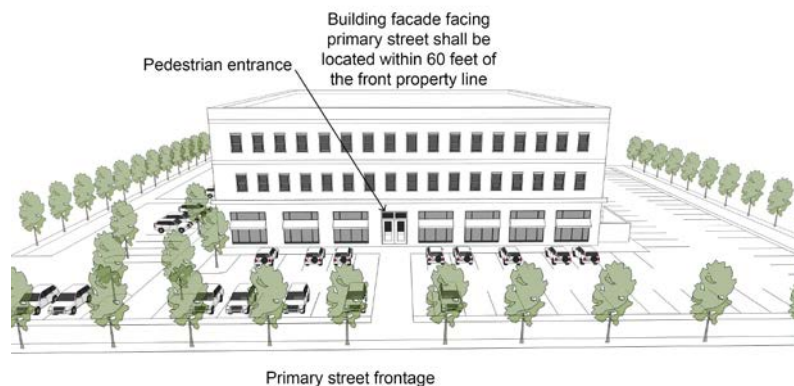
1. Pedestrian Oriented Design Area: Unless otherwise permitted under subsections 2 or 3 of this section, development must meet the requirements of this subsection for a pedestrian-oriented design area.

- a. Primary Frontage. At least 50% of a building's façade facing the primary public street shall be located within 20 feet of the property line where the primary street frontage exists. The illustration below provides an example of this concept. The requirement does not apply to buildings that are behind another building on the same lot when the other building has a footprint of at least 3000 square feet and has met the requirement. Where site constraints preclude strict compliance with the requirement, the building line shall be measured one foot behind the line created by that constraint. On a corner lot or a lot with frontages on multiple streets, the development services director shall determine the primary street frontage considering the following:
 - i. The street classification of the adjacent streets;
 - ii. The prevailing orientation of other buildings in the area;
 - iii. The length of the block face on which the building is located; or
 - iv. Unique characteristics of the lot or street.
- b. The building must include a prominent pedestrian entry on the primary frontage. Vehicle parking, other than where permitted for vehicle sales use, shall not be located within the first 20 feet of the primary street frontage. The first 20 feet of the primary street frontage may include building space, landscaping, artwork, seating areas, outdoor displays, and pedestrian and bicycle facilities.



2. Alternative Walkable Design Area Option: An alternative to the pedestrian-oriented design area requirements of subsection 1 in this section may be allowed by the development services director only for sites that the director has found to have unique and significant constraints related to pedestrian access and for which a phased design plan to increase pedestrian access and connectivity has been submitted to the development services department. While they currently may be largely auto-oriented, Walkable Design Areas have a high potential for walking, bicycling and transit service. If a development is allowed to use this standard, it shall be subject to the requirements of this subsection.

- a. Building Placement: For any new building permitted on a property after August 1, 2017, a minimum of 50% of the building's façade facing the primary street shall be located within 60 feet of the front property line or within 65 feet where a 5-foot landscaping area is provided between the parking lot and the sidewalk. When site constraints preclude strict compliance with this requirement, the building line shall be measured one foot behind the line created by that constraint.
- b. On a corner lot or a lot with frontages on multiple streets, the development services director shall determine the primary street frontage considering the following:
 - i. The street classification of the adjacent streets;
 - ii. The prevailing orientation of other buildings in the area;
 - iii. The length of the block face on which the building is located;
 - iv. The location of any alley or parking areas; or
 - v. Unique characteristics of the lot or street.
- c. No more than one double-sided row of parking spaces shall be allowed in the front of a building on its primary frontage.
- d. A pedestrian entrance must be located on the primary frontage.



- e. Required Amenity spaces, under subsection 4 below, shall be located to connect the building to the street as much as practicable, provided that amenity space may also be located between buildings where the space will be used in common.

3. Exceptions Process for Pedestrian or Walkable Design: An exception to the exact requirements of subsections 1 or 2 of this section may be allowed by the hearing examiner under a Type IIIA decision process to provide for design flexibility that still encourages pedestrian orientation and efficient land uses when the following criteria are met:

- a. The property is located within 300 feet of a highway interchange and has unique pedestrian access constraints or is primarily used for motor vehicle sales;
- b. The development provides business and pedestrian areas that are near the primary street frontage and likely to be active throughout the day and evening;
- c. The development features a prominent building entry for pedestrian use that is highly visible and connected by a well-lit walkway from the primary street frontage;
- d. At least 25% of the required amenity space shall be located to connect the building to the street in a manner that encourages pedestrian use and include seating, landscaping, and artwork
- e. Where a site has multiple buildings (excluding accessory utility buildings), 50% or more of the required amenity space shall be located between buildings to allow for shared use.
- f. No more than 50% of vehicle parking, other than that associated with a permitted vehicle sales facility use, may be located within 20 feet of the front property line.
- g. One or more buildings on the site must have at least two stories of useable space.

4. Amenity space. Amenity space is intended to provide residents, employees, and visitors with places for a variety of outdoor activities.

- a. An area equivalent to at least 5% of the building footprint shall be provided as amenity space. If a vehicle parking area is being added to the site without the concurrent development of a building of at least 2,000 square feet, amenity space must be provided to equal at least 5% of the additional parking area.
- b. The amenity space shall be outdoor space that incorporates pedestrian-oriented features, such as, but not limited to, seating, paths, gazebos, dining tables, pedestrian-scale lighting, and artwork. A minimum of 10% of the required amenity space shall be comprised of plantings, which may include tree canopy areas and other shade or screening features. Native vegetation is encouraged.
- c. The majority of the required amenity space must be provided in one or more of the following forms:
 - i. Recreation areas: an open space available for recreation. The area may be spatially defined by landscaping rather than building frontages. Its surface shall consist primarily of hardy groundcover or a material conducive to playground or recreational use. Decorative landscape features, such as flower

- beds, shall not comprise more than 15% of the total area.
- ii. Plazas: an open space available for community gathering and commercial activities. A plaza shall be spatially defined primarily by either building facades, with strong connections to interior uses, or close proximity to the public sidewalk, especially at the intersection of streets. Its surface shall be primarily hardscape, provided that trees, shade canopies, and other landscaping, as well as water features and artwork, may add visual or environmental features to the space.
- iii. Squares or courtyards: an open space available for unstructured recreation or community gathering purposes. A square is spatially defined by building facades with strong connections to interior uses. Its surface shall be primarily hardscape, supplemented by trees and other landscaping. Water features and artwork are optional.
- iv. Exception: A community garden may comprise a portion of any amenity space, provided that it:
 - 1) is located more than 20 feet from a primary street frontage;
 - 2) is dedicated to ongoing use by residents of the site, including for growing edible produce; and
 - 3) includes facilities for watering the garden and storing garden supplies.

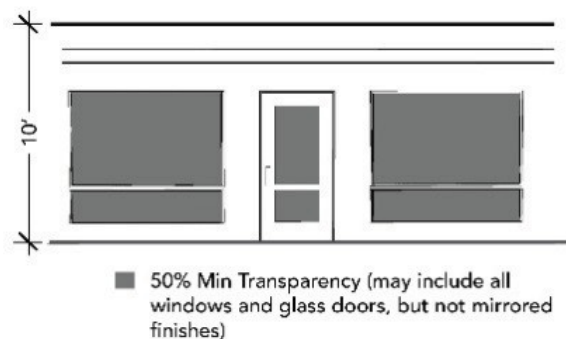
5. Lighting: All lighting shall be shielded and directed downward and away from adjacent parcels. This may be achieved through lower poles at the property lines and/or full “cut off” fixtures.
 - a. Parking lots shall have lighting poles that are a maximum of 25 feet in height. Pedestrian paths or walkways and outdoor steps shall have pedestrian-scaled lighting focused on the travel path. Pole height shall be a maximum of 14 feet, although lighting bollards are preferred.
 - b. For pedestrian paths and walkways on internal portions of the site, solar-powered lighting may be sufficient.
 - c. Entries shall have lighting for safety and visibility integrated with the building/canopy.

D. Building Design Standards

1. General. To provide variety and interest in appearance, the following design elements should be considered, and a project shall demonstrate how at least four of the elements will be used to vary the design of the site:
 - a. Building massing and unit layout,
 - b. Placement of structures and setbacks,
 - c. Location of pedestrian and vehicular facilities,
 - d. Composition and character of open space, plant materials and street trees,
 - e. Variety in architectural elements, façade articulation, and/or building materials,
 - f. Roof variation in slope, height and/or materials.

2. Building Design and Massing.

- a. Buildings shall convey a visually distinct “base” and “top, which may be achieved through differences in massing elements and/or architectural details.
- b. The bulk and scale of buildings of over 3,000 square feet in footprint shall be mitigated through the use of massing and design elements such as façade articulation and modulation, setbacks, step-backs, distinctive roof lines or forms, and other design details.
- c. Primary Frontage: On the primary frontage, to provide visual connection between activities inside and outside the building, 50% of the building façade between 2 and 10 feet in height, as measured from the adjacent sidewalk, shall be comprised of



windows or doors that are transparent, the bottom of which may not be more than 4 feet above the adjacent sidewalk. A departure from this standard may be approved when the façade will not be visible from the public street due to the placement of other buildings on the site, provided that the requirements of subsection “e” in this section shall apply.

- i. On the primary frontage, no vehicle parking shall be located within the first 20 feet of the first level of a building facing the street except where such parking is underground.
- d. All Other Building Frontages: All street-facing facades within 30 feet of a public street, other than for the primary frontage or those facing an alley or the last block of a dead-end street, shall comply with the standard below.
 - i. Thirty percent (30%) of the building façade between 2 and 10 feet in height shall be made of windows or doors that are transparent, the bottom of which may not be more than four feet above the adjacent sidewalk. Windows shall not be mirrored or have glass tinted darker than 40% in order to meet this requirement.
- e. Wall treatment: Building facades not subject to all requirements of ECDC 16.60.030.D.2.c or ECDC 16.60.030.D.2.d are intended to not display blank, unattractive walls to the public or to other building tenants. To accomplish this, walls greater than 30 feet in length shall have architectural treatment that incorporates at least four of the following elements into the design of the facade:
 - i. Masonry (except for flat concrete block).
 - ii. Concrete or masonry plinth at the base of the wall.

- iii. Belt courses of a different texture and color.
- iv. Projecting cornice.
- v. Projecting metal or wood canopy.
- vi. Decorative tilework.
- vii. Trellis containing planting.
- viii. Medallions.
- ix. Artwork or wall graphics.
- x. Vertical differentiation.
- xi. Decorative lighting fixtures.
- xii. Glazing
- xiii. An architectural element not listed above that is approved by the director to meet the intent of this subsection.

16.60.040 Operating restrictions.

A. Enclosed Building. All uses shall be carried on entirely within a completely enclosed building, except the following:

- 1. Public utilities;
- 2. Off street parking and loading areas;
- 3. Drive-in business;
- 4. Secondary uses permitted under ECDC 16.60.010(B);
- 5. Limited outdoor display of merchandise meeting the criteria of Chapter 17.65 ECDC;
- 6. Public markets; provided, that when located next to a single family residential zone, the market shall be entirely within a completely enclosed building;
- 7. Outdoor dining meeting the criteria of Chapter 17.75 ECDC;
- 8. Motorized and nonmotorized mobile vending units meeting the criteria of Chapter 4.12 ECC.

B. Interim Use Status – Public Markets.

- 1. Unless a public market is identified on a business license as a year-round market within the city of Edmonds, a premise licensed as a public market shall be considered a temporary use. As a temporary activity, any signs or structures used in accordance with the market do not require design review. When a location is utilized for a business use in addition to a public market, the public market use shall not decrease the required available parking for the other business use below the standards established in this chapter.

C. Ongoing Uses.

- 1. Audio equipment at drive-through facilities shall not be audible off site.
- 2. Development subject to the standards of this chapter shall continue to meet the standards of this chapter except as specifically permitted otherwise.

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20.60.045 Freestanding signs – Regulations.

A. Regulation. Permanent freestanding signs are discouraged. Freestanding signs shall be approved only where the applicant demonstrates by substantial evidence that there are no reasonable and feasible alternative signage methods to provide for adequate identification and/or advertisement.

B. Maximum Area. The maximum area of a freestanding sign shall be as follows:

<u>Zone</u>	<u>Maximum Area of Sign</u>
<u>RS, RM</u>	<u>10 square feet (subdivision, PRD, multifamily)</u> <u>4 square feet (individual residence sign)</u>
<u>BN, BP</u>	<u>24 square feet (single)</u> <u>48 square feet (group)</u>
<u>BC, BD,</u> <u>WMU, FVMU</u>	<u>32 square feet (single)</u> <u>48 square feet (group)</u>
<u>CW</u>	<u>32 square feet (single)</u> <u>48 square feet (group)</u>
<u>CG</u>	<u>Sign area shall be governed by subsection (C) of this section</u>

C. Allowable Sign Area for Freestanding Signs – CG Zone. The total allowable sign area for freestanding signs on general commercial sites shall be 56 square feet or one-half square foot of sign area for each lineal foot of street frontage, whichever is greater, up to a maximum of 160 square feet of freestanding sign area. Freestanding signs count against the overall allowable permanent sign area. Multiple business or tenant sites shall further be allowed an additional 24 square feet of freestanding sign area for each commercial tenant or

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occupant in excess of one up to a maximum sign area of 160 square feet. Corner lots choosing to accumulate sign area under the provisions of subsection (E) of this section shall be limited to 160 square feet.

D. Maximum Height. The maximum sign height of freestanding signs shall be as follows:

<u>Zone</u>	<u>Maximum Height of Sign</u>
<u>RS, RM</u>	<u>6 feet</u>
<u>BN, BP, BC, BD, CG, CW, WMU, FVMU</u>	<u>14 feet</u>

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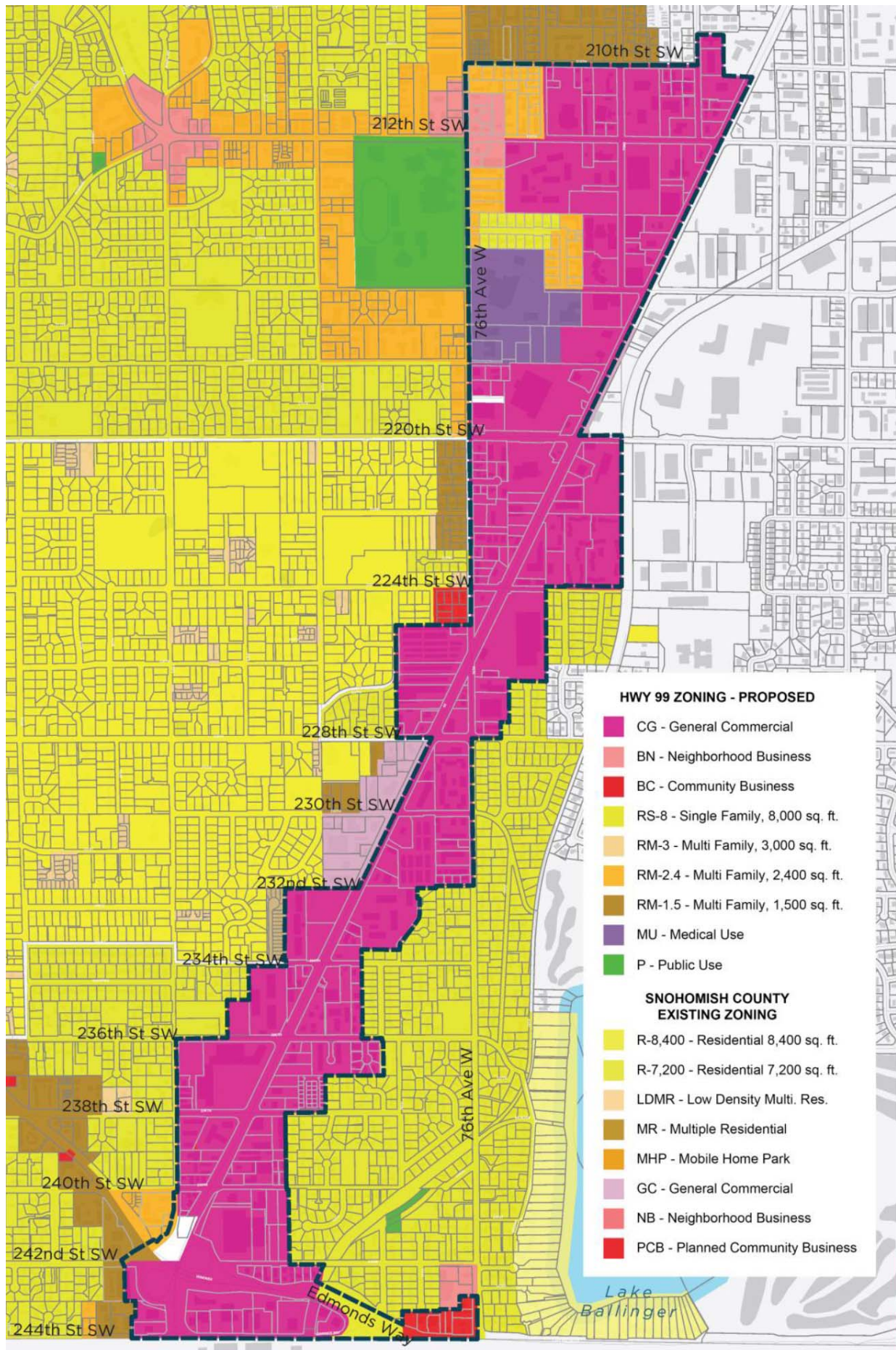
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Exhibit 3



ORDINANCE NO. _____

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF EDMONDS,
WASHINGTON, ESTABLISHING A PLANNED ACTION FOR THE HIGHWAY
99 SUBAREA, PURSUANT TO THE STATE ENVIRONMENTAL POLICY ACT

WHEREAS, the State Environmental Policy Act (SEPA) and implementing rules provide for the integration of environmental review with land use planning and project review through designation of Planned Actions by jurisdictions planning under the Growth Management Act "GMA"; and

WHEREAS, the City has adopted a Comprehensive Plan complying with the GMA; and

WHEREAS, the City has adopted development regulations providing for planned actions;

WHEREAS, the City has prepared a subarea plan and development regulations for the Highway 99 Subarea; and

WHEREAS, designation of a Planned Action expedites the permitting process for subsequent, implementing projects whose impacts have been previously addressed in a Planned Action environmental impact statement (EIS), and thereby encourages desired growth and economic development; and

WHEREAS, the Highway 99 Planned Action EIS identifies impacts and mitigation measures associated with planned development in the subarea; and

WHEREAS, the City has adopted development regulations which will help protect the environment, and is adopting zoning regulations specific to the sub-area which will guide the amount, location, form, and quality of desired development;

WHEREAS, the Highway 99 Subarea is deemed to be appropriate for designation of a Planned Action;

WHEREAS, the Edmonds Planning Board held an open record public hearing on May 10, 2017 to consider Highway 99 Subarea development regulations and on July 26, 2017 to consider the proposed planned action ordinance;

WHEREAS, the Edmonds City Council held an open record public hearing on June 20, 2017 to consider Highway 99 Subarea development regulations and on July 31, 2017, to consider the proposed planned action ordinance;

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF EDMONDS, WASHINGTON, DOES HEREBY ORDAIN AS FOLLOWS:

SECTION 1. Recitals. The Recitals set forth above are hereby adopted and incorporated by reference. The City Council bases its findings and conclusions on the entire record of testimony and exhibits, including all written and oral testimony before the Planning Board and City Council.

SECTION 2. Purpose. The City Council declares that the purpose of this ordinance is to:

- A. Combine analysis of environmental impacts with the City's development of plans and regulations;
- B. Designate the Highway 99 Subarea as a Planned Action for purposes of environmental review and permitting of subsequent, implementing projects pursuant to the State Environmental Policy Act (SEPA), RCW 43.21C.031;
- C. Determine that the EIS prepared for the sub-area plan meets the requirements of a Planned Action EIS pursuant to SEPA;
- D. Establish criteria and procedures, consistent with state law, that will determine whether subsequent, implementing projects qualify as Planned Actions;
- E. Provide the public with information about planned actions and how the City will process applications for implementing projects;
- F. Streamline and expedite the land use review and approval process for qualifying projects by relying on the EIS completed for the Planned Action; and
- G. Apply the City's development regulations together with the mitigation measures described in the Planned Action EIS and this Ordinance to address the impacts of future development contemplated by the Planned Action.

SECTION 3. Findings. The City Council finds as follows:

- A. The City is subject to the requirements of the Growth Management Act, RCW 36.70A, and is located within an Urban Growth Area;
- B. The City has adopted a Comprehensive Plan complying with the GMA, and is amending the Comprehensive Plan by adopting a subarea element specific to the Highway 99 Subarea;
- C. The City is adopting development regulations to implement the Highway 99 Subarea Plan to implement said Plan;
- D. The City has prepared an EIS for the Highway 99 subarea (Planned Action EIS) and finds that this EIS adequately addresses the probable significant environmental impacts associated with the type and amount of development planned to occur in the designated Planned Action area;
- E. The mitigation measures identified in the Planned Action EIS and attached to this ordinance as Exhibit B, together with adopted City development regulations, will adequately mitigate significant impacts from development within the Planned Action area;
- F. The Subarea Plan and Planned Action EIS identify the location, type and amount of development that is contemplated by the Planned Action;

G. Future projects that are implemented consistent with the Planned Action will protect the environment, benefit the public and enhance economic development;

H. The City has provided numerous opportunities for meaningful public involvement in the proposed Planned Action; has considered all comments received; and, as appropriate, has modified the proposal or mitigation measures in response to comments;

I. The Highway 99 Subarea Plan is not an essential public facility as defined by RCW 36.70A.200(1), and any future projects which meet the definition of an essential public facility will not qualify as Planned Actions;

J. The Planned Action applies to a defined area that is smaller than the overall City boundaries; and

K. Public services and facilities are adequate to serve the proposed Planned Action, with implementation of mitigation measures identified in the EIS.

SECTION 4. Procedures and Criteria for Evaluating and Determining Projects as Planned Actions.

A. *Planned Action Area.* The Planned Action designation shall apply to the area shown in Exhibit A.

B. *Environmental Document.* A Planned Action determination for a site-specific implementing project application shall be based on the environmental analysis contained in the Draft EIS issued by the City on June 2, 2017, and the Final EIS published on August 4, 2017. The Draft and Final EISs shall comprise the Planned Action EIS. The mitigation measures contained in Exhibit B are based upon the findings of the Planned Action EIS and shall, along with adopted City regulations, provide the framework that the City will use to impose appropriate conditions on qualifying Planned Action projects.

C. *Planned Action Designated.* Land uses and activities described in the Planned Action EIS, subject to the thresholds described in subsection 3.D, below, and the mitigation measures contained in Exhibit B, are designated Planned Actions or Planned Action Projects pursuant to RCW 43.21C.031. A development application for a site-specific Planned Action project located within the Highway 99 Subarea Planned Action Area shall be designated as a Planned Action if it meets the criteria set forth in subsection 3.D of this ordinance and applicable laws, codes, development regulations and standards of the City.

D. *Planned Action Qualifications.* The following thresholds shall be used to determine if a site-specific development proposed within the Highway 99 Planned Action Area is contemplated by the Planned Action and has had its environmental impacts evaluated in the Planned Action EIS:

(1) Land Use. The following general categories/types of land uses, which are permitted or conditionally permitted in zoning districts applicable to the Highway 99 Planned Action Area, and subject to any limitations in size contained in the applicable zoning districts, are considered Planned Actions: Anticipated land uses are further identified below:

(a) Multiple dwellings;

- (b) Office uses, including but not limited to medical office;
- (c) Retail and service uses;
- (d) Medical and health care uses;
- (e) Mixed use development;
- (e) Utilities and capital facilities.

To be considered Planned Actions proposed projects shall only include those uses specifically listed in development regulations applicable to the zoning classifications applied to properties within the Planned Action Area.

(2) Development Thresholds.

- a) The following amount of various new development are contemplated by the Planned Action:

Land Use	Development Amount
Non-residential uses, including office, retail, service and medical/health care uses	1,634,685 square feet of building area
Residential	3,325 dwelling units

(b) If future development proposals in the Highway 99 Planned Action Area would contribute enough new square footage and/or dwelling units to the square footage and/or dwelling units generated from earlier planned action projects to cause either of the above thresholds for cumulative development to be exceeded, that development proposal and all subsequent proposed projects will require additional SEPA review, pursuant to WAC 197-11-172. Furthermore, if proposed development would alter the assumptions and analysis in the Planned Action EIS, further environmental review may be required. Shifting the development amount between categories of uses may be permitted so long as the total build-out does not exceed the aggregate amount of development and trip generation reviewed in the EIS, and so long as the impacts of that development have been identified in the Planned Action EIS and are mitigated consistent with Exhibit B.

(c). Building Heights. Building heights shall be as established in the applicable zoning classification and as evaluated in the Planned Action EIS.

(3) Transportation.

(a) *Trip Ranges & Thresholds.* The Planned Action EIS analyzed and identified mitigation for 2,755 new pm peak hour trips to be generated from cumulative development occurring in the Planned

Action area. If a proposed project would contribute enough new pm peak hour trips to the trips generated from earlier planned action projects to cause this threshold for cumulative development to be exceeded, that proposed projects and all subsequent proposed projects will require additional SEPA review.

(b) *Concurrency*. The determination of transportation impacts shall be based on the City's concurrency management program and the level of service standards in the Comprehensive Plan.

(c) *Traffic Impact Mitigation*. All planned action projects shall pay, as a condition of approval, their proportionate share of local street improvements according to the schedule in Edmonds City Code 3.36.125. Impact fees will be determined according to the methodology contained in Chapter 3.36.

(d) *Director Discretion*. The Development Services Director, in consultation with the City Engineer, shall have discretion to determine incremental and total trip generation, consistent with the Institute of Traffic Engineers (ITE) Trip Generation Manual (latest edition) or an alternative manual accepted by the Director at his or her sole discretion, for each project permit application proposed under this Planned Action.

(4) Elements of the Environment and Degree of Impacts. A proposed project that would result in a significant change in the type or degree of impacts to any of the elements of the environment analyzed in the Planned Action EIS, would not qualify as a Planned Action.

(5) Changed Conditions. Should environmental conditions change significantly from those analyzed in the Planned Action EIS, the City's SEPA Responsible Official may determine that the Planned Action designation is no longer applicable until supplemental environmental review is conducted.

E. Planned Action Review Criteria.

(1) The City's SEPA Responsible Official may designate as planned actions, pursuant to RCW 43.21C.030, applications that meet all of the following conditions:

(a) the proposal is located within the Planned Action area identified in Exhibit A of this ordinance;

(b) the proposed uses and activities are consistent with those described in the Planned Action EIS and Section 4.D of this ordinance;

(c) the proposal does not cause the Planned Action thresholds and other criteria of Section 4.D of this ordinance to be exceeded;

(d) the proposal is consistent with the City of Edmonds Comprehensive Plan and the Highway 99 Subarea Plan;

(e) the proposal's significant adverse environmental impacts have been identified in the Planned Action EIS;

(f) the proposal's significant impacts have been mitigated by application of the measures identified in Exhibit B, and other applicable city regulations, together with any modifications or variances or special permits that may be required;

(g) the proposal complies with all applicable local, state and/or federal laws and regulations, and the Responsible Official determines that these constitute adequate mitigation; and

(h) the proposal is not an essential public facility as defined by RCW 36.70A.200(1).

(2) The City shall base its decision on review of a SEPA checklist, or an alternative form approved by the Department of Ecology, and review of the application and supporting documentation.

(3) A proposal that meets the criteria of this section shall be considered to qualify and be designated as a Planned Action, consistent with the requirements of RCW 43.21C.030, WAC 197-11-164 et seq, and this ordinance.

F. Effect of Planned Action

(1) Designation as a Planned Action project means that a qualifying proposal has been reviewed in accordance with this ordinance and found to be consistent with its development parameters and thresholds, and with the environmental analysis contained in the Planned Action EIS.

(2) Upon determination by the City's SEPA Responsible Official that the proposal meets the criteria of Section 4.D and qualifies as a Planned Action, the proposal shall not require a SEPA threshold determination, preparation of an EIS, or be subject to further review pursuant to SEPA.

G. Planned Action Permit Process. Applications for Planned Actions shall be reviewed pursuant to the following process.

(1) Development applications shall meet all applicable requirements of the Edmonds City Code and Community Development Code. Applications for Planned Actions shall be made on forms provided by the City and shall include a SEPA checklist, or an approved Planned Action checklist.

(2) The City's Director of Development Services or designee shall determine whether the application is complete as provided in Edmonds City Code 20.02.003.

(3) If the application is for a project within the Planned Action Area defined in Exhibit A, the application will be reviewed to determine if it is consistent with the criteria of this ordinance and thereby qualifies as a Planned Action project. The SEPA Responsible Official shall notify the applicant of his/her decision. If the project is determined to qualify as a Planned Action, it shall proceed in accordance with the applicable permit review procedures specified in Edmonds City Code Chapter 20.02, except that no SEPA threshold determination, EIS or additional SEPA review shall be required. The decision of the SEPA Responsible Official regarding qualification as a Planned Action shall be final.

(4) Public notice and review for projects that qualify as Planned Actions shall be tied to the underlying permit and shall follow the procedures set forth in Edmonds City Code Chapter 20.03. If notice is otherwise required for the underlying permit, the notice shall state that the project has qualified as a Planned Action. If notice is not otherwise required for the underlying permit, no special notice is required by this ordinance.

(5) Development Agreement. To provide additional certainty about applicable requirements, the City or an applicant may request consideration and execution of a development agreement, as allowed

in Ch. 20.08 ECDC of Edmond's city code, for a Planned Action project. The development agreement may address review procedures applicable to a Planned Action project, permitted uses, mitigation measures, payment of impact fees or provision of improvements through other methods, design standards, phasing, vesting of development rights, or any other topic that may properly be considered in a development agreement consistent with RCW 36.70B.170 et seq.

(6) If a project is determined to not qualify as a Planned Action, the SEPA Responsible Official shall so notify the applicant and prescribe a SEPA review procedure consistent with the City's SEPA regulations and the requirements of state law. The notice shall describe the elements of the application that result in failure to qualify as a Planned Action.

(7) Projects that fail to qualify as Planned Actions may incorporate or otherwise use relevant elements of the Planned Action EIS, as well as other relevant SEPA documents, to meet their SEPA requirements. The SEPA Responsible Official may limit the scope of SEPA review for the non-qualifying project to those issues and environmental impacts not previously addressed in the Planned Action EIS.

SECTION 5. Monitoring and Review.

A. The City shall monitor the progress of development in the designated Planned Action Sub-area to ensure that it is consistent with the assumptions of this ordinance and the Planned Action EIS regarding the type and amount of development and associated impacts, and with the mitigation measures and improvements planned for the Highway 99 Planned Action Area.

B. This Planned Action Ordinance shall be reviewed no later than five years from its effective date by the SEPA Responsible Official to determine the continuing relevance of its assumptions and findings with respect to environmental conditions in the Planned Action area, the impacts of development, and required mitigation measures. Based upon this review, the City may propose amendments to this ordinance or may supplement or revise the Planned Action EIS.

SECTION 6. Conflict. In the event of a conflict between this Ordinance or any mitigation measure imposed thereto, and any ordinance or regulation of the City, the provisions of this ordinance shall control EXCEPT that the provision of any International Code shall supersede.

SECTION 7. Severability. Should any section, subsection, paragraph, sentence, clause or phrase of this Ordinance or its application be declared to be unconstitutional or invalid by a court of competent jurisdiction, such decision shall not affect the constitutionality or validity of the remaining portions of this ordinance or its application to any other person or situation.

SECTION 8. Effective Date. This ordinance, being an exercise of a power specifically delegated to the City legislative body, is not subject to referendum, and shall take effect five (5) days after its passage, approval and publication as provided by law.

APPROVED:

MAYOR DAVID O. EARLING

ATTEST/AUTHENTICATED:

CITY CLERK, SCOTT PASSEY

APPROVED AS TO FORM:
OFFICE OF THE CITY ATTORNEY:

BY _____
JEFFREY B. TARADAY

FILED WITH THE CITY CLERK:
PASSED BY THE CITY COUNCIL:
PUBLISHED:
EFFECTIVE DATE:
ORDINANCE NO. _____

SUMMARY OF ORDINANCE NO. _____

of the City of Edmonds, Washington

On the ____ day of _____, 2017, the City Council of the City of Edmonds, passed Ordinance No. _____. A summary of the content of said ordinance, consisting of the title, provides as follows:

AN ORDINANCE of the City Council of the City of Edmonds, Washington, establishing a Planned Action for the Highway 99 Subarea, pursuant to the State Environmental Policy Act

The full text of this Ordinance will be mailed upon request.

DATED this ____ day of _____, 2017.

CITY CLERK, SCOTT PASSEY

EXHIBIT A
PLANNED ACTION AREA

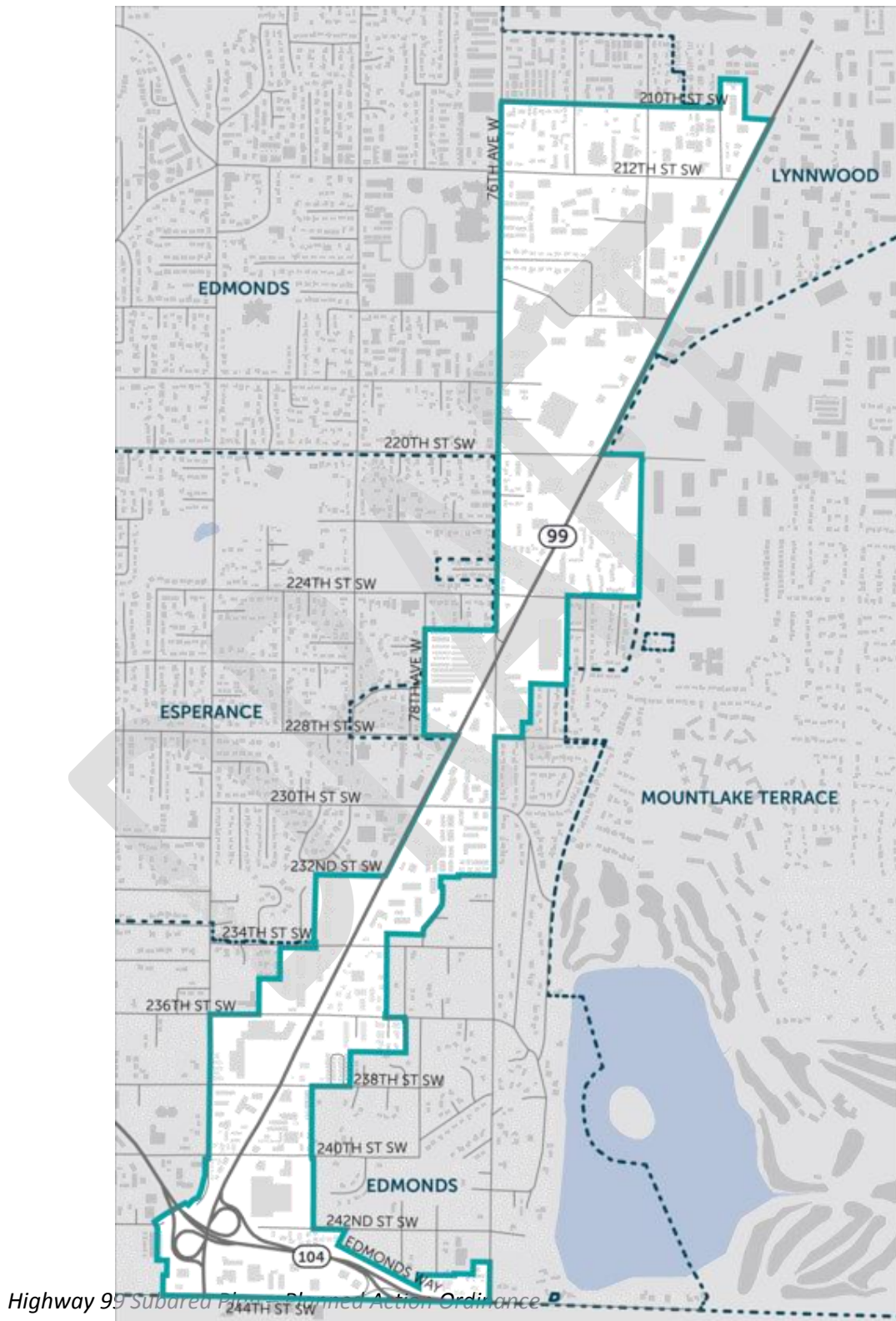


EXHIBIT B
Highway 99 Subarea Plan
PLANNED ACTION EIS MITIGATION DOCUMENT

The State Environmental Policy Act (SEPA) requires environmental review for project and non-project proposals that may have adverse impacts on the environment.

In order to meet SEPA requirements, the City of Edmonds issued the *Draft Highway 99 Planned Action Environmental Impact Statement* (EIS) on June 2, 2017 and the *Final Highway 99 Planned Action EIS* on August 4, 2017. The Draft and Final EIS are referenced collectively herein as the “EIS.” The EIS has identified probably significant impacts that would occur with future development in the Planned Action area, together with a number of potential measures to mitigate those significant impacts.

The purpose of this Mitigation Document is to establish specific mitigation measure for qualified planned action development proposals, based on significant impacts identified in the EIS. The mitigation measures would apply to future development proposals that are consistent with the planned action development envelope reviewed in the EIS and that are located within the Planned Action area (see Exhibit A).

USE OF TERMS

Brief definitions of terms used in this Mitigation Document are provided below.

SEPA Terms

The discussion of mitigation measures may refer to the word’s action, planned action or proposal and for reference, these terms are defined below:

- “Action” means projects or programs financed, licensed, regulated, conducted or approved by an agency. “Project actions” involve decisions on a specific project such as a construction or management activity for a defined geographic area. “Non-project” actions involve decisions about policies, plans or programs (WAC 197-11-704)
- “Planned Action” refers to types of project actions that are designated by ordinance for a specific geographic area and addressed in an EIS in conjunction with a comprehensive plan or subarea plan, a fully contained community, a master planned resort, a master planned development or phased project (WAC 197-11-164).
- “Proposal” means a proposed action that may be an action or regulatory decision of an agency, or any action proposed by applicants (WAC 197-11-784)

Other Terms

Highway 99 Subarea Plan – Planned Action Ordinance

The Planned Action area may be referred to as the Highway 99 Planned Action Area, Highway 99 Subarea, project area or project site in this document.

General Interpretation

Where a mitigation measure includes the words “shall” or “will,” inclusion of that measure is mandatory in order to qualify a project as a Planned Action. Where “should” or “would” appear, the mitigation measures may be considered by the project applicant as a source of additional mitigation, as feasible or necessary, to ensure that a project qualifies as a planned action and/or to reduce or avoid impacts.

Unless stated specifically otherwise, the mitigation measures that require preparation of plans, conduct of studies, construction of improvements, conduct of maintenance activities, etc., are the responsibility of the applicant or designee to fund and/or perform.

DEVELOPMENT PROPOSED UNDER THE PLANNED ACTION

The proposal reviewed in this EIS include designation of the Highway 99 Subarea (see Exhibit A) as a Planned Action area for the purpose of SEPA compliance, pursuant to RCW 43.21C.440 and WAC 197-11-164, adoption of comprehensive plan amendments for the Highway 99 Subarea, and adoption of zoning code amendments addressing zoning classifications, design standards, parking standards. The planned action designation would encourage redevelopment in the Highway 99 Subarea to create increased housing choices and an attractive pedestrian-oriented streetscape, provide opportunities for medical services growth, provide for enhanced multi-modal mobility, and provide for a greater mix of uses in the subarea. Under this Planned Action, redevelopment would add about 3,013 new jobs and 3,325 new housing units through 2035.

MITIGATION

Based on the EIS, which is incorporated by reference, this Mitigation Document summarizes significant adverse environmental impacts that are anticipated to occur in conjunction with the development of planned action projects. Mitigation measures, identified in the EIS, are reiterated here for inclusion in conjunction with proposed projects to mitigate related impacts and to qualify as planned action projects.

Consistency review under the Planned Action, site plan review, and other permit approvals will be required for specific development actions proposed under the Planned Action designation (WAC 197-11-172). Additional project conditions may be imposed on planned action projects based upon the analysis of the Planned Action in relationship to independent requirements of the City, state or federal requirements or review criteria.

Any applicant for a project within the planned action area may propose alternative mitigation measures, if appropriate and/or as a result of changed circumstance, in order to allow an equivalent substitute mitigation for identified impacts. Such modifications would be evaluated by the City SEPA Responsible Official prior to any project approvals by the City.

Highway 99 Subarea Plan – Planned Action Ordinance

As permitted by WAC 197-11-660, there may be some adverse impacts that are unavoidable because reasonable or feasible mitigation cannot be achieved for the Planned Action

The combination of regulations applicable to each element of the environment and mitigation measures identified in the EIS and documented in this Mitigation Document that are applied to any planned action proposal will adequately mitigate all significant environmental impacts associated with planned action proposals, except for those impacts that are identified as significant unavoidable adverse impacts.

Mitigation measures are provided below for each element of the environment considered in the EIS.

ENVIRONMENTAL IMPACTS AND MITIGATION MEASURES

The EIS identifies significant impacts, unavoidable adverse impacts, and mitigation measures for potential impacts associated with land use, plans and policies, aesthetics, transportation, and public services and utilities. Please refer to the Draft and Final EIS for complete text associated with each element of the environment. The following lists all mitigation measures applicable to impacts for each element of the environment.

Land Use Mitigation Measures

Incorporated Plan Features

The zoning code includes provisions to minimize the impacts associated with increases in building height and changes in land use patterns under the Planned Action. The proposed Subarea Plan includes policy language in support of the proposed stepback development regulations, which are intended to help mitigate for potential land use conflicts around the edges of the subarea.

Applicable Regulations and Commitments

Zoning designations provide sufficient capacity to accommodate the City's growth targets for the subarea. When combined with the City's remaining existing development and design standards, the Planned Action stepback standards will mitigate for land use incompatibilities in areas where the updated CG zone abuts single family zones.

Additionally, existing development and design standards require site design to be compatible with existing and planned character of the nearby area. Applicable site development standards include those for setbacks, screening and buffering, site design, lighting, building design and massing, and others.

Other Mitigation Measures

No additional mitigation measures are recommended.

Plans and Policies

Incorporated Plan Features

The locally-designated role of the Highway 99 Corridor will continue to be maintained and reinforced through the plan vision for a high density, walkable mixed-use neighborhood with urban amenities.

Within the Planned Action, amendments to the Comprehensive Plan clearly identify three distinct districts anchored around major transportation gateways and employment clusters, such as the hospital and international businesses (*Recommendation 3.1, February 2017 Draft Highway 99 Subarea Plan*). These amendments will bring the Comprehensive Plan and recommended Highway 99 Subarea Plan into alignment.

Regulations and Commitments

As required by the Growth Management Act (GMA), the draft Subarea Plan and regulations have been submitted to the Washington Department of Commerce for review and comment prior to final adoption.

Other Mitigation Measures

No additional mitigation measures are recommended.

Aesthetics Mitigation Measures

Incorporated Plan Features

The City's Highway 99 Corridor and Medical/Highway 99 Activity Center Comprehensive Plan Map designations within the Comprehensive Plan will guide aesthetic improvements under the planned action. Such improvements shall make the area more attractive and pedestrian friendly by:

- ensuring that the design of new development contributes to the quality and character of the area
- encouraging a variety of building types
- using landscaping and buffering to soften street fronts and to provide transitions between more and less intensive uses
- fostering distinct sub-district identities consistent with the Highway 99 Corridor Vision.

Additionally, the Planned Action contains policy guidance and recommended transportation improvement projects that are intended to enhance the aesthetics and urban design of the study area and support the community's vision for the future neighborhood character of the corridor. The policy guidance calls for improvements in signage and wayfinding, using design to strengthen business opportunity, development of a unique district design identity, supporting building types and uses typical of vibrant urban corridors, and making code updates to support more pedestrian- and transit-friendly building forms and streetscapes.

Highway 99 Subarea Plan – Planned Action Ordinance

Regulations and Commitments

Development under the Planned Action will be required to comply with existing development and design standards including those for setbacks, screening and buffering, site design, lighting, building design and massing, and others. These standards require site design to be compatible with existing and planned character of the nearby area.

Additionally, the Planned Action stepback standards provide for transitions in building height and bulk between portions of the subarea zoned for the highest intensity uses and adjacent single family zoned areas.

Other Mitigation Measures

No additional mitigation measures are recommended.

Transportation

Incorporated Plan Features

The City of Edmond's existing planned transportation improvements will help to mitigate for traffic impacts. The near-term and long-term transportation improvements in the Subarea Plan will contribute to the underlying infrastructure that creates transit, pedestrian, and bicycle-friendly places and will indirectly help to mitigate for traffic impacts.

Regulations and Commitments

Near-term and long-term transportation improvements identified in the proposed Subarea Plan will be incorporated into the Comprehensive Transportation Plan's capital improvement projects. The current Comprehensive Transportation Plan process (updating the Plan in a cycle approximately every six years) will be the mechanism for monitoring the LOS at impacted intersections.

The City's current six year Transportation Improvement Program will be used to prioritize projects and identifying funding. Flexibility will be built into each cycle of this program to modify the priority and funding of the capital projects serving the study area as new development occurs and creates opportunities for matching funds from private development; redirecting project priorities and timing to coincide with major developments.

The City will leverage the proposed Subarea Plan and Planned Action Ordinance to request early distribution of state funds (\$10,000,000) earmarked for Highway 99 within Edmonds in the State's Ten Year Transportation Investment Plan. Additionally, the City will continue to compete for funding from state and federal grants and continue to watch for potential new funding sources.

Other Mitigation Measures

Highway 99 Subarea Plan – Planned Action Ordinance

The EIS analysis indicates that mitigation for traffic impacts of improvement projects under the Planned Action would occur in two stages.

Stage 1

The City will work with Community Transit to identify and help implement Transportation Demand Management (TDM) mitigation measures to potentially reduce intersection level of service impacts under the No Action and Preferred Alternatives. The City may also consult with Sound Transit and the Washington State Department of Transportation on this subject.

Residential (any size), commercial (under 25 employees), and mixed-use developments may select from a menu of TDM measures specifically assembled for these types of land uses. The City will work with Community Transit and, if appropriate, other agencies, to develop guidelines and worksheets for property owners or tenants of new developments to formulate a trip reduction plan, provided that where the proposed development already incorporates measures that encourage vehicle trip reduction or transportation demand as part of its proposal. Where specific trip reductions plans are required, plans must be submitted to the Development Services Department prior of building permit application unless a different schedule has been approved by the Development Services Director. The Department will consult with Community Transit on the commute trip reduction plans and recommend any changes..

Developments comprised of larger employers are required to develop and implement TDM plans tailored to their workforce. Employers with 25 to 100 employees are required to develop a TDM plan selecting from the menu of TDM measures described above, or customize their own plan. TDM plans for employers with 100 employees or more must conform to the requirements of the Commute Trip Reduction (CTR) law that is part of the Washington Clean Air Act (RCW 70.94).

Menus of TDM strategies should include tiers of measures that have varying levels of effectiveness and cost including but not limited to measures within the following broad categories and associated example measures:

- Financial incentives, amenities and perks:
 - Fully or partially subsidized transit passes
 - Carpool/vanpool subsidies such as fuel vouchers, provision of vehicles, full or partial coverage of vehicle lease, fuel, insurance and maintenance
 - Car share membership for use by registered carpool and transit commuters
 - Emergency ride home program
 - Company vehicle available for employees who commute by alternative modes
 - Prize drawings to employees or residents who commute by alternative modes
 - Subsidized off-site services such as fitness center, daycare, dry cleaning, bicycle repair and maintenance, etc.

Highway 99 Subarea Plan – Planned Action Ordinance

- Service provided, or delivered, on-site such as dry cleaning pickup and delivery, ATM machine, fitness center, daycare, etc.
- Parking Management Strategies
 - Charge market rate for employee parking
 - Parking cashout program
 - Preferential parking for carpool/vanpools
 - Restrictions or limited on-site parking
 - Unbundled parking
 - On-site bike share and/or car share facilities
- Support Strategies and Assistance
 - Part or full-time on-site TDM coordinator
 - Commute options package for new employees and/or residents
 - Commute alternative information kiosk or website
 - Rideshare matching program
 - Discounts on purchasing bicycles and accessories
 - Sponsored events promoting alternative commute options

Note: Except where required by law or as a condition of approval, monitoring, refinement, and maintenance of individual TDM plans by new development is voluntary after the initial submittal for plan approval.

Stage 2

The City will implement new capacity-enhancing mitigation measures for intersection impacts under the Preferred Alternative. The following new intersection capacity-enhancing mitigation measures will be incorporated into the City's standardized six-year Transportation Improvement Program process for funding and prioritizing transportation projects:

- State Route 99 / 220th Street SW – Widen State Route 99 to add a second southbound left turn lane. This intersection is projected to operate at LOS F under buildout of the Preferred Alternative, exceeding the standard of LOS E even with implementation of the improvement called for in the 2015 Comprehensive Transportation Plan to widen 220th to add a westbound right turn lane and a second westbound left turn lane, and an eastbound right turn lane.
- State Route 99 / 224th Street SW – Convert the eastbound approach of 224th Street SW to provide an exclusive right turn lane, a shared through/right turn lane, and an exclusive left turn lane, or an alternate design as confirmed by further study. This

Highway 99 Subarea Plan – Planned Action Ordinance

intersection would operate at a LOS F under buildout of the Preferred Alternative. This intersection was not studied in the 2015 update to the Comprehensive Transportation Plan and, therefore, does not currently have any planned improvements.

Additionally, the City will take steps to enable the new capacity-enhancing mitigation measures when and if monitoring shows that the measures are required, and implement the improvements, as the following opportunities arise:

- Require any new development, redevelopment or site improvements requiring a building permit on the properties adjacent to the impacted intersections to not construct any form of structure or infrastructure (except landscaping or other streetscape improvements) on, under, or above the right of way potentially needed to be acquired for the intersection capacity improvements.
- Coordinate with WSDOT and adjacent municipalities on the potential land acquisitions needed for the intersection capacity improvements located within their jurisdictions and, if possible, request the adjacent municipalities to apply the same building restrictions.
- As funds become available through the City's Transportation Improvement Program process, construct the capacity improvements. This may include acquiring the necessary right of way from adjacent property owners through purchase or negotiated dedication.

Public Services

Incorporated Plan Features

Proposed transportation projects under the sub-area plan would improve pedestrian and bicycle character, access, and mobility within the study area, particularly crossing Highway 99. As such, east-west access across Highway 99 to park and recreation facilities would improve.

The sub-area plan provides greater incentive for mixed-use and commercial development in proximity to existing infrastructure on SR-99, making more efficient use of available stormwater capacity. Additionally, planned streetscape improvements under the Action Alternative would increase landscaping along the street – trees and other landscaping provide a natural ability to absorb stormwater and release it slowly to the atmosphere.

The City will continue to pursue energy efficiency measures to reduce energy consumption, thereby reducing stress on Snohomish County PUD as residential and jobs growth occurs. The sub-area plan encourages sustainable building practices, including considering requiring electric vehicle charging facilities and encouraging solar panels (*Recommendation 2.2 and 2.3, February 2017 Draft Highway 99 Subarea Plan*).

Regulations and Commitments

Police

Highway 99 Subarea Plan – Planned Action Ordinance

The Police Department will implement the 2016 agency goals to the extent feasible in its 2016-2021 *Multiyear Strategic Plan*. These goals include:

- bringing the Street Crimes Unit and second K-9 team back on line
- partnering with City Council and the Edmonds School District to secure funding for a School Resource Officer for Edmonds-Woodway High School
- establishing by policy the Peer Support Team to assist Department members and their families in time of need
- working with SNOCOM and Bair Analytics to secure a crime analysis workstation which interfaces with records management and helps bring a public crime mapping portal on-line

As recommended in the 2016-2021 *Multiyear Strategic Plan*, the Police Department should maintain, at a minimum, the current staffing ratio of 1.35 commissioned officers per 1,000 residents. Additionally, the Police Department should continue looking to future budget cycles and preparing to pursue and justify the addition of commissioned staff as the economic climate allows.

As recommended in the 2016-2021 *Multiyear Strategic Plan*, the Police Department should restore the Crime Prevention Officer position to aid the Department's ability to conduct crime prevention training and strategies for businesses, apartment management, various concerned groups, and individuals.

Fire and Emergency Medical Services (EMS)

Ongoing capital facilities improvement, budgeting, and operational planning by Fire District 1 and the City of Edmonds are anticipated to address incremental increases and other changes in demand for fire services, including the need for additional personnel, additional apparatus, and facility improvements. Fire District 1 recently completed the first Phase of a Capital Facilities Plan which evaluates existing conditions, including an inventory and assessment of existing facilities. Phase 1 of the plan indicated a need for minor near and mid-term maintenance and repairs at Stations 16 and 17, as well as potential seismic or safety upgrades. Station 20 is identified as one of 5-6 stations throughout the district which should be considered for replacement to support operation needs and code deficiencies (Fire District 2016c, 46, 48, C114-C145). Phase 2 will forecast future needs and phase 3 will provide an estimate of capital facility funding necessary to execute the plan, an implementation timeline and a recommended funding approach.

All potential development in the study area will be constructed in compliance with the City's current Fire Code (ECDC 19.25), which is comprised of the 2015 International Fire Code with Edmonds Amendments. Adequate fire flow to serve potential development will be provided as required by the Fire Code. Potential development will *Highway 99 Subarea Plan – Planned Action Ordinance*

also be required to comply with code requirements for emergency access to structures. The Department of Fire Prevention also reviews proposed street improvements on a project-by-project basis to identify potential negative impacts on response times and ensure street improvements are consistent with the City's Fire Code.

A portion of the tax revenue generated from potential redevelopment in the study area would accrue to the City and Fire District 1 to help fund additional fire and emergency medical services.

Schools

Ongoing capital facilities improvements, budgeting, and operational planning by the District, in conjunction with the City of Edmonds, are anticipated to accommodate projected student enrollment at acceptable levels of service over the next twenty years.

The School District will continue to replace, expand, modernize, and upgrade District facilities as approved by voters in the 2014 Capital Construction Bond and should implement the goals identified in Edmonds School District's Strategic Direction (ESD 2014).

Parks and Open Space

The City will, to the extent feasible, implement goals identified in the 2014 *Parks, Recreation, and Open Space Plan* which improve the park system within or near the study area to address geographic gaps in service (Edmonds 2014, 4-1 – 4-11).

Specifically, impacts identified in the EIS should be mitigated by:

- Expanding the partnership with the Edmonds School District, including negotiating an agreement for expanded, year-round public use of school grounds, sports fields and gyms for recreation purposes (Goal 1.A).
- Exploring property acquisition and development with partners, including the School District, Snohomish County and other public and private entities – continue to partner with neighboring and overlapping jurisdictions (cities, counties, school districts) as well as private entities (i.e. churches) to expand recreation opportunities for the community; continue discussions for possible acquisition of Esperance Park from Snohomish County for annexation and redevelopment into a community park with sports fields, community gardens, picnic shelters, and other recreation features; and consider acquisition of County park land within or adjacent to Edmonds (if made available), such as Chase Lake (Goal 2.C).
- Acquiring park land in the Highway 99/SR 104 areas to provide adequate park service in redeveloping areas. Create new civic spaces to enhance investment and revitalization while meeting recreation needs, especially where service gaps exist, or higher residential impact is planned (Goal 2.G).

- Defining the best routes for and treatments to create central north-south and east-west pedestrian and bicycle corridors, incorporate these into the City's transportation plans, and implement improvements (Goal 2.N).
- Increasing connections to the Interurban Trail, using signage, sidewalks, curb extensions, and other pedestrian/bicycle enhancements, especially focusing on crossing Highway 99 (Goal 2.O).
- Strongly considering the formation of a Metropolitan Park District in order to sustain the level of quality expected by the community while growing to meet future needs (Edmonds 2014, 5-5).

Electricity

Ongoing capital facilities improvements, budgeting, and operational planning by Snohomish County PUD are anticipated to address incremental increases and other changes in demand for electricity. Depending on the level of development and associated new loads, feasibility studies should be conducted for individual projects as part of the development review process. System capital projects should be developed to meet the demands of future loading if capacity improvements are necessary (Ha pers. com).

Stormwater

Any redevelopment or new development under both alternatives would be subject to today's stricter regulations governing stormwater. Green design and construction methods should be employed in buildings, streetscapes, and drainage features to detain and treat stormwater (Ecology 2014, 8-10).

The City's *Storm and Surface Water Management Comprehensive Plan* (2010) will guide infrastructure improvements. Specific elements of the stormwater improvements will be defined by the requirements of the State-mandated *NPDES Western Washington Phase II Municipal Stormwater Permit*. Under this set of regulations, the City maintains measures to protect and improve runoff conditions in relation to the receiving waters. The City of Edmond's stormwater management requirements and ongoing efforts are included in:

- Edmonds Community Development Code 18.30 and Stormwater Code Supplement to 18.30 (Edmonds 2010b; Edmonds 2016c) – the City is nearly finished updating the Stormwater Code and Supplement, anticipated to be adopted January 1, 2017 (Cawrsepers. com)
- Storm and Surface Water Management Comprehensive Plan (Edmonds 2010a)
- Stormwater Management Program Plan (Edmonds 2016f)

Other Mitigation Measures

Police

Highway 99 Subarea Plan – Planned Action Ordinance

The City will monitor growth and demand for police services in the study area in order to determine if/when additional personnel are needed and will regularly review trends to ensure the Police Department has enough advance time to address the needs.

New development under the Planned Action will employ Crime Prevention through Environmental Design (CPTED) techniques - incorporation of design features into development that would help reduce criminal activity and calls for service, including orienting buildings toward the sidewalk and public spaces, providing connections between buildings, and providing adequate lighting and visibility.

Fire and Emergency Medical Services (EMS)

The City will monitor growth and demand for fire and emergency medical services in the study area in order to determine if/when additional personnel, equipment, or facilities are needed and will regularly review trends to ensure the City and Fire District 1 have enough advance time to address the needs.

The City and Fire District 1 should work together to plan for pedestrian improvements, such as wider sidewalks, to ensure that the opportunity for emergency vehicle maneuvering is maintained.

Additionally, the City should continue efforts to find sufficient resources to retain and improve Fire District 1's current level of services provided. Efforts include exploring additional funding sources – such as a Fire Benefit Charge or Levy Lid Lift; pursuing ways to reduce unnecessary costs/eliminate redundancy, including potential opportunities to partner with neighboring cities, Fire District 7, and other Fire Protection agencies through regional consolidation; and planning for the possible formation of a Regional Fire Authority in South Snohomish County.

Schools

The Edmonds School District tracks information on growth in enrollment and demand for educational programs offering across all grade spans in the region, including the study area, as part of its determination about if/when additional personnel or facilities are needed. The City will periodically review trends and information from the Edmonds School District, to ensure the City and the Edmonds School District have enough advance time to address the needs, including grade configuration, optimum facility size, educational program offerings, classroom utilization, scheduling requirements, and the use of temporary classroom facilities.

Additionally, the Edmonds School District will continue to evaluate both condition and capacity of existing facilities at Westgate and Sherwood Elementary Schools to determine if capital improvements are needed.

Parks and Open Space

The following steps should be taken to mitigate for impacts to Parks and Open Space under the Planned Action:

- Require on-site open space as a residential amenity for new development.
- Encourage and promote public open spaces through public/private partnerships where possible.
- Implement pedestrian and bicycle transportation improvements to provide greater access to existing facilities within one-half mile of the study area, with a focus on removing Highway 99 as a physical barrier.
- Existing recreational programs may see increased enrollment and increased revenue as people living in the study area enroll in more programs. This increased enrollment may marginally help offset the costs of providing additional facilities.

Electricity

The following steps should be taken to mitigate for impacts to the electrical grid under the Planned Action:

- Evaluate and identify future service system needs through coordinated electricity demand planning between the City Development Services Department and Snohomish County PUD.
- The PUD is currently undergoing smart grid infrastructure modernization of its electrical distribution system to improve reliability and increase efficiencies for its customers.
- Where feasible, reduce the use of power in building heating and cooling through passive systems and modern power saving units.

Stormwater

No additional mitigation measures are recommended.

to call each Councilmember individually or send an email that may or may not be read. He recommended retaining the opportunity for public comment at committee meetings and having the meetings video-recorded and available on the City's website to achieve the goal of transparency.

6. PUBLIC HEARINGS

1. PUBLIC HEARING ON HIGHWAY 99 SUBAREA PLAN

Development Services Director Shane Hope introduced the Highway 99 Subarea Plan. Materials in packet including draft plan, Planning Board minutes and other materials and public comments online. she introduced John Fregonese, Principal, Fregonese Associates. Mr. Fregonese provided an aerial of the project area on Highway 99. He reviewed:

- Planning Process
 - March – April 2016: Understanding existing conditions
 - April – June 2016:
 - April – Nov 2016: Develop land use and transportation scenarios
 - Oct – Dec 2016: Develop Sub-Area Draft plan
 - Dec – Feb 2017: Final Sub-Area Plan
- March 2016 Public Workshop
 - Identified opportunities for new housing and business, community centers and services, and infrastructure upgrades
 - What did the public want?
- May 2016 Open House
 - Revealed near and long-term development and transportation opportunities and its impacts
- November 2016 Open House
 - Revealed implementation strategies and policy recommendations
 - Public had opportunity to review the recommendations at the Open House and online and provide feedback
 - Community values:
 - Connectivity
 - Destinations
 - Beautification
 - Safety
 - Walkability
 - Affordable housing
 - Healthy businesses
- Distinct Subdistricts
 - Major local and regional destinations on Hwy 99
 - International District
 - Diverse restaurants, grocers and shops; major Korean business cluster
 - Health District
 - Swedish Hospital and medical offices
 - Gateway District
 - Identified by the community during workshop
 - Desire for “gateway” and distinct transition point in and out of Edmonds
- Long segments without crossings
 - Central area requires 10-minute walk to find safe crossing
- Housing development
 - Widespread desire for housing, particularly in south
- Mixed Use Development
 - Widespread desire for mixed use, particularly in south and central

- Pedestrian Crossing
 - More mid-block crossings throughout
- Key Assets in Corridor Area
 - Opportunity to build on the momentum of ongoing improvements in Shoreline along Hwy 99
 - High-quality transit facilities already in place providing links to housing, jobs and amenities in the region
 - Distinct districts already emerging – International and Health Districts – that provide core services and amenities
 - Already a mixed-use district with retail uses adjacent to single- and multi-family housing
 - Business, developer community and residents are ready to see positive changes to create a safe, walkable, healthy place

Mr. Fregonese reviewed Implementation Strategies, Policy Recommendations + Actions

- Draft Zoning & Development Recommendations
 - Strengthen Economic Opportunity
 - Support unique business clusters such as International District and Health District
 - Major auto sales facilities remain important to the local economy. Pedestrian Activity Zone standard will allow auto sales to continue business as usual
 - Strengthen and continue support for business orgs. in county and state
 - Pursue broadband internet throughout corridor to attract high-tech business investment
 - Consider unique designs for streetscape improvements such as signage and lighting
 - Encourage Sustainable Building Practices
 - Transit and pedestrian-friendly development with less reliance on automobile trips, should be promoted through new design standards
 - Consider requiring electric vehicle charging facilities especially within new residential developments and bicycle facilities
 - Encourage use of solar panels and green building practices
 - Map of proposed update to Comprehensive Plan designations
 - Health District
 - International District
 - Gateway District
 - Current Zoning Map
 - The only difference between CG and CG2 is the height limit (CG = 60' and CG2 = 75')
 - Many current zones are remnants from the counties antiquated zoning
 - Many zones do not match with the parcel boundaries
 - Proposed Zoning Map
 - The proposal is to change these zones to the consolidated CG zone
 - Incorporate design standards directly into zones to ensure scale transition into neighborhoods
 - More predictable outcomes for community
 - Comprehensive Plan Map
 - New zoning is consistent with the Comprehensive Plan map
 - Strengthen current design standards
 - Incorporate them directly into the zoning code
 - Consideration of special circumstances within the corridor will be made to ensure the standards are feasible, such as large parcels that would have multiple buildings if redeveloped and parcels with unique access or transportation challenges may require a modified approach to the design standards.
 - Changes to Access and Parking

- Issue Today: current standards allow too much parking in the front of buildings, which negatively effects the pedestrian environment and hinders redevelopment potential
- Existing District-based Design Standards Hwy 99 Corridor CG/CG2 – criteria
 - Access and Parking
 - Not more than 50 percent of total project parking spaces may be located between the building’s front facade and the primary street. Parking lots may not be located on corner locations adjacent to public streets
- Recommended Design Standards for Urban Areas
 - Parking Predominately on Side or Rear
 - Parking areas may comprise 40% or less of street frontage
 - Better design than current standard: no more than 50% of total project parking spaces may be located between building’s front façade and the primary street
 - Buildings on the Street Frontage
 - Primary Frontage – min. 50% of primary street frontage should have buildings within 10 feet of front property line (at the edge of Pedestrian Activity Zone)
 - All Other Frontages – 50% of side and rear street frontages to have buildings, walls, or hedges at least 4 feet in height, within 10 ft of property line
 - Ground Floor Transparency
 - 50% of Primary Frontage building façade within 10 feet of frontage lot line be made of transparent windows and doors. All other building frontages require 30% transparency.
- Current Standards
 - Screening and Buffering – Parking lots
 - Type IV landscaping, minimum four feet wide, is required along all street frontages.
 - This standard creates landscaped barriers between pedestrians and buildings rather than enhancing a safe and comfortable pedestrian zone
- Changes to Screening and Buffering “Pedestrian Activity Zone”
 - Replace with required 10-foot Pedestrian Activity Zone
 - Allows for a range of active uses like sidewalk cafes and amenities such as public art, street furniture, street trees, bus shelters, pavement patterns, lighting, etc.
 - Expanded Sidewalk Width
 - Amenity Space
 - Outdoor amenity space, such as landscaping, benches, etc. should be required in conjunction with development
 - A portion of the required amenity space should be provided as common space and may include pedestrian areas
- New Stepback Design Standards
 - No current stepback regulations exist
 - Purpose: Ensure a transition in height and bulk between multifamily/mixed-use buildings in commercial zones and adjacent single family zones, while enabling more housing options to be built adjacent to Hwy 99
- Recommended Front Setback for Multifamily and Mixed-Use Adjacent to Single Family Zones
 - GROUND FLOOR SETBACK
 - For frontages on Highway 99, require a front setback of 10 feet to accommodate a Pedestrian Activity Zone.
 - For frontages not on Highway 99, reduce frontage setbacks to 5 foot and encourage enhanced pedestrian realm (larger sidewalks, useable landscaping, etc.).
 - Keep current 15 feet setback and 10’ landscaping requirements for lot line adjacency with single family zones.

- Recommended Front Stepback for Multifamily and Mixed-Use Adjacent to Single Family Zones
 - UPPER FLOOR SETBACK
 - Zero setback up to 25 feet in height (30 feet is the maximum height in RM 1.5, which is the predominant zone surrounding the commercial zones on Highway 99).
 - 10 foot setback beyond 25 feet in height on sides with lot line adjacency to single family zones
 - 20 foot setback beyond 55 feet in height on sides with lot line adjacency to single family zones
 - 8 foot stepback beyond 30 feet in height on sides across a street from single family zones
 - Stepback areas can be used for active outdoor space such as balconies.
- Recommended Transit-Supportive Parking Standards
 - Reduce minimums; follow market demand for parking
 - Residential
 - Current: Studio: 1.2 / 1-Bedroom: 1.5 / 2-Bedroom: 1.8 / 3-Bedroom: 2
 - Recommended: One space per unit that is less than 700 sq. ft.
 - Commercial
 - Current: 2.5 per 1,000 square feet (1 per 400 sq ft)
 - Recommended: 2 per 1,000 square feet
 - Exempt first 3,000 sq ft of commercial uses within mixed-use buildings that have a shared parking plan (parking study and management plan)
 - Reduces cost burden for small, local entrepreneurs
 - Staff decision on compliance

Mr. Fregonese continued his review:

- Draft Affordable Housing Recommendations
 - Define Hwy 99 area as a “target area” to allow Multi-Family Tax Exemption (MFTE) projects
 - Pass ordinance to enable MFTE project in Hwy 99 area
 - All residential-portion of building value tax exempt for 12 years with at least 20% affordable units
 - Continue to enhance fee waiver program for affordable housing
 - Enhance its City program to allow the reduction of transportation and park impact fees for projects that include affordable housing
 - Mixed-Use, Mixed-Income Demonstration Project
 - Identify site with willing owner or City purchase/transferrable option
 - Actively recruit developers (non-profit; public-private)
 - Pilot project for new MFTE and fee waiver programs, and other possible special assessment districts
 - Other Affordable Housing and Infrastructure Finance Recommendations
 - Key Financing Tools and Funding Sources to Pursue
 - City Fund for Redevelopment and Affordable Housing
 - Community Renewal Area (CRA) – used in Shoreline
 - Hospital Benefit Zone (HBZ) Financing Program
 - Local Infrastructure Project Area (LIPA) Financing
 - Landscape Conservation and Local Improvement Program (LCLIP)
 - Low Income Housing Tax Credits (LIHTC)
 - HUD HOME Program

- HUD CDBG Program
 - Enterprise Community Partners Regional Equitable Development Initiative
- Draft Signage & Wayfinding Recommendations
 - Gateway Signage at Hwy 99/Hwy 104 interchange
 - Transit Gateway Signage/Station at Hwy 99/228th
 - Improve wayfinding signage along corridor
 - Identify downtown, Lake Ballinger, multiuse path (Interurban Trail), new regional rail, International District, Health District, other activity nodes
 - Unique District Design Identity
 - Branding, public/local art, street furniture, unique bus shelter designs, pavement patterns, special lighting fixtures, colored crosswalks, banners, etc.
 - Prohibit new pole signs
- Draft Transportation Recommendations
 - Improve Transit Transfers:
 - Unify/consolidate BRT and local transit stops to reduce walking distance for transfers
 - Hwy 99 and 228th will be Key Intersection
 - 228th will connect Edmonds to regional rail in Mountlake Terrace.
 - Shuttle/transit service from Hwy 99 to regional rail station
 - Consolidated transit stop at 228th
 - New BRT station
 - Clear signage
 - High quality bike connection on 228th
 - Incentivize Alternative Transportation Options:
 - Car Share/Bike Share
 - On-site bike parking
 - Ride sharing services (Uber, Lyft)
 - Electric car charging stations
 - Incentives: Impact fee reductions and parking requirement offsets for providing dedicated accommodating alternative transportation options on-site
- Draft Transportation Infrastructure Recommendations
 - Expand use of grant programs to fund safety improvements and pedestrian facilities
 - Safe Route to School Program
 - Pedestrian and Bicycle Program
 - Highway Safety Improvements Program (HSIP)
 - Congestion Mitigation & Air Quality Program (CMAQ)
 - Proposed Short & Long-Term Transportation Improvements
 - Transportation Goals:
 - Improve pedestrian safety and access to/from Hwy 99 corridor
 - Improve pedestrian environment along Hwy 99 corridor
 - Safe pedestrian crossing of Hwy 99 and access to transit
 - Improved transit mobility and transit stop environment
 - Improved traffic flow and general safety with access management
 - Key Recommended Transportation Improvements
 - Close the most significant gap in the pedestrian crossings within the corridor
 - 238th to 228th – a distance of 3,700 ft. – has no controlled pedestrian crossing
 - Improve pedestrian access from the south at the SR 104 interchange
 - Long-term recommendation: Reconfigure ramps to conventional 90-degree stop control intersections
 - Short-term recommendation: Install Rectangular Rapid Flashing Beacons (RRFB's) with high visibility crosswalk

- Encourage walking and biking to access plan Highway 99 from surrounding neighborhoods
 - “Complete Streets” – sidewalks, safety lighting, street lighting, pedestrian-scaled lighting, drainage improvements, etc.
 - Streets that can be safely traveled by pedestrians and bicyclists at night will experience travel during the day
 - Improve connections between transit and major employment centers
 - Swift Stations at 216th and the Swedish Hospital Campus: Implement a pedestrian walkway system within campus with wayfinding
- Maps of Planned Transportation Improvements and Project Supportive Transportation Improvements, identifying the location of:
 - Improvement Index Number
 - Existing Signalized Intersection and Ped Xing
 - Proposed New Traffic Signal and Pedestrian Crossing
 - Intersection Safety & Capacity Improvements
 - New Bike Route Designation
 - New Class II Bicycle Lanes
 - Street Improvements for Pedestrians
 - Access Management (Raised Medians)
 - Pedestrian Hybrid Signal (HAWK)
- Photographs of:
 - Health District Gateway
 - Today
 - With initial public investments
 - With corresponding private investments
 - SW 234th
 - Today
 - With initial public investments
 - With corresponding private investment
- Next Steps:
 - March 21 – City Council Hearing of Subarea Plan
 - March 22 – Open House for Planned Action EIS
 - April 12 – Planning Board public hearing on Development Code Amendments and Planned Action Ordinance (PAO)
 - May 9 – City Council public hearing on Dev. Code and PAO
 - June 5 – Subarea Plan, Dev. Code Amendments and PAO Adoption

Further information such as project updates, workshop results, upcoming events and more are available at www.EdmondsHWY99.org.

Councilmember Buckshnis said the packet is very thorough and well done and can address any questions the public has.

Councilmember Teitzel referred to the Burlington Coat Factory property in the Gateway District and the Ranch Market in the International District, recalling Mr. Fregonese saying it may not be possible to redevelop certain areas like those in close proximity to the street. He asked what was envisioned for properties like those. Mr. Fregonese said in those situations, they recommend a functional slip lane where there is a major road with a smaller road for parking. A 1-2 lane parking lot with good crossings could be located in front of the building with pedestrian aspects to buffer pedestrians from the road. There may be places where the building could be located close to the road but have parking in front in areas where it is not feasible to walk along the road. Ms. Hope said the details would be specified in the code.

Councilmember Nelson recognized this is a very ambitious plan that will provide more housing and more affordable housing. He requested Recommendation 2.3, the use of solar panels and green building practices even beyond current standards should be encouraged, be emphasized. If the goal is new development, it needs to be done in an environmentally sensitive way and provide opportunities for solar power and renewable energy sources. Mr. Fregonese agreed, commenting solar is becoming one of the cheapest sources new of new power, and is much more feasible than it was five years ago.

Councilmember Tibbott referred to the commercial district around the medical center, a service industry that provides some commercial. He asked what kind of tax base those services provide the City and what other services were envisioned that would create economic development for the City. Mr. Fregonese answered uses that occur around a medical facility such as Swedish Hospital include medical offices, pharmacies, medical devices, imaging, etc. as well as uses that serve employees and customers of the hospital such as restaurants, coffee shops, personal services, etc. He summarized uses would include a mixture of those found in any employment concentrated area plus specific medical services. Councilmember Tibbott asked if his studies found opportunity for additional services in that area that justify making it a designated area on Highway 99. Mr. Fregonese answered yes, noting Swedish Hospital is not done expanding. The area is underserved now; there is a campus but not a lot of the services found around a more mature hospital to serve the hospital's customers and employees.

Councilmember Fraley-Monillas observed the presentation did not identify specific types of affordable housing such as low income or senior housing. Mr. Fregonese answered there is a variety of housing, it is all feasible and these types of buildings are great for senior housing and low income housing but they have not specified a target. Ms. Hope said some market rate housing is also expected. Affordable housing is a range of housing that includes low income, moderate income, workforce housing, etc. Market rate housing would help ensure it is a viable, lively neighborhood. Mr. Fregonese said proformas were done on all the redevelopment sites and designed at or slightly below the median market rate without assistance/subsidy that 45% of median income could afford to rent. Market rate housing is not luxury housing; it is affordable to working incomes without a subsidy. The density and lack of parking means projects can be at lower, more competitive rates which allows the subsidy to go further for the affordable component.

Councilmember Fraley-Monillas commented affordable housing is not necessarily affordable for low income or seniors on a fixed income such as on Social Security. She relayed her understanding that that would all be possible, but it would depend on the interest of the property owners. Mr. Fregonese answered it will depend on the interest of the property owner and the advocacy of the lower income housing community to partner. He referred to the recommendation to do a mixed income housing demonstration project, an area where the City can provide leadership on a private-public partnership that will provide market rate housing and affordable housing of some type. A demonstration project will put in place a path forward to catalyze other development. That cannot be regulated; the City will need to be opportunistic and look for partners to put that together.

Councilmember Fraley-Monillas asked about the plans for 238th near Safeway where the primary crossings are located and where people congregate to catch SWIFT. Mr. Fregonese referred to the map, identifying a bike path, pedestrian enhance, SWIFT stops on both sides, access controls, and a pedestrian crossing in that area. Councilmember Fraley-Monillas asked if there were plans for any housing in that area. Mr. Fregonese pointed out properties (purple on the map) where redevelopment was most feasible. He assured that was just a guess, it could happen anywhere but newer buildings or properties with an active business were unlikely to redevelop.

Mayor Earling opened the public participation portion of the public hearing.

Wendy Kendall, Edmonds, referred to the photograph of the SW 234th intersection on Highway 99 where it appeared lanes were lost. She expressed concern with the existing traffic on Highway 99 and feared additional signals would further affect traffic flow. She asked if there was any prediction on property tax increases for residences within a couple blocks of Highway 99.

Rich Senderoff, Edmonds, recalled when he served on a previous Economic Development Commission, there was often discussion about the concept of a hotel on Highway 99, especially one near the hospital. When Swedish first acquired Stevens, representatives making a presentation to the EDC also talked about the need for hotel for people visiting patients that could also serve players and patrons of the nearby ice rink where Pac-12 hockey is played. In addition to residential, he suggested including concepts that would attract a hotel group and possibly involving Swedish Hospital in that effort.

Marjory Green, Edmonds, asked whether the new buildings would be higher. She has heard there will be major changes at Mill Park Condominiums at 224th & Highway 99 and asked if there were any plans for changes in that area.

Rebecca Wolfe, Edmonds, suggested the City contact American Council for an Energy-Efficient Economy, ACEEE.org who has stated through more conservation and clean renewable energy such as wind and solar, the United States can be completely off fossil fuels by 2030. She encouraged the Council to think of every possibility to save energy for renters.

Robert Siew, owner of the property where Burlington Coat Factory is currently located, said the design requirement to have building close to the road works great if the traffic is slow and there is parking on both sides but it is difficult to achieve on all parcels on Highway 99 and imposing a uniform requirement may impede the design for specific developers. His property is a classic example; he has attended all the Highway 99 Subarea Plan meetings and has been in contact with Ms. Hope and the design consultant who are aware of his concerns. There are only a few large parcels on Highway 99 like theirs such as Aurora Marketplace and Ranch Market. For large parcels, especially those with existing buildings with tenants, it is difficult to develop the entire parcel at once because a new building at the street will block the existing business' visibility. They are anxious to try a mixed-use development on the former McFinster's site with retail on the bottom floor and residential above. They have a 30-year lease with Burlington who does not want anything further west than their building that would block their visibility. With that requirement, they will not be able to redevelop the restaurant property. He encouraged the Council to allow as much flexibility as possible to address issues on specific parcels to avoid impeding development.

Imad Bahbah, Principal, IHB Architects, Kent, said he represents a developer who has been through the pre-application process for a housing mixed use project off 238th. He expressed support for the Council's approval of the Highway 99 Subarea Plan in hopes of gaining developer incentives. The initial proposal was .75 stalls per unit and was increased to 1.0 for 700 square feet. He encouraged the Council to reduce that to stay competitive particularly in a transit-oriented zone and inquired about the parking ratio for units over 700 square feet. They postponed their project to see how the plan develops and in hopes of better incentives including the MFTE. He encouraged Council to approve the Subarea Plan so they can proceed with development of their project, 7 stories, approximately 68 units if the parking ratios work, and about 4500 square feet of retail on the first level, pedestrian oriented, etc. He was glad to hear the community and citizens in the area were supportive of housing in the area and suggested publicizing that on the City's website attract to development.

Susie Schaefer, Edmonds, suggested including incentives for native plants in the plan. She saw this as a great opportunity because native plants have a better chance for survival, plants and trees makes it more

of a neighborhood and make an area more inviting. She pointed out Shoreline is looking a lot better and native plants give a place class.

Bee Wilson, Edmonds, said she lives downtown but has grandchildren and great-grandchildren growing up near 228th & 80th. She thanked the Council for the study. As the plan will lead to more people, roof space, etc. she suggested including raingardens and bioswales in the plan as well as considering pervious pavement.

Stanley Piha, Edmonds, a member of the former Highway 99 Task Force, acknowledged the work done by staff and the consultant on the plan. He supported blanketing the subarea with the General Commercial Zone including parcels that are currently zoned RM2.4.

Glenda Smith, Edmonds, said she lives close to Ranch Market and is very worried about the proposed plan. At a recent Mill Park Condominium HOA meeting, the Board informed her that half their building is residential and the other half is commercial. She lives in the half that is zoned commercial and is worried they may be bought out. She asked whether that was a possibility.

Patrick Doherty, speaking on behalf of the Economic Development Commission, summarized a memo the Commission emailed to the Council:

- Commercial development is important within the corridor. Would like to see a commercial component in mixed use building
- It is important that the Council act quickly on plan. There is interest from the development community, want to capitalize on the current economic climate.
- Once the plan and zoning are approved, an open house or event should be held to attract developers to the community.
- Supportive of any incentives for development
- Supportive of proposal to find a way to create a catalyst project
- Interested in more specificity regarding how to enhance the sense of the International District as a special place

Seth Hale, Seattle, an architect, recommended the Council adopt a plan that will provide a lot of design flexibility, and give architects an opportunity to provide design alternatives and create unique environments. With regard to the setbacks, he observed the proposal for an 8-foot setback for property across the street from residential at 30 feet above the street. He suggested the height be coordinated with the setback for properties adjacent to single family so that the setback for a building on a corner would occur at the same location. He pointed out 30 feet is an awkward elevation for that change as typically the first floor would be 15 feet and upper residential floors 10 feet.

Bruce Witenberg, Edmonds, congratulated the Development Services Department on the public process for the Highway 99 Subarea Plan. The public meetings were well attended and included thoughtful questions and commentary. Despite the long process, he echoed the concerns of some citizens that the draft plan has only been available to the public for a week. The breadth of this plan is unlike anything the City has contemplated recently, affecting several square miles and residents need additional time to absorb the complexities presented in the plan and to continue to offer input. He was glad the schedule offers these opportunities. After a brief review, he offered the following comments:

- Would like to see further discussion of the transportation study and hoped the study would take into account the plan's effect on some of the most affordable housing in Edmonds, the single family residences in and beyond the proposed zoning in the plan.
- The plan identifies Gateway, International and Hospital Districts but he could not visualize where additional housing necessary to help the City achieve its GMA targets would be located in the Gateway District.

- Shaded areas (packet page 257) identifying housing development, do not show the number units or residents and he recommended providing more specificity.
- Strongly support transit oriented development including limited parking for multi-family residential units off the Highway 99 corridor. Limiting the amount of parking, perhaps even more than the plans suggests, is a good method for accomplishing the goal of transit oriented affordable housing.

He looked forward to having additional opportunities to comment on the plan. He agreed with Mr. Senderoff's comments about a hotel on Highway 99 and summarized the redevelopment plan for the highway was generally quite exciting and if brought to fruition would greatly enhance Edmonds' portion of Highway 99.

Hearing no further comment, Mayor Earling closed the public hearing.

Mayor Earling advised another public hearing is scheduled on March 21. Ms. Hope said there will be several more opportunities for public input. This is the first public hearing before the City Council; the Planning Board held a public hearing and an earlier draft of the plan was available to the public. No decisions are requested tonight. She offered to develop a Q&A response for the questions that arose during the public hearing as well as talk to individuals with specific questions. The EIS will also answer some questions. Mayor Earling summarized this a work in progress; this is the first step as far as the Council having an opportunity to ask questions and hear public comment.

7. ACTION ITEMS

1. ORDINANCE REQUIRING THE REPORTING OF LOST OR STOLEN FIREARMS

Councilmember Nelson reported since the Council last considered this ordinance, he spoke with Beth Ford, Lighthouse Law Group, and Seattle's City Attorney as well as Seattle Councilmember Tim Burgess, the author of Seattle's ordinance. Ms. Ford was satisfied with information she received from Seattle's City Attorney regarding the ordinance.

Councilmember Nelson pointed out the Department of Justice views lost and stolen firearms as posing a substantial threat to public safety and law enforcement. He believed the proposed ordinance would, 1) promote public safety by enabling law enforcement to better track illegal guns used in the commission of crimes, 2) help return lost guns to their rightful owners, and 3) protect gun owners from being wrongly implicated in crimes committed with their lost or stolen guns. As a gun owner, he did not take anything to do with firearms lightly. The Pledge of Alliance says liberty and justice for all; he asked where was the justice for the thousands of victims who have are killed every year by guns that citizens fail to report as stolen or missing.

Councilmember Buckshnis explained she raised this with Councilmember Nelson last October. She was shocked by the number of guns that are stolen, used in crimes and the number of children killed by guns that were not stored safely. She offered to provide the data she had collected, noting a great deal of information was available from Everytown for Gun Safety. She expressed her support for the ordinance.

COUNCILMEMBER BUCKSHNIS MOVED, SECONDED BY COUNCILMEMBER TEITZEL, TO APPROVE ORDINANCE 4060, AN ORDINANCE OF THE CITY OF EDMONDS, WASHINGTON, REQUIRING THE REPORTING OF LOST AND STOLEN FIREARMS. MOTION CARRIED UNANIMOUSLY.

2. CITY COUNCIL MEETING FORMAT AND POTENTIAL COMMITTEE STRUCTURE

Council President Mesaros explained the proposal is for three committees; the key issue is the format of the meetings. The proposal is for committees to meet on the 1st and 3rd Tuesdays from 7:15 – 8:15 p.m.,

Councilmember Fraley-Monillas referred to incentives for smaller units and lower rents for those units and asked how the lower cost could be guaranteed. She asked whether the incentive included putting a cap on the rent. Chair Nordling Rubenkonig relayed the development team indicated there are ways of reducing the cost of the construction. With regard to the Westgate project, it will be subsidized and 19% of the units will be at the affordable housing rate. Vice Chair Monroe recalled the team indicating it was market driven; a certain amount can be charged per square foot for apartments in the area, less square footage equals a lower price. Councilmember Fraley-Monillas concluded the rents would be market rate but lower due to the size.

Council President Mesaros asked what the developers of the Westgate characterized as incentives. Vice Chair Monroe said they suggested starting with a 2-story box as the base and then providing items such as public meeting areas, artwork or other features could qualify for additional stories.

Councilmember Nelson referred to the comment about long term investor and the importance of that distinction. This is the first time the developer has done a project outside of Seattle but they are referencing standard incentives. He wanted to be certain the developer's first project in Snohomish County had Edmonds appeal and not Seattle's. Vice Chair Monroe asked if Councilmember Nelson was interested in ensuing the project was not cookie cutter, commenting the City's Planning Department will work with the development team. At the meeting the Planning Board had, he felt they were very vested in the project and the area. Chair Nordling Rubenkonig said the development team praised City staff for working with them to create a project they are proud of. She noted Edmonds is urban and the Westgate project is similar to projects in Seattle.

Councilmember Teitzel said he reads the Planning Board minutes and is impressed with the work they do. He relayed the Council hears from the public at Council meetings, emails, phone calls, etc. and asked how the Planning Board engages with citizens. Chair Nordling Rubenkonig commented the tree ordinance generated significant public input; there was also a good turnout for the sign code. Planning Board meetings are televised. The Council setup is very formal; the Planning Board is more relaxed. She acknowledged the Planning Board did not have the amount of public participation the members would like. Councilmember Teitzel commented it would be great to see more people engaged at the Planning Board level before issues come to the City Council.

8. PUBLIC HEARING

1. PUBLIC HEARING ON HIGHWAY 99 AREA-: CG REZONE (MAP) & DEVELOPMENT REGULATIONS

Development Services Director Shane Hope introduced John Fregonese, Principal, Fregonese Associates, explained tonight's public hearing follows public open houses, news releases, meetings by the Planning Board and City Council and other outreach. He displayed a map of the project area, primarily the commercial zones around Highway 99 in Edmonds. The City Council last discussed Highway 99 Area Zone and Code Changes on June 6: Discussion included:

- Comparison with existing zone map and development code
- Review of proposed CG site and building design standards
- Question about additional incentives for "green buildings"
- Question about vehicle parking within buildings at street level

The focus of tonight's public hearing is the Planning Board's recommendation for Zoning Map and Development Code. No action is requested at this time.

He displayed the proposed Zoning Map and explained the proposal:

- Change these zones to the consolidated CG Zone
- Incorporate design standards directly into zones to ensure scale transition into neighborhoods
- More predictable outcomes for community

He displayed the Comprehensive Plan Map, advising the new zoning is consistent with the Comprehensive Plan map. He reviewed:

- Draft Zoning and Development Recommendations
 - Strengthen current design standards
 - Incorporate them directly into the zoning code
 - Consideration of special circumstances within the corridor will be made to ensure the standards are feasible, such as large parcels that would have multiple buildings if redeveloped and parcels with unique access or transportation challenges.
- Site development standards General 16.60.020
 - The pedestrian area adjacent to the street is composed of three zones: the streetscape zone, the pedestrian zone, and the streetscape zone (Section 16.60.020.C.2)
 - Changed “amenity zone” to “streetscape zone”
 - Added dimensions
 - Additional building setback when adjacent to RS zones
 - Upper stories setback 10’ for 25’ of building height; and 20’ for 55’ of building height
- Site development standards – design standards 16.60.030.A.1.E
 - 15’ setback with 10’ landscape buffer
- A minimum five feet wide type IV landscaping is required along all street frontages where parking lots about the street. Section 16.60.030.A.1
- Access and vehicle parking
 - All off-street surface parking shall be located to the side or rear of the primary building, except as otherwise specifically allowed by this chapter, and shall be screened from the sidewalk by a wall or plantings between 2 to 4 feet in height.
 - Outdoor parking areas shall comprise 40% or less of the public street frontage area within 100 feet of the primary street for the lot or tract and, on corner lots, may not be located at the corner. The requirements of this subsection do not apply to permitted auto sales uses.
- Required electric vehicle charging stations (Section 16.60.030.B.5)
 - One or more electric vehicle charging stations must be provided for all new development that includes housing.
- Bicycle storage spaces (Section 16.60.030.B.6)
 - Bicycle storage spaces for multifamily housing, excluding housing for assisted living or other specialized facilities, shall be provided for residents
 - 1 bicycle storage space for each residential unit under 700 square feet and 2 bicycle storage spaces for each residential unit greater than 700 square feet
- Paths within parking lots (Section 16.60.030.B.8)
 - Pedestrian walkways in parking lots shall be delineated by separate paved routes that meet federal accessibility requirements and that use a variation in textures and/or colors and may include landscape barriers and landscape islands
- Pedestrian and transit access (Section 16.60.030.B.11)
 - Where a transit station or bus stop is located in front of or adjacent to a parcel, pedestrian connections linking the station or stop directly to the development are required.
 - Pedestrians routes shall connect buildings on the same site to each other
- Site Development Standards – Site Design and Layout 16.60.030.C
 1. Pedestrian Oriented Design (Section 16.60.030.C.1)
 - At least 50% of a building’s façade facing the primary public street shall be located within 20 feet of the property line where the primary street frontage exists.

- Building must include a prominent pedestrian entry on the primary frontage
- Vehicle parking shall not be located within the first 20 feet of the primary street frontage, other than as allowed for vehicle sales use.
- 2. Alternative Walkable Design Area Option
 - For sites with unique constraints.
 - At least 50% of the building's façade facing the primary street shall be located within 60 feet of the front property line
- 3. Exceptions Process for Pedestrian and Walkable Design Options
 - Exemptions may be allowed by hearing examiner to provide for design flexibility that still encourages pedestrian orientation and efficient land uses under following criteria:
 - Property is located within 300 feet of highway interchange or has unique pedestrian access constraints
 - One or more buildings are located facing the primary street frontage
 - The development provides business and pedestrian areas near the primary street frontage and likely to be active through the day/evening.
 - At least 25% of required amenity space is located to connect building to the street
 - Where a site has multiple buildings, amenity space should be located between buildings to allow shared use
 - One or more buildings on the site must have at least two stories of usable space
 - Amenity space (Section 16.60.030.C.4)
 - An area equivalent to at least 5% of the building footprint shall be provided as amenity space.
 - If a vehicle parking area is being added to the site without the concurrent development of a building of at least 2,000 square feet, amenity space must be provided to equal at least 5% of the additional parking area.
 - Example of amenity space in Costa Mesa, California
 - Building Design and Massing (Section 16.60.030.D.2)
 - On the primary frontage, 50% of the building façade between two and 10 feet in height shall be comprised of windows or doors that are transparent

Mr. Fregonese reviewed examples of amenity space and building design and massing:

- New Seasons building on N Williams Street in Portland
- Safeway on Hawthorne Boulevard in Portland
- Safeway on Hwy 99/Barbur Boulevard in Portland
- Fred Meyer on Interstate Avenue,
- Auto dealers

Mr. Fregonese reviewed photographs illustrating the Health District Gateway and SW 234th today and with corresponding private investment. He reviewed the proposed changes to the sign code

- Revisions to 20.65.045:
 - Limit freestanding signs (such as monument signs) to maximum height of 14 feet in this district
 - Require freestanding signs to be counted as part of total maximum sign area for this district

Mr. Fregonese described next steps:

- Council's next meeting on this topic – July 18
- Council action scheduled – August 15

Councilmember Teitzel referred to the use of stepbacks to preclude a large wall facing a neighbor, providing a friendly transition. He asked if the use of the stepback would be restricted, for example would decks, tall trees, awnings, etc. be allowed, commenting allowing a 20-foot tree in the stepback would defeat the purpose of the stepback. Mr. Fregonese answered stepbacks are typically used for outdoor areas that would

include furniture, trees in pots, etc. The intent would be for the setbacks to be used as outdoor space, an amenity for the building. If desired, tree height could be limited to 10-12 feet. Councilmember Teitzel concluded there would not be a restriction on what was allowed in the setback. Mr. Fregonese agreed it is typically patio furniture and trees in pots.

Councilmember Fraley-Monillas asked if the amenity space was private or public. Mr. Fregonese answered it is privately owned intended to be open for public use as part of the pedestrian environment. Ms. Hope commented there is also opportunity for an internal plaza or courtyard for the tenants/residents/owners in addition to the required public space that is typically at the sidewalk level. Councilmember Fraley-Monillas recalled at least one project where the interpretation was it was not for the public's use but for the tenants' use. Ms. Hope anticipated it could be a mixture, some public space near the sidewalk as well as some private space for residents; the mix will depend on the use. Councilmember Fraley-Monillas concluded the plans for each project would specify whether the space was public or private use. Ms. Hope agreed.

Councilmember Tibbott said he liked the architectural features in the renderings but there is a minimal list of suggested building items. He asked whether more detail, perhaps pictures or illustration to help developers understand what the City is looking for with regard to architectural design standards. Mr. Fregonese answered there is typically a brochure or illustrations of how to implement the design standards. Compared to other codes implemented at the staff level, this code has both building transparency and building façade requirements. With creativity, that will lead to the type of buildings in the illustrations. Quality can never be guaranteed via the code but it can guarantee it won't be a white elephant. Ms. Hope said the draft code includes some illustrations which may be supplemented. Typically, commercial projects include professionals who are used to these design terms, but the intent is for the code to be understandable to the average person.

Councilmember Tibbott recalled the Westgate plan included an adaptation of form based code, recalling the intent was to lower the risk to a developer by illustrating what the City was looking for which saves time for the developer and for the City. Ms. Hope said this is a type of form based code as well, it addresses uses that are allowed/not allowed uses but focuses on the building shape, bulk, glazing, as well as the site design and building articulation.

Councilmember Tibbott recalled he expressed concern during the last presentation about the possibility of a 75-foot building across the street from single family residences. In areas of city where that would be allowed, it may not pencil out but it could happen. If a proposal like that were submitted to the City, he how it would be handled. Ms. Hope answered the code currently allows a 75-foot height next to single family housing in some instances with no requirement for setback or building setback. This code strengthens and respects the neighborhood more than the existing zoning.

Councilmember Tibbott expressed concern that the proposed zoning eliminates the transition areas. When driving around Shoreline and Edmonds today, he saw taller buildings on Hwy 99 with nearby residential areas. In one location, there was the interurban trail and vegetation between single family and the 75-foot tall building. He appreciated there would be landscaping between the curb and the building, but that does not break up the bulk of the building. He asked what would be done when a use adjacent to another use may not be compatible. Mr. Fregonese answered there would be 15-foot side and rear yards setback in addition to the setback. In the case of across the street, there is a 10-20-foot setback in front, the 60-foot right-of-way, and the 20-foot single family setback, providing approximately a 100-foot separation between the structures. Although setback could be required, a 75-foot tall building 100 feet away is beyond the ratio where it would be beneficial. Councilmember Tibbott commented parking would provide additional distance.

Councilmember Nelson thanked Mr. Fregonese for the requirement for electrical vehicle charging station and the description of amenity space. He referred to the sample photograph of the car dealership, remarking when he thought of amenity space, he was not thinking it would look like that. Mr. Fregonese recalled the Council's concern with retaining car dealerships. He reviewed the car dealership examples, and the intent to do something useful with the space in front that is not just parking. Councilmember Nelson suggested not using that photograph as an example.

Councilmember Nelson asked if the amenity spaces for multiple properties could be combined and collectively create a park. Ms. Hope answered that was possible. Mr. Fregonese said he lives next to a Fred Meyer whose amenity space ended up being a corner park.

Mayor Earling opened the public participation portion of the public hearing.

Dean Phillips, Council President, Edmonds Lutheran Church, referred to the church's letter expressing support for the Highway 99 Subarea Plan.

Ann Wermus, Edmonds recognized the work the City has accomplished in the plan for Highway 99. It promises to be a greatly revitalized area. She expressed support for the concept in the plan and stressed the need for continued work on providing permanent low income housing. Under the current plan, if a developer took advantage of the tax exemption option, a limited amount of low income housing could be supplied. In the GMA, the goal is to encourage the availability of affordable housing to all economic segments of the population and encourages the use of innovative techniques to meet the housing needs of all economic segments of the population and requires the city to provide opportunity for a range of housing types. The Highway 99 subarea plan offers a unique opportunity for housing that could be affordable for workforce housing as well as low income, potentially relieving the pressure on low income families. She urged the Council to develop and employ innovative strategies to promote all efforts as way to increase housing opportunity for all economic segments of the population.

Bea Wilson, Edmonds, representing herself and her son who owns property on 225th Place SW. She thanked the City Council and Planning Board for the proposed plan. She asked the definition of affordable housing. Mayor Earling suggested she speak with Ms. Hope. Ms. Wilson concluded the plan looks great.

Robert Sieu, Edmonds, property owner on Highway 99, said he spoke at the Planning Board May 10 public hearing regarding the exception on design standards. He also spoke with Ms. Hope and the consultant and both agreed some language needed to be change. The Planning Board approved the plan on the condition those changes would be made but at the June 6 Council meeting, the language had not been changed. The changes are related to exceptions for car dealerships and property 300 feet from an interchange.

Seth Hale, Seattle, also representing **Stan Piha, Edmonds**, expressed their support for the Planning Board's recommendation to approve the ordinance as proposed. The consultant has made the appropriate recommendations regarding setbacks, stepbacks and activity zones to ensure protection for properties adjacent to and across from CG zoned parcels within the corridor. Speaking as an architect, he commended the City on the flexibility in the code, providing a unique opportunity for great design in the corridor instead of code driven design.

Alvin Rutledge, Edmonds, asked about the affect the zoning changes would have on adult entertainment. He recalled a Snohomish County levy in 1995-1999 that did not pass.

Bruce Witenberg, Edmonds, said the plan works well on Highway 99 frontage where none of the parcels are across street or abut single family residences but more attention needs to be paid to the transition zone.

Single family residences in close proximity to the Highway 99 corridor represent some of the least expensive housing in the City. Among the purposes of the CG zone in 16.60.005, it recognizes the evolving identity and sense of place including distinctions between different parts of the district and to be sensitive to adjacent resident zones. He suggested using “adjacent and neighboring” instead of “adjacent” to clarify consideration of residents next to and across the street from CG zones. The table in 16.60.020.A is not sympathetic to adjacent and neighboring residential zones. The minimum street, side and rear setback of 10-15 feet may be more adequately sensitive to neighboring residential zones if each was increased by a minimum of 10 feet. With regard to 16.60.020.D regarding setbacks adjacent to RS zones, he suggested using “adjacent to and across the street from RS zones” instead of “adjacent.” He recommended surface parking lots adjacent to or across the street from residential zones be prohibited and that residential parking requirements in 16.60.030.1 be further reduced from a minimum of .75 cars for units 700 square feet or less and 1.75 cars for units above 750 square feet to encourage use of public transportation. Adopting these recommendations would more clearly demonstrate the Council’s commitment to being sensitive to neighboring RS zones while allowing for needed development.

Hearing no further comment, Mayor Earling closed the public hearing. He relayed staff is seeking Council input for consideration on July 18.

Councilmember Fraley-Monillas asked for clarification whether the zoning change would allow adult entertainment. Ms. Hope answered nothing had changed with regard to adult entertainment.

Councilmember Tibbott expressed interest in staff’s suggestions to address the issue he raised regarding multi-family directly across street from single family. Admittedly those streets were quieter and there would be parking on the street, yet the possibility existed for a 75-foot tall building across the street. He was open to considering options such as a parking buffer, reduced heights, setback, etc.

Councilmember Johnson commented TOD is very conducive to bicycles. She expressed interest in ways to accommodate bicycles in the Highway 99 corridor and consider the corridor as whole as a linear park as there are not a lot of parks in the area.

Councilmember Fraley-Monillas asked how to ensure people are not parking in the neighborhoods. Although the hope is fewer cars in a TOD, parking could be an issue if residents of the TOD parked in residential neighborhoods adjacent to the corridor. Ms. Hope said that is a legitimate issue and it is a balancing act. If too much parking is required, the result will not be TOD, yet enough parking needs to be required. The code tries to strike that balance between what is enough so there is some required parking but not so much that the TOD is lost or development of the site is so expensive that nothing happens. She described a development in Redmond where initially the city had fairly hefty parking standards and after the project was built, they found only about half the parking was being used because residents were using transit due to the location. If the need arose, parking programs could be considered.

9. ACTION ITEMS

1. PROPOSED NOISE ORDINANCE

Public Works Director Phil Williams explained this is in response to the current need which will continue so staff is recommending a change to the code. He reviewed:

- Current Noise Ordinance ECC 5.30
 - Written 1981 – last modified 1985 (32 years ago)
 - Sets standards for noise generation by limiting sound pressure levels at receiving property lines
 - Establishes two standards
 - Day time (7 am – 10 pm)
 - Night time (10 pm – 7 am)

afternoon of July 18 due to a blockage that caused a sewer backup. The center remained closed the next day and porta-potties were delivered for planned weekend events. The center has a combination of many old sewer lines, some cement, some cast iron and the blockage occurred where those join. The center was able to reopen with porta-potties and two operating restrooms. A City team arrived at 6 a.m. July 25, determined the problem by 9 a.m., were laying gravel by 2 p.m. and the system was again operational by the next morning, hopefully for an extended period or at least another year. He commended the effective, hardworking, good-natured City staff, particularly Facilities Manager Tom Sullivan, Facilities Maintenance Worker Patrick Cleveland, Sewer Lead Worker Tim Harris, Senior Sewer Maintenance Workers Jim Clemens, Don Crofton and Rick Wickers. He requested staff be recognized for their work.

5. APPROVAL OF CONSENT AGENDA ITEMS

COUNCILMEMBER BUCKSHNIS MOVED, SECONDED BY COUNCILMEMBER FRALEY-MONILLAS, TO APPROVE THE CONSENT AGENDA. MOTION CARRIED UNANIMOUSLY. The agenda items approved are as follows:

- 1. APPROVAL OF COUNCIL MEETING MINUTES OF JULY 25, 2017**
- 2. APPROVAL OF CLAIM AND PAYROLL CHECKS.**
- 3. ACKNOWLEDGE THE RECEIPT OF A CLAIM FOR DAMAGES FROM RICHARD MILLER (\$169.99).**
- 4. ORDINANCE TO CHANGE THE TIMING OF THE HEARING EXAMINER'S ANNUAL REPORT**
- 5. HISTORIC INFORMATIONAL PANELS FOR YOST PARK AND WATERFRONT MILLS**
- 6. SOCIAL WORKER INTERLOCAL AGREEMENT**

6. PUBLIC HEARING

1. PUBLIC HEARING ON HIGHWAY 99 PLANNED ACTION ORDINANCE

Development Services Director Shane Hope introduced Anna Snyder Kelly, 3 Square Blocks, who worked on the environmental impact statement (EIS) as well as the Planned Action Ordinance. Ms. Snyder Kelly explained a Planned Action Ordinance regulates the projects covered by the environment analysis done in an EIS, deemed to be “planned actions” under the Planned Action Ordinance. A Planned Action Ordinance also regulates what must be done to mitigate the environmental impacts. The Planned Action Ordinance does not just permit developments because they are within the EIS; it requires environmental impacts created by a project to be mitigated. The benefit of a Planned Action Ordinance is it speeds up the SEPA process for developers which encourages the kind of growth and economic development the City has stated it desires, creating benefit for all parties.

Ms. Snyder Kelly explained there are three basic steps in preparing a Planned Action Ordinance, 1) prepare an EIS that analyzes the environment impacts that various types of development might have on an area, 2) develop and adopt a Planned Action Ordinance, and 3) review individual development projects to determine if they are a planned action under the Planned Action Ordinance. She provided an analogy; if a development process is a trip on a plane, a Planned Action Ordinance is the equivalent of TSA precheck. A Planned Action Ordinance does not just let development occur because it happened to have been described in the EIS, but it speeds up the line and requires development to mitigate for the environmental impacts it has on

an area. More specifically, a Planned Action Ordinance is procedural in the sense that it does not change the underlying codes and standards; each development considered under a Planned Action Ordinance still must follow the underlying plans and codes. The City determines if a project is a planned action and can be considered under the Planned Action Ordinance.

Ms. Snyder Kelly explained the proposed Planned Action Ordinance outlines five thresholds used to determine if a project qualifies:

1. Land Use
 - Meets land uses allowed by the code and described in the EIS
2. Development threshold (for entire study area)
 - 3,325 dwelling units
 - 1,634,685 square feet of building area
3. Transportation
 - Not more than 2,755 new evening peak hour trips (for entire study area).
4. Elements of the Environment and Degree of Impacts
 - A proposed project that would result in a significant change in the type or degree of impacts to any of the elements of the environment analyzed in the Planned Action EIS would not qualify as a planned action
5. Changed Conditions
 - Should environmental conditions change significantly from those analyzed in the Planned Action EIS, the City's SEPA Responsible Official may determine that the Planned Action designation is no longer applicable until supplemental environmental review is conducted.

Council President Mesaros asked how the thresholds were determined. Ms. Snyder Kelly responded the transportation thresholds were calculated based on level of service (LOS) in the City's code and requirements for concurrence. The development thresholds are also related to concurrency. Council President Mesaros relayed his understanding there was no change to those. Ms. Snyder Kelly agreed.

Councilmember Buckshnis asked whether this was the first time the City has considered a Planned Action Ordinance. Ms. Hope answered yes, this is the first time the City has undertaken a Planned Action Ordinance and an EIS for a subarea. Councilmember Buckshnis asked why the City wanted to speed up the SEPA process, observing SEPA is extremely important. Ms. Hope clarified the real value is not that it makes the SEPA process a few days faster; the value is having an EIS that has assessed the impacts of development which helps assure the community that impacts of development have been considered and the Planned Action Ordinance memorializes that. A SEPA Checklist is still required for each project.

Councilmember Nelson asked how much time is saved. Ms. Hope answered for a developer it may be a few days or a couple weeks, it is not a huge amount of time unless a huge project is proposed which could potentially save a developer several weeks. Councilmember Nelson observed the time savings was also a cost savings. Ms. Hope agreed.

Councilmember Johnson asked whether the EIS had been published. Ms. Hope advised the draft EIS is available online and the final EIS will be posted tomorrow; there was little change between the two. Ms. Snyder Kelly advised the final EIS incorporates a number of public comments which were used to make a few tweaks such as incorporating things Community Transit is doing.

Councilmember Johnson referred to G5 in the Planned Action Ordinance related to development agreements which states the City or an applicant may request consideration and execution of a development agreement as allowed in Chapter 20.08 ECDC for a Planned Action project. She recalled the Council had a robust discussion regarding development agreements 3-5 years ago but the discussion was never concluded. That discussion included types of incentives and tradeoffs for development agreements. She

asked the status of the development agreement process. Ms. Hope answered development agreements are allowed under the existing code; if a project doesn't meet certain aspect of the code but meets the intent, the Council can consider a development agreement that binds the developer to certain conditions. A development agreement requires a decision by the City Council. A number of cities allow for development agreements and Ms. Hope acknowledged there are pros and cons to their use but there are occasions when they are useful. She felt it was worthwhile to again consider the chapter related to development agreements, but that was a different process; this ordinance simply references the existing code regarding development agreements and does not change anything.

Councilmember Johnson relayed her understanding the Council would work with a developer on case-by-case basis. Ms. Hope answered yes. She was not expecting requests for a development agreement and she has not heard anything that would likely lead to a development agreement; the ordinance simply leaves that option open.

Councilmember Johnson observed the Development Services Director or designee determines if an application is complete and that the decision of the SEPA Responsible Official regarding qualification as a planned action shall be final. She asked who is the SEPA Responsible Official Ms. Hope advised the Planning Manager is the SEPA Responsible Official, currently Rob Chave. Councilmember Johnson observed there was no way to challenge that decision. Ms. Hope answered that is typically considered an administrative decision. Councilmember Johnson observed the Planned Action Ordinance would be reviewed in five years. Ms. Hope agreed.

Councilmember Johnson observed the ordinance takes effect 5 days after passage and asked whether the Planned Action Ordinance could be adopted prior to the Comprehensive Plan when it was related to the Hwy 99 subarea plan. Ms. Hope answered subarea plans can be adopted outside the normal Comprehensive Plan amendment process. Once the Council approves the Hwy 99 subarea plan, which is considered part of the Comprehensive Plan, the Planned Action Ordinance could go forward.

Councilmember Tibbott relayed his understanding that development could go forward unless there were significant impacts and asked for examples of impacts that would disallow development. Ms. Hope answered an obvious example would be the traffic proposed at an intersection would cause the intersection's LOS to fail; specific mitigation and additional study would be required. Councilmember Tibbott asked for other examples. Ms. Hope answered traffic would be the mostly likely as there are no wetlands in this area. Ms. Snyder Kelly pointed out if a project does not meet the qualifications to be considered a planned action, it automatically reverts to the original SEPA process.

Councilmember Tibbott asked for an example of changed conditions. Ms. Hope answered the most likely would be if traffic patterns did not occur in the way the modeling indicates. Ms. Snyder Kelly answered another example would be an unexpected increase in population and the utilities no longer support the development that would allowed, the environmental impact of the project would need to be considered. Councilmember Tibbott relayed his understanding there were conditions that could require further study or deeper analysis. He asked who decides when the impact or changed conditions required further analysis. Ms. Hope answered that would typically be the SEPA official or it could be raised by others. Councilmember Tibbott concluded the involvement of the SEPA Official provides additional objectivity.

Councilmember Buckshnis asked why the public hearing was being held tonight when the Council has not seen the EIS which should have been included in the packet and it had not been reviewed by the Planning Board. Ms. Hope clarified it did go through the Planning Board; the Planning Board held at least two meetings including a public hearing and recommended adoption. The Planning Board minutes were sent to Council today.

Councilmember Buckshnis echoed Councilmember Johnson's concern that the Council never completed its discussion about development agreements and according to the 20.08.050, the attorney does everything and the Council is not involved. Ms. Hope assured a development agreement requires Council approval. Mr. Taraday pointed out 20.08.040 states a development agreement is a Type V development project permit application which is a Council decision.

Councilmember Buckshnis relayed her support for Hwy 99 but she had not received all the documents necessary to fully understand. Ms. Hope said the development agreement reference is not essential to this Planned Action Ordinance; it simply references the existing chapter. If the Council chose not to include that provision, that would be okay.

Mayor Earling opened the public participation of the public hearing.

Seth Hale, Seattle, offered suggestions to enhance the goals of the proposed code language with regard to required parking and alternative transportation modes for transit oriented development (TOD) within ¼ mile of BRT stations. Characteristics of TODs include multiple transportation choices and reduced parking. Studies have shown that increased transportation choices and reduced parking benefit the environment and reduce auto congestion. Incentivizing development to provide additional transportation choices in exchange for reduced parking only increases these benefits. Examples of incentives include charging stations. Incentivizing charging stations within TODs would provide a higher number of charging stations or capacity. For example, for every 4 additional charging stations or capacity, the number of standard vehicle stalls could be reduced by 1 stall, up to a maximum of 5% of required parking. This is a good trade with regard to cost, each charging station costs \$4,000 to \$6,000; structured parking stalls range from \$10,000 to \$30,000. Reducing one parking stall offsets the cost of an EV station. Under the proposed code, a charging station shall be installed to serve at least 10% of the required residential parking stalls. While Washington is a leader in the number of electrical vehicles they account for less than 1% of the registered vehicles.

Mr. Hale relayed concern that as technology advances, charging stations will also likely advance and today's equipment may become obsolete. He suggested an initial lower percentage of functioning charging stations with a potential for increasing or exceeding capacity. Another example is car sharing; car sharing users tend to be millennials and there are significantly more renters under 30 than homeowners. He has seen car sharing used successfully in Seattle where car sharing spaces reduced the required parking by 3 stalls to a maximum of 15% of the required parking stalls. He acknowledged the cost to provide additional transportation modes such as bicycle parking charging stations, car sharing, subsidized transit passes, etc. adds to the overall project costs. A reduction in required parking stalls helps offset these costs and ensures TODs can compete with typical development to provide affordable housing with alternative transportation modes close to BRT stations.

Alvin Rutledge, Edmonds, referred to casinos in neighboring cities and adult entertainment in Shoreline that are used by Edmonds residents. As more businesses locate on Highway 99, the traffic increases. He questioned the existing zoning and the proposed zoning, recalling the reason development has not occurred in Edmonds is the cost. He provided a Seattle Times article from 1987 regarding the Council considering a topless-bar law.

Mark Tekin and Dominic Rambes, Tekin & Associates, owners of the Denny's property on Highway 99, explained a year ago they got the space back as it was no longer financial feasible for Denny's. They have proposed a project with 4 tenants, 2 restaurants tenants and 2 retail tenants that would provide daytime and evening services to the community. They submitted a land use application which was approved on July 14, 2017. Five days after approval, they were informed by City staff it was no longer valid due to the new development guidelines under consideration. He relayed they spent a lot of time and energy on the

redevelopment project and although they had interest from fast food, they have proposed a nice, modern redevelopment with no drive throughs.

Eric Nielsen, Arca Architecture, representing the project on 212th St SW & 72nd Ave W, referred to the proposed increase in the building setback from 4 feet to 10 feet, relaying a preference for more flexibility in the code language to allow for site specific solutions as a blanket 10-foot setback may not provide the desired solution on every site. He referred to images he provided the Council, first an example of a 4-foot setback or other minimal dimension that allows the developer opportunity to setback the building higher up, essentially transition the building to the street level. He referred to sections in the images, an example of an elevation with a 10-foot setback and a vertical building along the sidewalk. The second elevation provides a more pedestrian scale on the sidewalk with setback above and opportunity for landscaping above the street. He recommended the Council consider more flexible in the code with two options, 1) the 10 foot setback and 2) reduced setbacks as long as the building is setback above by 10 feet. With regard to the proposal to not allow parking within 20 feet of the building wall, he pointed out this pressures building owners to provide commercial and residential uses on the ground floor; however, not all sites are suitable for residential or commercial uses, potentially resulting in vacant space on the ground floor.

Hearing no further comment, Mayor Earling closed the public participation portion of the public hearing.

Councilmember Fraley-Monillas asked Ms. Hope to comment on the Denny's site. Ms. Hope said she would need to research the particulars of the site. It was her understanding if a building permit was approved, it would not be invalidated based on a code that was not yet adopted. City Attorney Jeff Taraday relayed he was contacted by their attorney; the issue is they have approved design review but not an approved building permit so the project is not yet vested. The attorney's request was for the Council to consider a delayed effective date that would allow the applicant to submit a building permit application before the ordinance is effective.

Councilmember Fraley-Monillas asked how the new code would prohibit the owners of the Denny's site from developing. Ms. Hope answered it was related to the specifics of development, not whether they could develop. For example, the setbacks may be different; basically, the design standards would change for the site and the building. Councilmember Fraley-Monillas suggested if they were interested in developing with their original plan, they should apply for a permit sooner rather than later. Ms. Hope agreed, advising the other option, if City Council wanted to permit this project, specific language could be included in the code or the effective date could be changed. If a building permit is submitted as a complete application prior to a new code taking effect, the project is vested. Councilmember Fraley-Monillas observed that corner has had its challenges and it would be nice to see it redeveloped.

Councilmember Johnson asked about the effective date of new ordinance. Ms. Hope said their concern was not related to the Planned Action Ordinance but rather the underlying development regulations. If the development regulations were adopted on August 15, they are effective 5 days after publication. If the Council chose to forward approval to the Consent Agenda, there would be a slight delay. Another option would be for her to research and provide more information at the Council's next meeting and determine if the Council wants to craft language related to that project given that a lot of work has been done it.

7. STUDY ITEMS

1. REVIEW OF PROPOSED HIGHWAY 99 SUBAREA PLAN AND DEVELOPMENT REGULATIONS

Development Services Director Shane Hope recalled the Council last reviewed Highway 99 Area zone and code changes on July 18. At that time discussion included comparison with existing zone map and development code, review of proposed CG site & building design standards and specific questions and

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comments. The focus of tonight's meeting is next steps for draft plan, zone map and development code. She sought Council direction to proceed with draft ordinances for the August 15 Council meeting. She displayed an aerial view of the project area (Highway 99 corridor and surrounding area) and reviewed:

- Planning process and timeline
 - Understanding existing conditions: March – April 2016
 - Develop land use and transportation scenarios: April – June 2016
 - SEPA & Planned Action Environmental Impact Assessment: April – November 2017
 - Develop subarea Draft Plan: October – December 2016
 - Final Subarea Plan: January – June 2017
- Community Values
 - Connectivity
 - Designations
 - Beautification
 - Safety
 - Walkability
 - Affordable housing
 - Healthy businesses
- Distinct Subdistricts
 - Major local and regional destinations on Hwy 99
 - International District
 - Diverse restaurants, grocers and shops; major Korean business cluster
 - Health District
 - Swedish Hospital and medical offices
 - Gateway District
 - Identified by the community during workshop
 - Desire for “gateway” and distinct transition point in and out of Edmonds
- Subarea Plan: implementation strategies, policy recommendations and actions
 - Zoning and development recommendations
 - Affordable housing recommendations
 - Signage and wayfinding recommendations
 - Transit recommendations
 - Transportation infrastructure recommendations
 - Map: Planned Transportation Improvements
- Current Zoning Map
 - The only difference between CG and CG2 is the height limit (CG = 60' and CG2 = 75')
 - Many current zones are remnants from the county's antiquated zoning
 - Some zones do not match with the parcel boundaries
- Proposed Zoning Map
 - The proposal is to change these zones to the consolidated CG zone
 - Incorporate design standards that will increase vitality and ensure transition into neighborhoods
 - More predictable outcomes for community
- Zoning Map & Development Regulations – Goals
 - Vitality and livability
 - Sustainability
 - Consistency with subarea plan
 - Reasonable balance of requirements and options
- Site development standards – General 16.60.020
 - Pedestrian area - required adjacent to street
 - Composed of three zones: streetscape zone, pedestrian zone and activity zone
 - Comparison of pedestrian area requirements
 - Required in existing CG regulations? – no

- Required in proposed CG regulations? – yes
- Comparison of dimensional requirements

	Existing CG Chapter	Proposed CG Chapter
Height	60-75'	75' max*
Street setback	4' min.	5'/10'
Side/rear setback	0/15	0/15
Stepback	No additional stepback required for upper stories	Additional stepback required for upper stories adjacent to single family
- Additional building stepback when adjacent to RS zones
 - Upper Stories stepback 10' for 25' of building height; and 20' for 55' of building height
- Site development standards – design standards 16.60.030.A.1.E
 - Illustration of 15' setback with 10' landscape buffer
 - Additional building stepback with adjacent to RS zones
 - Illustration of upper stories stepback 10' for 25' of building height; and 20' for 55' of building height
 - Photo example of across-the-street transition illustrating streetscape zone separation provided by street and
 - Minimum 5 feet wide type IV landscaping is required along all street frontages where parking lots abut the street
 - Access and vehicle parking
 - All off-street surface parking shall be located to the side or rear of the primary building, except as otherwise specifically allowed by this chapter, and shall be screened from sidewalk by a wall or plantings between 2 to 4 feet in height
 - Outdoor parking areas shall comprise 40% or less of the public street frontage area within 100 feet of the primary street for the lot or tract and, on corner lots, may not be located at the corner. Requirements of this subsection do not apply to permitted auto sales uses
 - High parking requirements can impede development
 - Proposed regulations aimed to encourage transit-oriented development options
 - 0.75 parking spaces per residential unit less than 700 sq. ft.
 - 1.25 parking spaces per residential unit of 700-1100 sq. ft.
 - 1.75 parking spaces per residential units greater than 1100 sq. ft.
 - Guest parking: 1 space/per 20 units
 - Required electric vehicle charging stations
 - One or more electric vehicle charging stations must be provided for all new development that includes housing (to serve 10% of required parking spaces)
 - Each electric vehicle charging station typically serves two vehicles
 - 1 station/10 required residential stalls plus planned capacity to double that amount in the future
 - Required bicycle storage spaces
 - Bicycle storage spaces for multifamily housing, excluding housing for assisted living or other specialized facilities, shall be provided for residents
 - 1 bicycle storage space for each residential unit under 700 square feet and 2 bicycle storage spaces for each residential unit greater than 700 square feet
 - With 50% reduction for bikes stored in common spaces if additional racks are available for guests
 - Paths within parking lots
 - Pedestrian walkways in parking lots shall be delineated by separate paved routes that meet federal accessibility requirements and that use a variation in textures and/or colors and may include landscape barriers and landscape islands
 - Pedestrian and transit access

- Where a transit station or bus stop is located in front of or adjacent to a parcel, pedestrian connections linking the station or stop directly to the development are required.
 - Pedestrians routes shall connect buildings on the same site to each other
- Site Development Standards – Site Design and Layout 16.60.030.C
 - 1) Pedestrian Oriented Design
 - At least 50% of a building’s façade facing the primary public street shall be located within 20 feet of the property line where the primary street frontage exists.
 - Building must include a prominent pedestrian entry on the primary frontage
 - Vehicle parking shall not be located within the first 20 feet of the primary street frontage, other than as allowed for vehicle sales use.
 - 2) Alternative walkable design area option
 - For sites with unique constraints.
 - At least 50% of the building’s façade facing the primary street shall be located within 60 feet of the front property line
 - 3) Exceptions process for pedestrian and walkable design options
 - Exemptions may be allowed by hearing examiner to provide for design flexibility that still encourages pedestrian orientation and efficient land uses under following criteria:
 - Property is located within 300 feet of highway interchange or has unique pedestrian access constraints
 - One or more buildings are located facing the primary street frontage
 - The development provides business and pedestrian areas near the primary street frontage and likely to be active through the day/evening.
 - Not more than 50% of required parking within first 20 feet of property
 - At least 25% of required amenity space is located to connect building to the street
 - Where a site has multiple buildings, amenity space should be located between buildings to allow shared use
 - One or more buildings on the site must have at least two stories of usable space
 - Amenity Space
 - An area equivalent to at least 5% of the building footprint shall be provided as amenity space.
 - If a vehicle parking area is being added to the site without concurrent development of a building of at least 2,000 square feet, amenity space must be provided to equal at least 5% of additional parking area.
 - Building Design and Massing
 - On the primary frontage, 50% of the building façade between 2 and 10 feet in height shall be comprised of windows or doors that are transparent
 - New standard for vehicle parking within buildings
 - On primary frontage, no vehicle parking within first 20 feet of building facing street
- How do proposed regulations encourage sustainable development?
 - Examples:
 - Bicycle storage
 - Electric vehicle charging
 - Wider pedestrian areas
 - Not excessive vehicle parking (transit friendly)
 - Amenity space
 - Stormwater mgmt.
 - Compact development
 - Landscaping and street trees
 - Concept illustration of possible redevelopment
- Proposed change to sign code
 - Revision to 20.65.045

- Limit freestanding signs (such as monument signs) to maximum height of 14 feet in this district
- Require freestanding signs to be counted as part of total maximum sign area for this district

Ms. Hope relayed tonight was an opportunity for Council review and direction to proceed with draft ordinances and final presentation and adoption of the ordinances on August 15.

Councilmember Buckshnis recalled public comment tonight about the code, not the Planned Action Ordinance. She asked if the public hearings had been completed. Ms. Hope explained several public meetings and hearings have been held and there were no plans for more. There will be another presentation on August 15 and opportunity for public comment during Audience Comments. Councilmember Buckshnis asked whether the Planned Action Ordinance and the code had to be approved at the same time. Ms. Hope answered no, it would be ideal to approve the subarea plan and the code amendments at the same time. The Planned Action Ordinance can be adopted at the same time or a little later. Councilmember Buckshnis relayed her interest in reviewing the Planning Board minutes and the EIS.

Councilmember Buckshnis recalled the Council also approved a resolution of intent to designate the Hwy 99 subarea as a Residential Targeted Area on July 18 and asked how that fit with these other regulations. Ms. Hope answered one of the recommendations in the subarea plan was to allow the Multifamily Tax Exemption (MFTE) in this area. The Council approved a resolution of intent to designate the Highway 99 Subarea as Residential Targeted Area for implementation of the MFTE program and will hold a public hearing on August 15.

Councilmember Teitzel commended staff and the consultant on the Hwy 99 subarea plan, an excellent example of involving the public. He referred to Esperance on the west side of Hwy 99 which comprises approximately 10% of the Hwy 99 frontage and asked if Hwy 99 is rezoned to CG, will the zoning of Esperance be inconsistent if the City annexes it in the future. He asked if it would behoove the City to work with Snohomish County now to modify the zoning to be more consistent with the intent for that area. Ms. Hope answered staff has involved Snohomish County in the development of the plan and they have been very supportive. It may be some time before Esperance is annexed by the City but staff will continue working with Snohomish County. Councilmember Teitzel asked if Snohomish County was interested in CG-type zoning in that area. Ms. Hope said Snohomish County has had no objections but they would have to take action. Snohomish County has many unincorporated areas and have not looked at them individually; they expect cities to annex them and then it becomes a city issue.

Councilmember Johnson referred to the new development on 196th in Lynnwood near the convention center, relaying her objection to the six-story flat wall. She hoped something more architecturally interesting and less oppressive for pedestrians would be built on Hwy 99. Ms. Hope said she would consider that and address it at the next meeting.

Councilmember Johnson asked if there was a roadway or profile design beyond the pedestrian area. Ms. Hope said a detailed corridor design is planned using State funds. The \$1 million allocated by the State has been delayed due to delays at the legislature in approving the capital budget. Councilmember Johnson said in her conversations with WSDOT, bicycles are not excluded from Hwy 99 or SR-104. She expressed interested in not necessarily a bicycle lane but an opportunity to protect bicycle riders. She recalled this was an issue of right-of-way width on SR-104 but Hwy 99 is wider. Ms. Hope answered Hwy 99 is 100 feet wide. Councilmember Johnson suggested future planning include how to accommodate bicycles. She relayed her continued concern that the bicycle route on 212th did not extend to the Interurban Trail.

Councilmember Johnson suggested another issue she wants addressed in the Hwy 99 subarea plan is whether an urban interchange is needed for SR-104/Hwy 99 which is currently incomplete for all

movements. As this will be expensive, she suggested it be included in the plan so it can get on a list. That interchange would improve access to adjacent properties and improve movement on SR-104, a highway of statewide significance. She appreciated that the plan did not allow vehicle parking on the first level but pointed out the Everett Clinic has parking on the first and second levels that is visible from the street. She suggested consideration be given to how to better screen or eliminate visibility of parking. Ms. Hope agreed vehicle parking could occur on the second level, not just the first level, and the proposed code does not address that.

Councilmember Johnson suggested consideration be given to expanding Park & Rides within the corridor. There are three Swift transit stations and it is assumed people will walk ¼ mile. She uses the Swift line but lives more than a ¼ mile away and typically parks in a Park & Ride lot. There is no parking for the Swift line, yet if Edmonds residents utilize it, they may need parking. Ms. Hope advised that was not in the current scope; Community Transit's policy is to not build Park & Ride lots on that valuable land. Councilmember Johnson suggested consideration be given to increasing size of existing Park & Ride lots or other options. To encourage people to ride the bus, the amount of parking is reduced; however, there is not a lot of on-street parking in the surrounding area. For example, she currently parks at Safeway but that extra parking may not exist in the future.

Councilmember Tibbott recalled Ms. Hope stating two electrical vehicles could be charged in one parking space. Ms. Hope clarified each charging station has two outlets. Councilmember Tibbott asked about the requirement to provide future capacity for charging stations. Ms. Hope explained the wiring would be installed but not the stations themselves. Councilmember Tibbott asked about the percentage. Ms. Hope answered the requirement for future capacity is doubling the currently requirement. For example, if five stations are required, the developer would have to rough-in an additional five.

Councilmember Tibbott was intrigued by parking for vehicle sharing or drop off zones. He asked what capacity could be added or provided in a residential development. Ms. Hope said that is identified in the code as one of the ways to offset some of the parking requirements; the developer would be required to demonstrate how that would be provided and the amount of parking it would offset. Councilmember Tibbott recognized not all future transportation forms could be anticipated and he was interested in providing some flexibility.

Councilmember Fraley-Monillas observed Hwy 99 has no bike lanes, not in Seattle, Shoreline or Lynnwood, likely because it was deemed to be unsafe. Ms. Hope said given that Hwy 99 is a highway as well as the amount of traffic, alternative parallel routes have been provided. Councilmember Fraley-Monillas observed there are often bikes on 76th Avenue and on the Interurban Trail.

Councilmember Fraley-Monillas referred to Councilmember Johnson's comment about Park & Ride lots, pointing out the purpose of TOD is to increase the number of people using transit but they do not need a Park & Ride lot. Ms. Hope agreed residents in new development on Hwy 99 could walk to transit and they have parking on site. People who do not live in the Hwy 99 area who want to use transit may want a Park & Ride lot; however, that is not related to the Hwy 99 plan. The Swift line is the highest performing line Community Transit has, even without a Park & Ride lot. Councilmember Fraley-Monillas commented there is only one Park & Ride lot on the corridor in Edmonds. She questioned where Swift riders park now. Ms. Hope said she could check with Community Transit.

COUNCILMEMBER FRALEY-MONILLAS MOVED, SECONDED BY COUNCIL PRESIDENT MESAROS, TO DIRECT STAFF TO PREPARE THE PROPOSED HIGHWAY 99 SUBAREA PLAN AND DEVELOPMENT REGULATIONS IN ORDINANCE FORMAT FOR COUNCIL CONSIDERATION AND ADOPTION ON AUGUST 15. MOTION CARRIED UNANIMOUSLY.

Mayor Earling declared a brief recess.

2. DISCUSSION ON DEVELOPING A PROCESS FOR ACTING ON BOARD AND COMMISSION RECOMMENDATIONS TO CITY COUNCIL

Council President Mesaros explained with the new committee system, a question arose regarding whether committees needed to review recommendations from boards and commissions. He asked for the Council's input.

Councilmember Buckshnis said boards and commissions did not report to committees in the past unless it was an issue like the Harbor Square master planning process. She acknowledged it would depend, but annual/quarterly reports never went through committee before. Items like the recent climate change resolution should go through committee. Council President Mesaros clarified that was not from a board or commission. Councilmember Buckshnis concluded there may be special circumstances but she preferred to have boards and commissions report to the full Council.

Council President Mesaros agreed boards and commissions should report directly to the full Council. He recalled the code states the Planning Board reports to the City Council and he anticipated other boards/committees would prefer to report directly to the full Council.

Councilmember Johnson commented typically nothing from planning goes through the Parks, Planning & Public Works Committee but seems to go to the full Council. Council President Mesaros recalled Ms. Hope presented the change in the timing of the Hearing Examiner report to the Parks, Planning & Public Works Committee at their last meeting. Ms. Hope said her understanding and in reviewing past minutes and talking with staff who were here when the committees were in place previously, the Planning Board did not report to committees on items such as code amendments, those went to the entire Council. Nothing the Planning Board recommends to Council is a potential Consent Agenda item; Public Works projects can potentially be approved on the Consent Agenda.

Councilmember Nelson agreed in general, it makes sense for boards and commissions to present to the full Council. He did not see any benefit to having recommendations presented to committee before presenting to the full Council.

Councilmember Tibbott agreed items the Planning Board reviews and forwards should be presented to the full Council. However, there may be topics other boards and commissions are working on at the request of Council and they may want input from a committee before making a recommendation. Ms. Hope said if a board/commission wanted committee input, they could certainly seek it; the issue is whether the Council wants to require boards/commissions report to a committee first. In her experience, reports from the Economic Development Commission or Historic Preservation Commission typically go to full Council as any action is taken by the Council. Council President Mesaros agreed if a board/commission wanted input from a committee they could seek that but it would not be required.

Councilmember Teitzel agreed an item that has been fully vetted by the Planning Board could/should come directly to full Council. However, two weeks ago a majority of Council opted to send the historic panels back to the Parks, Planning & Public Works Committee for review before approving it on the Consent Agenda tonight. That seems to be inconsistent with the direction Councilmembers are providing tonight. Council President Mesaros said that was what sparked this discussion. Council could still refer an item to committee for further discussion which was what happened with the historic panels.

Councilmember Buckshnis commented the resolution regarding climate change was referred from the Mayor's Climate Protection Committee. She recalled a variety of issues reviewed by the Planning Board went to the Parks, Planning and Public Works Committee in the past. She asked if the intent was to codify this issue or establish a policy. Council President Mesaros anticipated a policy but he was looking for

Council input on the process. Councilmember Buckshnis commented the committees can be used to expedite things and ask questions of committee members who are assigned to committees based on their level of expertise.

Councilmember Fraley-Monillas recalled boards and commissions products never went to committees in the past, they always went to full Council. She commented this was much ado about nothing; the Council can refer an issue wherever they wish.

Councilmember Johnson said if the Council has a committee system, it is not a bad policy to send things to committees such as the climate protection resolution or historic panels. Council President Mesaros summarized the direction from Council seemed to be that that would be an option but on the whole, boards and commissions would report directly to Council. Councilmember Johnson suggested if an individual has an idea he/she wants to bring forward, it could be channeled through the committee process. Council President Mesaros commented that was a different topic.

Councilmember Fraley-Monillas commented it was almost impossible to determine what goes to committee, it depends on who is the Council President, etc.

Council President Mesaros asked City Attorney Jeff Taraday whether the Council could establish policy rather than codify it. Mr. Taraday answered many of the Council processes that are codified do not have to be. It is simply a matter of whether the Council wants everything codified so there is more visibility, etc. Council President Mesaros offered to confer with Mr. Taraday and determine if this needed to be codified so the policy can be found in the future.

8. COUNCIL COMMITTEE REPORTS

1. COUNCIL COMMITTEE REPORTS

Finance Committee

Councilmember Buckshnis reported agenda topics included:

1. 2018 Budget Internal Procedures – information only
2. Paid Internship from UW via federal grant – approved on Consent Agenda
3. Work Plan for Developing Policies – staff will provide matrix of policies

Public Safety & Personnel Committee

Councilmember Nelson reported agenda topics included:

1. FD1 Fire Prevention Group introductions – information only
2. Social Worker Interlocal Agreement 67%/33% partnership between Lynnwood and Edmonds – approved on Consent Agenda

Parks, Planning & Public Works Committee

Councilmember Johnson reported agenda topics included:

- Ordinance to Change the Timing of the Hearing Examiner's Annual Report – approved on Consent Agenda
- Pine Street Lighting Discussion – further discussion at PPP Committee's August 8 meeting
- Historic Informational Panels for Yost Park and Waterfront Mills – approved on Consent Agenda

9. MAYOR'S COMMENTS

Mayor Earling reported tomorrow he will announce and issue a press release regarding the membership of a Housing Strategy Task Force, led by Shane Hope. There are many people interested in housing strategies and many different ideas.

Mayor Earling reminded the public to vote by tomorrow, August 1.

10. COUNCIL COMMENTS

Councilmember Fraley-Monillas invited the Council and the public to a ribbon cutting tomorrow at the Edmonds Police Department at 1:30 p.m. for the drug takeback boxes that have been installed to deposit old medications, particular narcotics. She thanked Edmonds Family Medicine on 73rd and 212th and the Edmond QFC pharmacy for also installing boxes in their pharmacies. She encouraged other pharmacies in Edmonds to install drug takeback boxes.

Councilmember Johnson provided Councilmembers and the Mayor copies of a Homelessness & Housing Tool Kit for Cities developed by MSRC and AWC. She also has copies for the members of the new Housing Strategy Task Force.

Councilmember Johnson was glad to learn there would be medical takeback boxes in Edmonds; she has used the box in the Lynwood Walgreens and it was full last time. She was glad people were turning in their drugs and that there were three more locations in Edmonds.

Councilmember Buckshnis reminded the public to be aware of their pets during the upcoming heat wave and to check on seniors.

11. CONVENE IN EXECUTIVE SESSION REGARDING PENDING OR POTENTIAL LITIGATION PER RCW 42.30.110(1)(i)

At 8:55 p.m., Mayor Earling announced that the City Council would meet in executive session regarding pending litigation per RCW 42.30.110(1)(i). He stated that the executive session was scheduled to last approximately 15 minutes and would be held in the Jury Meeting Room, located in the Public Safety Complex. Action was anticipated to occur as a result of meeting in executive session. Elected officials present at the executive session were: Mayor Earling, and Councilmembers Johnson, Fraley-Monillas, Buckshnis, Teitzel, Tibbott, Mesaros and Nelson. Others present were City Attorney Jeff Taraday and Parks & Recreation Director Carrie Hite. The executive session concluded at 9:15 p.m.

12. RECONVENE IN OPEN SESSION. POTENTIAL ACTION AS A RESULT OF MEETING IN EXECUTIVE SESSION

Mayor Earling reconvened the regular City Council meeting at 9:15 p.m.

COUNCIL PRESIDENT MESAROS MOVED, SECONDED BY COUNCILMEMBER TIBBOTT, THAT THE CITY COUNCIL AUTHORIZE THE CITY ATTORNEY TO INITIATE A DECLARATORY JUDGMENT ACTION IN SNOHOMISH COUNTY SUPERIOR COURT TO RESOLVE THE DISPUTE OVER THE SCOPE OF THE PUBLIC ACCESS EASEMENT THAT THE CITY OWNS IN FRONT OF THE EBBTIDE CONDOMINIUMS. MOTION CARRIED UNANIMOUSLY.

13. ADJOURN

With no further business, the Council meeting was adjourned at 9:15 p.m.

Mr. Phipps noted the lack of trees in the downtown corridors. While there are nice trees on 5th Avenue and Main Street, there are very few trees on the other downtown streets. When he thinks of a beautiful downtown, he thinks of tree-lined streets. He suggested that a tree-planting program should be part of the UFMP, as well. Adding to the tree canopy would make the downtown area much more pleasant. He concluded his remarks by reminding the Board that once the open spaces are gone, they are gone for good. He encouraged them to be known for protecting the land. Tree canopy is extremely important when fighting climate change and global warming. He encouraged the Board to recommend approval of a strong UFMP and a strong tree ordinance.

DEVELOPMENT SERVICES DIRECTOR REPORT TO PLANNING BOARD

Chair Rubenkonig referred the Board to the written report that was provided by the Development Services Director. Board Member Lovell asked if there was any new information relative to the State Legislature's efforts relative to the capital budget. Ms. Hope reported that, unfortunately, the Legislature dismissed without an agreement on the capital budget, and Governor Inslee has indicated he would not reconvene the Legislature unless it is known that they will come up with a capital budget. This is very concerning to many, including the City of Edmonds, because they have some projects expected to be on the capital budget.

Board Member Lovell referred to the update from Snohomish County Tomorrow (SCT) relative to the Alliance for Housing Affordability, and asked if they are getting close to issuing a report to the City Council. He reminded the Board that the Five Corners Subarea Plan is on hold until the study has been completed. Ms. Hope said the Board's extended agenda includes meetings that will cover housing and the Five Corners Subarea Plan. In preparation for these discussions, staff is working with the Alliance for Housing Affordability to update the City's housing profile information. Staff will provide a report on this effort to the Planning Board on September 13th. Board Member Lovell asked if the effort would result in the City Council considering some type of tax incentive relative to low-income housing development on Highway 99. Ms. Hope answered that is a separate discussion, and the City Council recently approved having a public hearing on adding the Highway 99 Subarea into the City's program for multi-family tax deductions. Theoretically, they could take action sometime in August.

Chair Rubenkonig requested an explanation for how the Office of Financial Management (OFM) determined the City's populations projections. Ms. Hope explained that the OFM does population projections every year. They look at a number of factors, including the population at the last determination and any new community data that has been gathered. They also consider the number of mailboxes, building permits, etc. She cautioned that the population projection is an estimate based on an April 1st point in time.



PUBLIC HEARING ON PROPOSED PLANNED ACTION CODE LANGUAGE FOR HIGHWAY 99 SUBAREA PLAN

Ms. Hope advised that the Planned Action Ordinance (PAO) before the Board relates to the Highway 99 Subarea Plan. She provided a map of the subarea, which has been subject to study since the beginning of 2016. The intent was to create a plan that had vision and included action items that could be implemented. It recommended a set of Development Code amendments to better implement the Highway 99 Subarea Plan's vision. She cautioned that the PAO before the Board would not change any of their previous work. She explained that the Growth Management Act (GMA) and the State Environmental Policy Act (SEPA) allow for a "planned action" process to streamline the environmental review process for projects that have already been covered by an Environmental Impact Statement (EIS) developed for the subarea. This is, in part, because the EIS will have analyzed impacts of development and identified mitigation measures to address the impacts. To memorialize the EIS work for the subarea, the City may adopt a PAO or code that specifies the type and amount of development that may be allowed to proceed in the subarea without additional SEPA review. All other applicable laws and regulations would still apply. If a project would exceed the thresholds identified in the PAO, additional environmental work would be required to make sure all impacts are considered.

Ms. Hope referred the Board to the draft EIS that was prepared for the PAO and noted that the final EIS will be issued next week. She advised that the draft PAO assumes that the final EIS will be approved, and the final EIS will be done before the Board's recommendation is presented to the City Council. The City Council will have a work session and public hearing before the PAO is approved in its final form.

Board Member Lovell clarified that the Board received two documents: a draft PAO ordinance and a PAO mitigation document. Ms. Hope explained that the mitigation document is a supplement to the draft PAO and provides additional explanation. The draft EIS is available online, and has been available for public comment. Two comments were received. One was from the Washington State Department of Transportation (WSDOT), which was generally supportive of the PAO. Community Transit also issued a comment in support of the PAO.

Board Member Lovell asked if the concept of a multi-family tax exemption is entirely separate from the draft PAO, and Ms. Hope answered affirmatively.

Ms. Hope advised that the Engineering Department has requested that the two transportation projects identified in the draft PAO (Page 17) should be added to the Transportation Improvement Plan (TIP). One is improvements at SR-99 and 220th Street, and the other is at SR-99 and 224th Street. In addition, the Traffic Engineer has requested that the language describing the SR-99/224th Street Project be updated by adding after “left turn lane,” the following phrase, “or an alternative design as confirmed by further study.” She explained that there may be a better way to solve the issues at this intersection, and the proposed change would allow the City to consider other options.

Chair Rubenkonig referred to Items 3a and 3b on Page 19 of the PAO. She noted that Item 3a identifies a threshold for when the additional trips associated with a project will trigger additional SEPA review. She shared an example of a project with 2,000 PM peak trips followed by another project with 1,000 PM peak trips. Although the total additional trips would be over the limit, neither project would be subject to additional SEPA review. However, it appears that the two projects would move the City into an issue with concurrency as described in Item 3b. Ms. Hope said the basic idea is that once the total limit of 2,755 is reached, any other projects, regardless of the number of trips, would have to be considered under a separate process that would require a regular SEPA review. She emphasized that 2,755 is considered a cumulative cap, and the count would be based on the type of use and the projected PM peak hour trips associated with each project that occurs.

Regarding concurrency, Ms. Hope explained that when a proposed project exceeds the level of service standards adopted in the Comprehensive Plan, GMA requires the City to deny the application, require mitigation so that the level of service will not be exceeded, or allow another alternative that keeps the project from exceeding the level of service standards.

Ms. Hope explained that when a project comes in for a permit, the applicant would still have to fill out a SEPA Checklist, which would be reviewed by staff to make sure the project fits within the parameters of the PAO. If so, no additional SEPA work would be required.

Chair Rubenkonig opened the hearing for public testimony. No one in the audience indicated a desire to participate in the hearing, and the hearing was closed.

Board Member Lovell commented that the draft PAO is a validation of the planning work that was done by the consultants relative to the Highway 99 Subarea Plan, which the Board previously moved on to the City Council with a recommendation of approval. He said he would support moving the PAO forward to the City Council with a recommendation of approval, including the changes proposed by the Traffic Engineer.

BOARD MEMBER LOVELL MOVED THAT THE BOARD FORWARD THE DRAFT PLANNED ACTION ORDINANCE FOR THE HIGHWAY 99 SUBAREA PLAN TO THE CITY COUNCIL WITH A RECOMMENDATION OF APPROVAL AS PRESENTED, IN ACCORDANCE WITH THE JULY 26, 2017 STAFF REPORT, WHICH REFLECTS THE BOARD’S MAY 10, 2017 REVIEW, WITH A CONCURRENT PUBLIC HEARING ON JULY 26, 2017. HE FURTHER MOVED THAT THE BOARD RECOMMEND APPROVAL OF THE SUPPLEMENTAL LANGUAGE PUT FORWARDED BY THE TRAFFIC ENGINEER IN THE STAGE 2 SECTION OF THE PLANNED ACTION EIS MIGITATION DOCUMENT (EXHIBIT B). BOARD MEMBER ROBLES SECONDED THE MOTION, WHICH CARRIED UNANIMOUSLY.

COMPARISON TABLES

Draft CG Regulations

16.60.020 Site development standards – General.

Dimensional Requirements Table						
	Minimum Lot Area	Minimum Lot Width	Minimum Street Setback	Minimum Side/Rear Setback	Maximum Height	Maximum Floor Area
CG	None	None	5'/10' ²	0/15 ¹	75' ³	None

A. Table. Except as hereinafter provided, development requirements shall be as follows:

1 Fifteen feet from all lot lines adjacent to RM or RS zoned property; otherwise no setback is required by this subsection..

2 The 5' minimum width applies only to permitted outdoor auto sales use; otherwise the minimum is 10'.

3 None for structures located within an area designated as a high-rise node on the comprehensive plan map.

Existing Regulations

16.60.020 Site development standards – General.

A. Table. Except as hereinafter provided, development requirements shall be as follows:

	Minimum Lot Area	Minimum Lot Width	Minimum Street Setback	Minimum Side/Rear Setback	Maximum Height	Maximum Floor Area
CG	None	None	4' ²	None ¹	60' ³	None
CG2	None	None	4' ²	None ¹	75' ³	None

¹ Fifteen feet from all lot lines adjacent to RM or RS zoned property regardless of the setback provisions established by any other provision of this code.

² Street setback area shall be fully landscaped.

³ None for structures located within an area designated as a high-rise node on the comprehensive plan map.

City Council Agenda Item

Meeting Date: 08/15/2017

Amendments to Multi-Family Tax Exemption Code Provisions (15 min)

Staff Lead: Patrick Doherty

Department: Economic Development

Preparer: Patrick Doherty

Background/History

On October 18, 2016 the Edmonds City Council approved addition of Chapter 3.38 to the Edmonds City Code, creating the Multi-Family Tax Exemption Program (MFTE). The initial "residential targeted area" where the MFTE was implemented was the Westgate Mixed-Use Zone, as identified in the new Chapter 3.38.

Staff Recommendation

Upon conclusion of the Public Hearing on the Highway 99 Subarea Residential Targeted Area and anticipated approval of the Resolution designating the Highway 99 Subarea as a residential targeted area, consider the proposed amendments to Chapter 3.38 for approval.

Narrative

At this time, with the approval of the Highway 99 Subarea Plan, the Highway 99 Subarea is proposed to be designated a residential targeted area for the purposes of implementing the MFTE in that district as well. This is intended to promote the redevelopment of the Highway 99 Subarea, especially including market-rate and affordable housing. For this reason, as well as to clarify two other provisions in the existing code language, minor amendments to ECC Chapter 3.38 are proposed.

The attached proposed amendments to ECC Chapter 3.38 can be summarized as follows:

Sec. 3.38.020 - Definitions. To clarify the definitions of "low-income households" and "moderate-income households" as provided in the applicable RCW 84.14.010, customized definitions more readily applicable to Edmonds and Snohomish County are proposed.

Sec. 3.38.030 - Duration - Valuation - Exceptions. While per State law and existing ECC code, 20% of the multifamily housing units must be provided as low- and moderate-income households to qualify for the 12-year partial tax exemption, this proposal would specify that 10% of the units be made available to low-income households and 10% to moderate-income households.

Sec. 3.38.040 - Residential targeted areas - Designation. This adds Highway 99 Subarea as the second residential targeted area in the City.

At its meeting on 8/8/17, the Parks, Planning and Public Works Council Committee reviewed the proposal and voted to forward the proposed amendments to the 8/15/17 Council Regular meeting for

discussion and approval.

Attachments:

2017-08-03 Ordinance - MFTE code amendments

ORDINANCE NO. _____

AN ORDINANCE OF THE CITY OF EDMONDS, WASHINGTON, ~~ADDING~~ AMENDING PORTIONS OF A NEW CHAPTER 3.38 ~~TO OF~~ THE EDMONDS CITY CODE, ~~TO ESTABLISH THE~~ MULTI-FAMILY TAX EXEMPTION PROGRAM.

WHEREAS, Chapter 84.14 of the Revised Code of Washington (RCW) provides for temporary special valuations for eligible improvements associated with multifamily housing, especially within urban centers deficient in planned residential development, and

WHEREAS, in order to establish a Multifamily Tax Exemption Program, the City must designate one or more residential targeted areas within which the temporary special valuation may be granted to qualifying residential improvements; and

WHEREAS, after complying with all the corresponding requirements set out in RCW 84.14.040, on August 15, 2017 the Edmonds City Council designated the Highway 99 Subarea, as identified in the Highway 99 Subarea Plan, as a residential targeted area for the purposes of establishing a Multifamily Tax Exemption Program; and

WHEREAS, Edmonds City Code Chapter 3.38, Multi-Family Tax Exemption, must be amended to include mention of the Highway 99 Subarea as the City's second residential targeted area; and

WHEREAS, other minor amendments to Edmonds City Code Chapter 3.38 are proposed to clarify the definitions of low- and moderate-income households for the purposes of including affordable housing; and

WHEREAS, the Mayor and City Council find it to be in the public interest to promote redevelopment in the Highway 99 Subarea and Westgate Mixed-Use Zone District, especially including market-rate and affordable housing;

NOW, THEREFORE,

THE CITY COUNCIL OF THE CITY OF EDMONDS, WASHINGTON, DOES ORDAIN AS FOLLOWS:

Section 1. ~~A new Chapter 3.38~~ Sections 3.38.020, 3.38.030, and 3.38.040 of the Edmonds City Code ~~is are added hereby amended to the Edmonds City Code, to read~~ as follows (new text is shown in underline; deleted text is shown in strikethrough):

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3.38.020 Definitions

Unless expressly provided for below, in construing the provisions of this chapter, definitions of key terms in this chapter shall follow the definitions as set out in RCW 84.14.010.

A. "City" means the city of Edmonds, Washington.

B. "Director" means the director of the city's department of development services or authorized designee.

C. "Low-income household" means a single person, family, or unrelated persons living together whose adjusted income is at or below eighty percent of the median family income adjusted for family size, as calculated using the United States census bureau's median family income data for Snohomish County in conjunction with the median family income documentation system published by the United States department of housing and urban development.

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~~B.D.~~ "Moderate-income household" means a single person, family, or unrelated persons living together whose adjusted income is more than eighty percent but is at or below one hundred fifteen percent of the median family income adjusted for family size, as calculated using the United States census bureau's median family income data for Snohomish County in conjunction with the median family income documentation system published by the United States department of housing and urban development.

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~~E.~~ "Residential targeted area" means any urban center so designated by the Edmonds city council in accordance with this chapter and Chapter 84.14 RCW, and which has been found by the city council to be lacking sufficient available, convenient, attractive, livable, and desirable residential housing to meet the needs of the public.

~~D.~~ "Urban center" means any district or subarea of the City of Edmonds designated as a mixed-use center through a subarea or comprehensive planning process, where urban residents may obtain a variety of products and services including several business establishments, such as

shops, offices, banks, restaurants, medical facilities, governmental agencies and a mixture of uses and activities that may include housing, recreation, and cultural activities in association with either commercial or office uses, or both uses.

3.38.030 Tax exemption – Duration – Valuation – Exceptions

A. The value of new housing construction, conversion, and rehabilitation improvements qualifying under this chapter is exempt from ad valorem property taxation for 12 successive years beginning January 1st of the year immediately following the calendar year of issuance of the final certificate of tax exemption if the property otherwise qualifies for the exemption under this chapter and the applicants commits to renting or selling at least ~~20~~ 10 percent of the multifamily housing units as affordable housing units to low-income households and 10 percent of the multifamily housing units as affordable housing units to moderate-income households, and the property must satisfy that commitment.

B. The exemption does not apply to the value of land or to the value of nonhousing improvements not qualifying under ECC 3.38.050, nor does the exemption apply to increases in assessed valuation of land and nonqualifying improvements. This article also does not apply to increases in assessed valuation made by the assessor on nonqualifying portions of building and value of land, nor to increases made by lawful order of a county board of equalization, the Department of Revenue, or a county, to a class of property throughout the county or specific area of the county to achieve the uniformity of assessment or appraisal required by law.

3.38.040 Residential targeted areas – Designation

A. The following area is designated by the City Council as a residential targeted area, consistent with the requirements of RCW 84.14.040:1. Westgate Mixed Use (WMU) Zoning District.

2. Highway 99 Subarea, as designated in the Highway 99 Subarea Plan

B. If part of any legal lot is within a residential targeted area, the entire lot shall be deemed to lie within the residential targeted area.

C. The area(s) designated in subsection A of this section may be amended and other areas may be added by action of the City Council consistent with requirements of RCW 84.14.040. Any amendment to the residential targeted areas shall not affect the status of a project for which the City has received a complete application for property tax exemption under this chapter.

Section 2. Severability. If any section, subsection, clause, sentence, or phrase of this ordinance should be held invalid or unconstitutional, such decision shall not affect the validity of the remaining portions of this ordinance.

Section 3. Effective Date. This ordinance, being an exercise of a power specifically delegated to the City legislative body, is not subject to referendum and shall take effect five (5) days after passage and publication of an approved summary thereof consisting of the title.

APPROVED:

MAYOR DAVE EARLING

ATTEST/AUTHENTICATED:

CITY CLERK, SCOTT PASSEY

APPROVED AS TO FORM:
OFFICE OF THE CITY ATTORNEY:

BY _____
JEFF TARADAY

FILED WITH THE CITY CLERK:
PASSED BY THE CITY COUNCIL:
PUBLISHED:
EFFECTIVE DATE:
ORDINANCE NO. _____

SUMMARY OF ORDINANCE NO. _____

of the City of Edmonds, Washington

On the ____ day of _____, 2017, the City Council of the City of Edmonds, passed Ordinance No. _____. A summary of the content of said ordinance, consisting of the title, provides as follows:

AN ORDINANCE OF THE CITY OF EDMONDS,
WASHINGTON, AMENDING PORTIONS OF
CHAPTER 3.38 ~~TO OF~~ THE EDMONDS CITY CODE,
MULTI-FAMILY TAX EXEMPTION PROGRAM.

The full text of this Ordinance will be mailed upon request.

DATED this ____ day of _____, 2017.

CITY CLERK, SCOTT PASSEY

4840-7251-8158, v. 1

City Council Agenda Item

Meeting Date: 08/15/2017

Amendments to Chapter 3.75, Business Improvement District (10 min)

Staff Lead: Patrick Doherty

Department: Economic Development

Preparer: Patrick Doherty

Background/History

After over four years since their inception in 2013, it is appropriate to update the Edmonds City Code provisions related to the Edmonds Downtown Business Improvement (hereinafter referred to as “EDBID,” but also doing business as “Edmonds Downtown Alliance”). The purpose of these proposed amendments is to align the code provisions more closely with the evolution of the month-to-month conduct of business over the past four years.

Staff Recommendation

Approval at the 8/15/17 Regular Council meeting.

Narrative

Here is a brief summary of the most salient amendments proposed, as approved by the Edmonds Downtown Alliance Board at their July 13, 2017 meeting:

3.75.010 - Adds “modification of boundaries” to this section in recognition of the potential for the EDBID to expand.

3.75.040 - Expands the nomenclature for the second, lower-dues-paying classification of businesses previously denominated simply as “by appointment” businesses to include “and/or office-based” and adds new examples such as: professional service firms, assembly or production of goods, corporate offices, etc.

3.75.070 - Adds a clarification that assessments for members in newly expanded areas of the EDBID will pay their dues after the first full quarter after inclusion in the EDBID.

3.75.075 - Removes reference to the original effective date of the ordinance for the one-year exemption of new businesses from paying dues to a simple reference to “new businesses” on an on-going basis.

3.75.120 - Removes references to the initial Mayor and Council appointment/confirmation of EDBID Board members. Updates language regarding the required annual report and allows for submittal of the annual report by October 31st of each year, rather than October 1st. This allows for more work to be accomplished in each year about which to report, as well as dovetail more closely to the City’s annual budget process.

3.75.130 - Adds reference to general support and advice on matters related to programs, projects, activities and administration provided by the community services/economic development director.

3.75.140 - Removes outdated reference to community services/economic development and/or finance director serving as administrators until administrative staff is brought on, which has since occurred and is on-going.

Please see attached proposed code amendments for full detail.

These amendments were reviewed by the Council Finance Committee at its 8/8/17 meeting, who moved to forward the amendments for discussion and approval at the Regular Council meeting of 8/15/17.

Attachments:

2017-08-15 Ordinance - BID code amendments

ORDINANCE NO. _____

AN ORDINANCE OF THE CITY OF EDMONDS,
WASHINGTON, AMENDING PORTIONS OF CHAPTER 3.75
OF THE EDMONDS CITY CODE, BUSINESS
IMPROVEMENT DISTRICT.

WHEREAS, Chapter 35.87A of the Revised Code of Washington (RCW) provides for the establishment of business improvement districts in local municipalities “(t)o aid general economic development and neighborhood revitalization, and to facilitate the cooperation of merchants, businesses, and residential property owners which assists trade, economic viability, and liveability”; and

WHEREAS, pursuant to Ordinance 3909, adopted on January 15, 2013, the Edmonds City Council added a new Chapter to the Edmonds City Code entitled Chapter 3.75, Business Improvement District, thus establishing the Edmonds Downtown Business Improvement District, aka “Edmonds Downtown Alliance”; and

WHEREAS, over the passage of four years the Edmonds Downtown Alliance Members Advisory Board and City staff have identified the need for various, nonsubstantive changes to Chapter 3.75 to reflect current practice and conditions; and

WHEREAS, on July 13, 2017 the Edmonds Downtown Alliance Members Board approved the amendments to Edmonds City Code Chapter 3.75, proposed herein; and

NOW, THEREFORE,

THE CITY COUNCIL OF THE CITY OF EDMONDS, WASHINGTON, DOES
ORDAIN AS FOLLOWS:

Section 1. Sections 3.75.010, 3.75.030, 3.75.040, 3.75.070, 3.75.075, 3.75.100, 3.75.120, 3.75.130, of the Edmonds City Code are hereby amended to read as follows (new text is shown in underline; deleted text is shown in ~~striketrough~~):

3.75.010 Area established – Modification of boundaries.

A. As authorized by Chapter [35.87A](#) RCW, there is hereby established an Edmonds downtown business improvement district (“EDBID”) within the following boundaries as shown on the map attached to the ordinance codified in this chapter as “Attachment A” (when a street or alley is named, the area boundary is the centerline of the right-of-way including vacated portions unless otherwise specified in the description). The EDBID includes the parcels with the tax ID numbers set forth in Attachment B to the ordinance codified in this chapter, which is incorporated herein as if set forth in full (for additional detail, please see the attached map). In case of a conflict between the list of parcels (Attachment B) and the map (Attachment A), the map shall control. [Ord. 3909 § 1, 2013].

B. [Modification or expansion of the boundaries delineating the area that contains the EDBID shall be permissible in accordance with the criteria and requirements set forth in RCW 35.87A.075.](#)

3.75.030 Programs.

Assessment revenues may be used for any combination of the following component programs, but not for any other purpose:

A. Marketing and Hospitality.

1. Maps/brochures/kiosks/directories.
2. Web site.
3. Social media.
4. Marketing/advertising campaigns.
5. Holiday decorations.
6. Street performers/artists.
7. Historic education/heritage advocacy.
8. Special public events.

B. Safety and Cleanliness.

1. Maintenance.

2. Security.

3. Pedestrian environment enhancements.

C. Appearance and Environment.

1. Design enhancements.

2. Neighborhood advocacy and communication.

3. Streetscapes/lighting/furniture.

D. Transportation.

1. Transportation alternatives.

2. Directional signage.

3. Parking management and mitigation.

E. Business Recruitment and Retention.

1. Education/seminars.

2. Market research.

3. Business recruitment.

F. ~~Organization~~ Administration.

1. Contract staff and professional services.

2. Administration costs. [Ord. 3909 § 1, 2013].

3.75.040 Assessment methodology.

The special assessment amount shall be based on the type of business multiplied by the square footage of a business. There shall be two business classifications for the purposes of calculating the assessment: “open door” businesses (e.g., restaurants, retailers, banks, etc.); and “by appointment and/or office-based” businesses (e.g., hair salons, doctors, lawyers, nail salons, insurance providers, counselors, financial advisors,

professional services firms, assembly or production of goods, corporate offices, etc.). Open-door establishments are assessed at a higher rate than by-appointment/office-based businesses due to the relatively higher benefit that open-door businesses are expected to receive from the programs. Table 1 shows the annual assessment for both classifications of businesses based on their square footage within the EDBID.

Table 1

Square Footage	Assessment for “Open Door”	Assessment for “By Appointment/<u>Office-Based</u>”
0 – 499	\$90 quarter (\$30 per month)	\$30 quarter (\$10 per month)
500 – 999	\$105 quarter (\$35 per month)	\$45 quarter (\$15 per month)
1,000 – 1,999	\$120 quarter (\$40 per month)	\$60 quarter (\$20 per month)
2,000 – 4,999	\$135 quarter (\$45 per month)	\$75 quarter (\$25 per month)
5,000 and over	\$150 quarter (\$50 per month)	\$90 quarter (\$30 per month)

~~The initial~~ Assessment calculations shall be based on the city of Edmonds’ business license records ~~for year 2012~~. Governmental entities, public utilities, nonprofits operating under Internal Revenue Code Section 501(c)(3) shall not be assessed. [Ord. 3909 § 1, 2013].

3.75.070 Collection schedule.

A. Assessments shall be paid and collected on a quarterly basis; provided, that members may opt to pay the full yearly assessment at the time of the billing for the first quarter. [Ord. 3929 § 1 (Att. A), 2013; Ord. 3909 § 1, 2013].

B. ~~Assessments for members located within expanded boundaries of the EDBID shall be collected- the first full quarter after inclusion in the EDBID.~~

3.75.075 Exemption for new businesses.

~~New b~~Businesses ~~established after the effective date of April 22, and~~ located within the Edmonds downtown business improvement district shall be exempt from special assessments imposed pursuant to this chapter for a period of one year from the date a business license is issued by the city of Edmonds. [Ord. 3965 § 1, 2014].

3.75.100 Disputes.

Any member seeking to challenge the amount of an assessment or delinquency charge may submit additional information concerning the assessment classification and/or square footage of the business to the finance director or designee, who may adjust the assessment based on Table 1 if he determines that the original classification and/or square footage was erroneous. The finance director shall provide a written determination to the member, which may be appealed to the city's hearing examiner within 10 days of the finance director's determination by paying an appeal fee in the amount of \$250.00 and filing a notice of appeal with the city clerk that sets forth the alleged error in the finance director's determination. The member has the burden of proof before the hearing examiner to show that the assessment or delinquency charge is incorrect. [Ord. 3929 § 1 (Att. A), 2013; Ord. 3909 § 1, 2013].

3.75.120 Members advisory board.

The EDBID shall be governed by a ~~mayor shall appoint, subject to confirmation by the city council, an interim~~ members advisory board comprised of and odd number of at least seven andbut no more than 11eleven members from the EDBID membership ~~within 60 days of the effective date of the ordinance codified in this chapter. The mayor shall appoint at least seven and no more than 11 interim board members.~~ The interim board shall be composed of both open door and by appointment office-based EDBID members, respectively, in rough proportion to the dollar value of assessments to be levied on each classification of businesses. In addition, the finance director and the community services/economic development director shall be nonvoting, ex officio members of the ~~interim~~ board.

The ~~interim~~ members advisory board shall be responsible for proposing any amendments to EDBID bylaws for city council approval and policy guidelines to the city council that will address, among other matters, the composition and method of appointing or electing ~~future~~ board members. ~~The initial member advisory board shall submit their proposed bylaws to the city council within 90 days of the city council's confirmation of the mayor's initial appointments.~~ The ~~interim~~ members advisory board shall also provide advice and consultation to the finance director, and to any individual or agency, that may be hired to manage the day-to-day operations of the business improvement district program, on all matters related to the operation of the program.

Each year no later than October 31st ~~The the interim~~ members advisory board shall submit to the city council a proposed annual work program and budget ~~for 2013 to the city council within 90 days of the city council's confirmation of the mayor's initial appointments and shall recommend a work program and budget for the following all subsequent years no later than October 1st of the year before~~, unless otherwise approved by the city council. The board shall address and discuss member concerns and questions regarding the EDBID and program; review all reports submitted to the finance department by the members advisory board or program administrator, if hired; and sponsor the ~~initial~~ annual members meeting. [Ord. 3941 § 1 (Att. A), 2013; Ord. 3914 § 1 (Att. A), 2013; Ord. 3909 § 1, 2013].

3.75.130 Business improvement district fund oversight and programmatic support.

The finance director shall administer the BID fund for the city with authority to:

- A. Consider adjusting the assessment amount based on an erroneous classification or square footage;
- B. Collect the assessments; refund assessments when overpaid or otherwise improperly collected; extend the deadline for payment; and waive delinquency charges and interest whenever the delinquency results from extenuating circumstances beyond the member's control, such as a casualty loss causing premature closure of the business, bankruptcy, or the total principal payment due to the city (exclusive of penalty and interest) is \$10.00 or less;
- C. Calculate and collect the interest for late payments; and
- D. Accept and deposit advance payment of assessments by members; accept donations from governmental agencies, the public, and owners and operators of businesses. [Ord. 3909 § 1, 2013].

E. The community services/economic development director shall provide general support and advice on matters related to programs, projects, activities and administration of the EDBID.

Section 2. Section 3.75.140 of the Edmonds City Code is hereby repealed.

~~3.75.140 Contract for program management.~~

~~The community services/economic development director and/or finance director shall serve as the program administrator until permanent provisions are made pursuant to adopted bylaws. [Ord. 3909 § 1, 2013].~~

Section 3. Severability. If any section, subsection, clause, sentence, or phrase of this ordinance should be held invalid or unconstitutional, such decision shall not affect the validity of the remaining portions of this ordinance.

Section 4. Effective Date. This ordinance, being an exercise of a power specifically delegated to the City legislative body, is not subject to referendum and shall take effect five (5) days after passage and publication of an approved summary thereof consisting of the title.

APPROVED:

MAYOR DAVE EARLING

ATTEST/AUTHENTICATED:

CITY CLERK, SCOTT PASSEY

APPROVED AS TO FORM:
OFFICE OF THE CITY ATTORNEY:

BY _____
JEFF TARADAY

FILED WITH THE CITY CLERK:
PASSED BY THE CITY COUNCIL:
PUBLISHED:
EFFECTIVE DATE:
ORDINANCE NO. _____

SUMMARY OF ORDINANCE NO. _____

of the City of Edmonds, Washington

On the ____ day of _____, 2017, the City Council of the City of Edmonds, passed Ordinance No. _____. A summary of the content of said ordinance, consisting of the title, provides as follows:

AN ORDINANCE OF THE CITY OF EDMONDS,
WASHINGTON, AMENDING PORTIONS OF
CHAPTER 3.75 OF THE EDMONDS CITY CODE,
BUSINESS IMPROVEMENT DISTRICT.

The full text of this Ordinance will be mailed upon request.

DATED this ____ day of _____, 2017.

CITY CLERK, SCOTT PASSEY

4840-7251-8158, v. 1

Attachment: 2017-08-15 Ordinance - BID code amendments (Amendments to Chapter 3.75, Business Improvement District)

City Council Agenda Item

Meeting Date: 08/15/2017

Pine Street Lighting Discussion (20 min.)

Staff Lead: Phil Williams

Department: Public Works & Utilities

Preparer: Royce Napolitano

Background/History

Council authorized funding in the 2017 City budget for the installation of lights on Pine Street just west of SR104 to improve safety conditions for pedestrians. Work was initiated on the project in the Spring. Significant public comment was subsequently given to the Council regarding the project. Council placed the project on hold pending a review. Staff is prepared to discuss options and costs for the completion of the project. This issue was discussed with the PPP committee on July 25th & August 8th, with no decision made regarding staff recommendation. Council President Mesaros has called the item forward as a Presentation & Action item on this agenda.

Staff Recommendation

Presentation of lighting options & Council discussion for authorization to proceed.

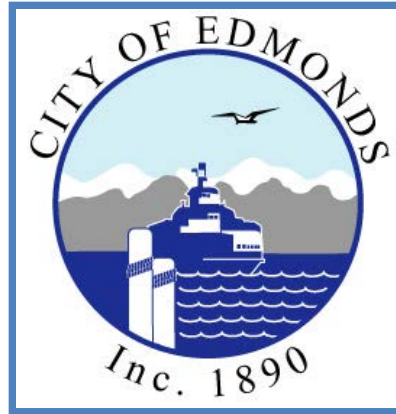
Narrative

Concerns expressed about the street lights proposed on Pine Street were primarily related to any light that might impinge on the Wildlife Demonstration Garden immediately north of the Pine Street Right-of-Way. Staff will present two lighting options that will include illumination mapping to show the distribution of light from the individual luminaires, including mounting heights, wattage equivalents, and any shielding being proposed. After a discussion with Council staff is looking for a consensus direction on the project and authorization to proceed.

Attachments:

Pine Street Lighting

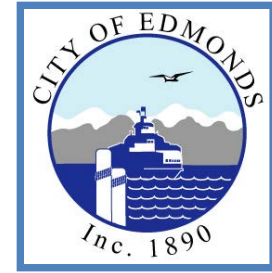
PUD ATBS_.pdf



Pine Street Lighting

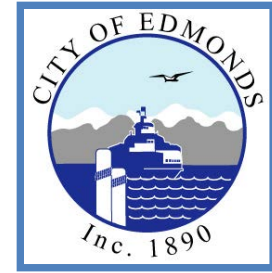
City Council 7/25/2017

PPP Committee



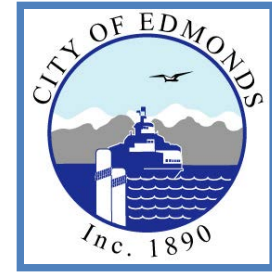
Review

- 2014/2015 Point Edwards residents express safety concerns regarding conflicts between cars and pedestrians as well as general security along Pine Street
- November, 2016 - Council authorizes \$20,000 for street lighting on lower Pine Street west of SR104
- Late 2016-early 2017 - City staff works with SnoPUD on a proposal to install street lights.
- May, 2017 PW staff begins installation of lights



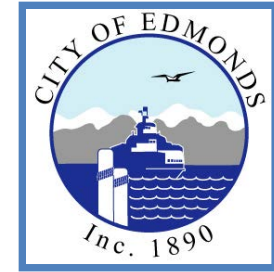
Review

- May 2, 2017 Council puts project on “hold” pending further discussion and discussion of options.
- Last 60 days PW Staff has researched two different lighting options: a package from SnoPUD, and a package from Sternberg
- Tonight begins a review of those options



SnoPUD Option

- Three poles – 35' mounting height
- 161' spacing
- 50w LED Acuity Roadway Type II 3,000 deg. K
- Equivalent to 118w HPS
- Choice of house-side shield or light trespass shield (most aggressive)
- City installs buried conduit (halted)
- PUD install and maintain fiberglass direct-bury poles and luminaires
- Monthly charge approx. \$6.00@



SnoPUD Option



Consistent with LEED® goals
& Green Globes™ criteria
for light pollution reduction

Autobahn Series ATBS Roadway & Security Lighting

PRODUCT OVERVIEW



Applications:

Residential streets
Parking lots
General security lighting

8.50 x 11.00 in

DIMENSIONS

Features:

OPTICAL

Same Light: Performance is comparable to 50W – 150W HPS and up to 175W Mercury Vapor roadway and security lighting luminaires.

White Light: Correlated color temperature - 4000K, 70 CRI minimum, 3000K, 70 CRI minimum or optional 5000K, 70 CRI minimum.

IP66 rated borosilicate glass optics ensure longevity and minimize dirt depreciation. Unique IP66 rated LED light engines provide 0% uplight and restrict backlight to within sidewalk depth, providing optimal application coverage and optimal pole spacing.

Available distributions are Type II, III, and V roadway distributions. When used with the optional acrylic refractor the unit provides approximately 10% uplight and increased vertical foot-candles

ELECTRICAL

Expected Life: LED light engines are rated >100,000 hours at 25°C, L70. Electronic driver has an expected life of 100,000 hours at a 25°C ambient.

Lower Energy: Saves an expected 40-60% over comparable HID luminaires.

Robust Surge Protection: Three different surge protection options provide a minimum of ANSI C136.2 10kV/5kA protection. 20kV/10kA surge protection is also available.



Attachment: Pine Street Lighting (Pine Street Lighting Discussion)

Series	Performance Packages	Voltage	Optics
ATBS Autobahn LED Roadway & Security	A 2,500 lumens B 3,200 lumens C 3,800 lumens E 4,700 lumens F 5,400 lumens G 6,100 lumens H 7,100 lumens I 8,500 lumens	MVOLT Multi-volt, 120-277V	R2 Roadway Type II R3 Roadway Type III R5 Roadway Type V D2 Type II, Drop Refractor included D3 Type III, Drop Refractor included D5 Type V, Drop Refractor included

Options			
<u>Color Temperature (CCT)</u> (Blank) 4000K CCT, 70 CRI Min. 3K 3000K CCT, 70 CRI Min. 5K 5000K CCT, 70 CRI Min. <u>Paint</u> Blank Gray (Standard) BK Black WH White BZ Bronze <u>Surge Protection</u> Standard 10kV/5kA SPD Blank Acuity SPD-10kV/5kA with inductive filter (Standard) 20 20kV/10KA SPD MP MOV Pack IL SPD with Indicator Light	XL Not CSA Certified <u>Controls</u> (Blank) 3 Pin NEMA Photocontrol Receptacle NR ¹ No Photocontrol Receptacle DM ² 0V-10V Dimmable Driver P5 5 Pin Photocontrol Receptacle (dimmable driver included) P7 7 Pin Photocontrol Receptacle (dimmable driver included) PCSS ¹ DTL DSS Photocontrol PCL ¹ DTL DLL Photocontrol 120-277V A0 Field Adjustable Output SH Shorting Cap <u>Install Packages</u> PKGS DTL DSS Photocontrol	<u>Accessories</u> ATBSREF Drop Refractor for field installation ATBSHSS House Side Shield for field installation ATBSLTS Light Trespass Shield for field installation	



SNOHOMISH COUNTY PUD #1

Be a Conservation Sensation

2320 California Street
PO BOX 1107
Everett, WA 98201
(425) 783-8272

QUOTE

Name and Address

CITY OF EDMONDS
ATTN: TOD MOLES

Paid Date

Cashier

Office Paid

Receipt

Check

Payment Type

Connection Object

Business Partner

Contract Account

Property Address

ST LTS

7110 210TH ST SW, ED

Quote Date

Quote Expires

Construction Type

5/10/2017

8/8/2017

New

Designer & Phone

Office

Eddie Haugen (425-783-8276)

Everett

Work Order

Description

Qty

Acct

Unit Cost

Total

0

INSTL (3) FIBERGLASS SL POLES AND PED

1

0

12,772.00

12,772.00

Notes FEE COVERS INSTALLATION OF (3) FIBERGLASS ST LT POLES AND PED AT 7110 210TH ST SW, EDMONDS. PLEASE SEND PAYMENT OR PO TO PUD, ATTN: EDDIE HAUGEN, PO BOX 1107, EVERETT, WA 98206.

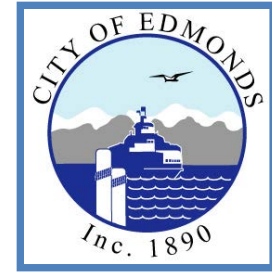
Total \$ 12,772.00



Attachment: Pine Street Lighting (Pine Street Lighting Discussion)



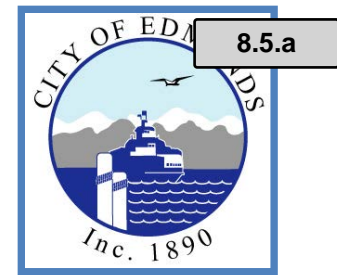
Attachment: Pine Street Lighting (Pine Street Lighting Discussion)



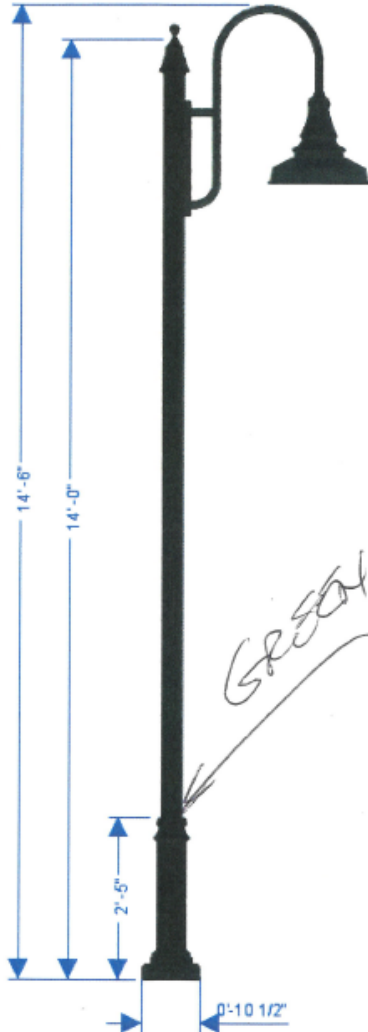
Sternberg Option

- 5 poles - 13.5' mounting height
- 82' spacing
- 32w LED Roadway Type IV 3,000 deg. K
- Equivalent to approx. 80w HPS
- No additional house-side shield or cut-off available
- City does entire installation including concrete bases
- City owns and maintains system – buys power from PUD

Catalog Number: 1A-1910LEDRLM18-1RND35T4-MDL-FL/91WPM/4514FP4/BCC/BK Type: _____
Customer Approval: _____



Conceptual assembly drawing, subject to Engineering verification by factory.



NUMBER OF ARMS: 1

ARM ARM MOUNTED FIXTURE: 1910LEDRLM18

The 1910RLM Park Ridge series is a decorative downlight fixture which consists of a decorative cast aluminum fitter, cast ballast housing assembly, a spun aluminum shade, and lens.

LIGHT SOURCE: -1RND35T4-MDL-FL

Array: 1RND (28 LEDs, 32 Watts) (1RND)
Color Temp: 3500K (35) **3000**
Distribution: Type 4 (T4)
Driver: Multi-Volt Dimmable Low-Range Driver, 120-277V (MDL)
Fixture Style: Flat Lens w/ 18 Inch Shade (FL)

ARM: 91WPM

91WPM

POLE: 4514FP4/

The 10-1/2" diameter cast 356 aluminum alloy base and aluminum shaft shall be a one-piece construction. The pole shall be U.L. or E.T.L. listed in U.S. and Canada.

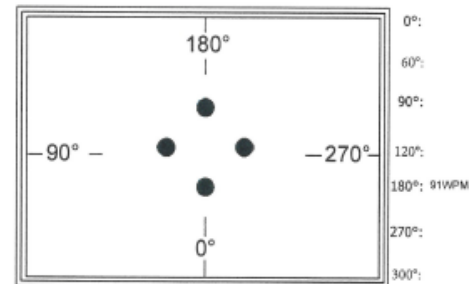
Model: 4500 (45)
Height: 14 Ft (14)
Shaft Type: Fluted Straight 4 Inch, 6061-T6 Aluminum Alloy (FP4)

POLE CAP: BCC

2" Ball Center Cap - BCC

FINISH: BK

Assembly shall be powder coated to Black Smooth finish. Prior to coating, the assembly shall be chemically cleaned and etched in a 5-stage washing system which includes alkaline cleaning, rinsing, phosphoric etching, reverse osmosis water rinsing, and non-chrome sealing to ensure corrosion resistance.



Bolt Info: (4) 1/2" X 18" Anchor Bolts, 8" Bolt Circle, Diamond pattern

Access Door Orientation: 0°

Street Side Orientation: 180°

Rev	Description	By	Date	Job Name:	Drawing No.
A				City of Edmonds, Wa	16744
B				Job Location:	
C				Drawn By:	Checked By:
D				DE	
E				Drawn Date:	Checked Date:



Date: Jun 28, 2017

Quote: PLSWA17-38201-1

Quote

Page 1/1



Pacific Lighting Systems
6363 7th Ave. S. Ste. 100
Seattle WA 98108
Phone: (206) 323-2200
Fax: (206) 726-8315
From: Patrick Armstrong
Quoter Ph: (206) 436-8820x.
email: parmstrong@pacificlightingsystems.com

Project Pine Street Lighting
Location Edmonds WA
Quote PLSWA17-38201-1

To: Chuck Liatt
City of Edmonds
WA

For
Bid Date Jun 28, 2017
Expires Jul 28, 2017

QTY	Type	MFG	Part	Price	UQ	ExtPrice
5		STERNBERG	1A-1910LEDRLM18-5P-1RND35T4-MDL03-FL/ 91WPM/4514FP4/BCC/BK	\$2,175.00		\$10,875.00

Note LEAD TIME IS AN ESTIMATED 12 WEEKS
Note FREIGHT TERMS: FULL FREIGHT ALLOWED FOR
Note ONE STANDARD SHIPMENT & QUOTED QUANTITIES.
Note STANDARD SHIPMENT IS VAN DELIVERY ON
Note AT MINIMUM POLES AND FIXTURES / FLATBED ON
Note STEEL POLES.
Note ALL ORDERS \$10,000 AND OVER REQUIRE
Note APPROVED DRAWINGS PRIOR TO RELEASE.
Note ALL QUOTES SUBJECT TO STANDARD TERMS AND
CONDITIONS.SEE STERNBERG WEBSITE FOR
DETAILS.

Total: **\$10,875.00**

Mfg Terms:
STERN Sternberg Lighting

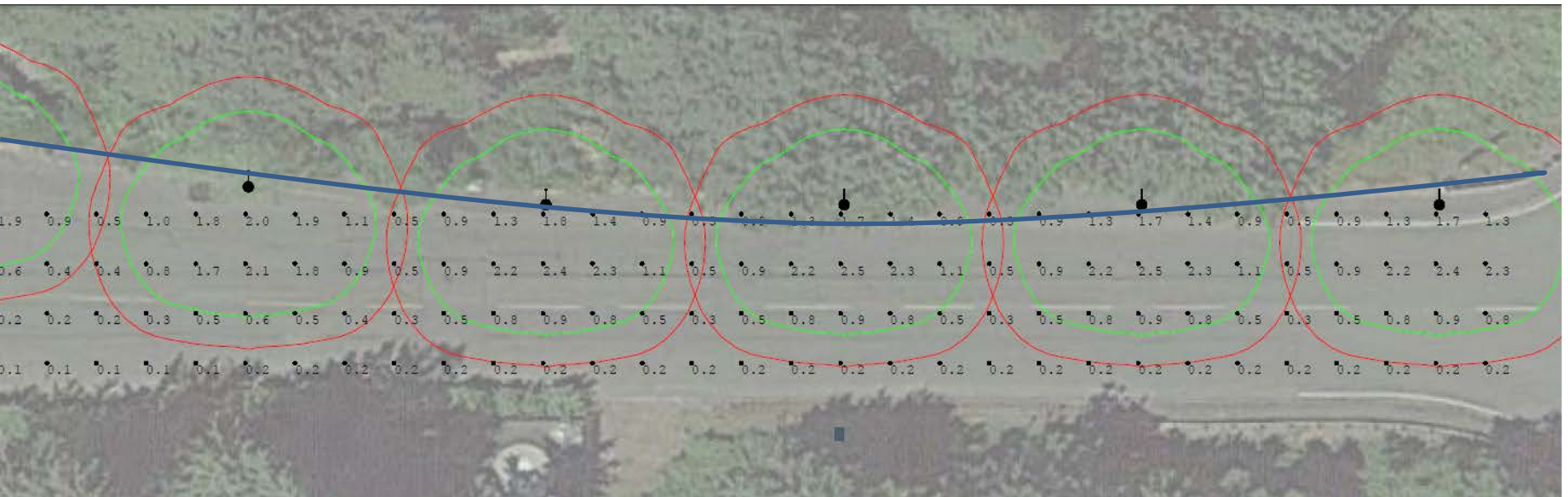
Freight
Allowance
Included

Minimum
Order



Attachment: Pine Street Lighting (Pine Street Lighting Discussion)

Attachment: Pine Street Lighting (Pine Street Lighting Discussion)





Discussion & Recommendation

- Initial cost is similar, affordable
- Choice between a city-owned and operated/maintained system vs. a system where SnoPUD is responsible for O&M and replacement
- Concern regarding the IES footprint of the Sternberg Type IV fixture without an additional shield
- Recommend SnoPUD



Consistent with LEED® goals
& Green Globes™ criteria
for light pollution reduction

Autobahn Series ATBS Roadway & Security Lighting

8.5.b

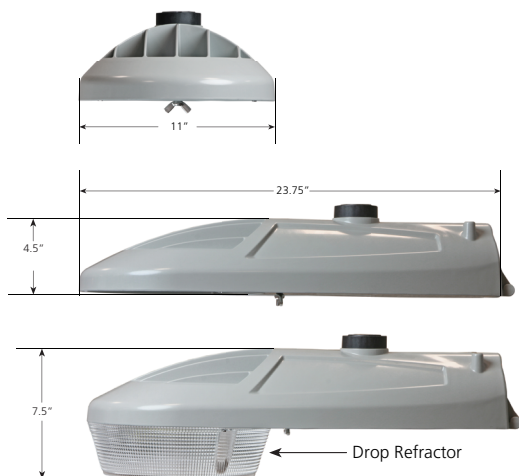
PRODUCT OVERVIEW



Applications:

Residential streets
Parking lots
General security lighting

DIMENSIONS



Effective Projected Area (EPA) The EPA for the ATBS is 0.3 sq. ft.,
Approx. Wt. = 12 lbs. (5 kg)

STANDARDS

DesignLights Consortium® (DLC) qualified product. Not all versions of this product may be DLC qualified. Please check the DLC Qualified Products List at www.designlights.org/QPL to confirm which versions are qualified.

Color temperatures of $\leq 3000\text{K}$ must be specified for International Dark-Sky Association certification.

Rated for -40°C to 40°C ambient

CSA Certified to U.S. and Canadian standards

Complies with ANSI: C136.2, C136.10, C136.14, C136.31, C136.15, C136.37

Note: Actual performance may differ as a result of end-user environment and application. Specifications subject to change without notice.

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Features:

OPTICAL

Same Light: Performance is comparable to 50W – 150W HPS and up to 175W Mercury Vapor roadway and security lighting luminaires.

White Light: Correlated color temperature - 4000K, 70 CRI minimum, 3000K, 70 CRI minimum or optional 5000K, 70 CRI minimum.

IP66 rated borosilicate glass optics ensure longevity and minimize dirt depreciation. Unique IP66 rated LED light engines provide 0% uplight and restrict backlight to within sidewalk depth, providing optimal application coverage and optimal pole spacing.

Available distributions are Type II, III, and V roadway distributions. When used with the optional acrylic refractor the unit provides approximately 10% uplight and increased vertical foot-candles

ELECTRICAL

Expected Life: LED light engines are rated $>100,000$ hours at 25°C , L70. Electronic driver has an expected life of 100,000 hours at a 25°C ambient.

Lower Energy: Saves an expected 40-60% over comparable HID luminaires.

Robust Surge Protection: Three different surge protection options provide a minimum of ANSI C136.2 10kV/5kA protection. 20kV/10kA surge protection is also available.

MECHANICAL

Includes standard AEL lineman-friendly features such as tool-less entry, 3 station terminal block and quick disconnects. Bubble level located inside the electrical compartment for easy leveling at installation.

Rugged die-cast aluminum housing and door are polyester powder-coated for durability and corrosion resistance. Rigorous five-stage pre-treating and painting process yields a finish that achieves a scribe creepage rating of 8 (per ASTM D1654) after over 5000 hours exposure to salt fog chamber (operated per ASTM B117).

Mast arm mount is adjustable for arms from 1-1/4" to 2" (1-5/8" to 2-3/8" O.D.) diameter. The 2 – bolt clamping mechanism provides 3G vibration rating per ANSI C136.

The Wildlife shield is cast into the housing (not a separate piece).

CONTROLS

NEMA 3 pin photocontrol receptacle is standard, with the Acuity designed ANSI standard 5 pin and 7 pin receptacles optionally available.

Premium solid state locking-style photocontrol – PCSS (10 year rated life) Extreme long life solid state locking-style photocontrol – PCL1 (20 year rated life)

Optional onboard Adjustable Output module allows the light output and input wattage to be modified to meet site specific requirements, and also can allow a single fixture to be flexibly applied in many different applications.

Attachment: PUD ATBS_pdf (Pine Street Lighting Discussion)

Autobahn Series ATBS

Roadway & Security Lighting

8.5.b

ORDERING INFORMATION

Example: ATBS A MVOLT R2

Series	Performance Packages	Voltage	Optics
ATBS Autobahn LED Roadway & Security	A 2,500 lumens B 3,200 lumens C 3,800 lumens E 4,700 lumens F 5,400 lumens G 6,100 lumens H 7,100 lumens I 8,500 lumens	MVOLT Multi-volt, 120-277V	R2 Roadway Type II R3 Roadway Type III R5 Roadway Type V D2 Type II, Drop Refractor included D3 Type III, Drop Refractor included D5 Type V, Drop Refractor included
Options			
<u>Color Temperature (CCT)</u> (Blank) 4000K CCT, 70 CRI Min. 3K 3000K CCT, 70 CRI Min. 5K 5000K CCT, 70 CRI Min.	XL Not CSA Certified		<u>Accessories</u> ATBSREF Drop Refractor for field installation ATBSHSS House Side Shield for field installation ATBSLTS Light Trespass Shield for field installation
<u>Paint</u> Blank Gray (Standard) BK Black WH White BZ Bronze	<u>Controls</u> (Blank) 3 Pin NEMA Photocontrol Receptacle NR¹ No Photocontrol Receptacle DM² 0V-10V Dimmable Driver P5 5 Pin Photocontrol Receptacle (dimmable driver included) P7 7 Pin Photocontrol Receptacle (dimmable driver included) PCSS¹ DTL DSS Photocontrol PCL1¹ DTL DLL Photocontrol 120-277V A0 Field Adjustable Output SH Shorting Cap		
<u>Surge Protection</u> Standard 10kV/5kA SPD Blank Acuity SPD-10kV/5kA with inductive filter (Standard) 20 20kV/10KA SPD MP MOV Pack IL SPD with Indicator Light	<u>Install Packages</u> PKGS DTL DSS Photocontrol PKGL DTL DLL Photocontrol Packages ship with selected photocontrol, 24", 1 1/4" diameter arm, 5' of prewire and mounting hardware		
<u>Misc.</u> HSS House Side Shield NL NEMA Label			

Notes

1. Not available with Install Packages.
2. Not available with AO option.



AEL Headquarters, 3825 Columbus Road, Granville, OH 43023
www.americanelectriclighting.com

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Warranty Five-year limited warranty. Complete warranty terms located at: www.acuitybrands.com/CustomerResources/Terms_and_conditions.aspx
Actual performance may differ as a result of end-user environment and application. All values are design or typical values, measured under laboratory conditions at 25 °C. Specifications subject to change without notice.

Please contact your sales representative for the latest product information.

Packet Pg. 390

Attachment: PUD ATBS_pdf (Pine Street Lighting Discussion)

PERFORMANCE PACKAGE

Performance Package	Distribution	Lumens	Input Watts	LPW	50K Hours	LLD @ 25°C 75K Hours	100K Hours
A	R2	2,514	19	132	0.93	0.89	0.85
	R3	2,515		132			
	R5	2,649		139			
	D2	2,394		126			
	D3	2,372		125			
	D5	2,521		133			
B	R2	3,166	24	132	0.93	0.89	0.85
	R3	3,167		132			
	R5	3,336		139			
	D2	3,015		126			
	D3	2,988		124			
	D5	3,175		132			
C	R2	3,784	31	122	0.93	0.89	0.85
	R3	3,780		122			
	R5	4,029		130			
	D2	3,604		116			
	D3	3,566		115			
	D5	3,835		124			
E	R2	4,770	40	119	0.93	0.89	0.85
	R3	4,704		118			
	R5	4,867		122			
	D2	4,543		114			
	D3	4,438		111			
	D5	4,650		116			
F	R2	5,392	47	115	0.93	0.89	0.85
	R3	5,407		115			
	R5	5,175		110			
	D2	5,135		109			
	D3	5,101		109			
	D5	5,051		107			
G	R2	6,235	50	125	0.94	0.92	0.90
	R3	6,101		122			
	R5	6,404		128			
	D2	5,938		119			
	D3	5,756		115			
	D5	6,193		124			
H	R2	7,194	60	120	0.94	0.92	0.90
	R3	7,141		119			
	R5	7,508		125			
	D2	6,851		114			
	D3	6,737		112			
	D5	7,150		119			
I	R2	8,653	76	114	0.94	0.92	0.90
	R3	8,525		112			
	R5	9,003		118			
	D2	8,241		108			
	D3	8,042		106			
	D5	8,574		113			

Note: Information shown above is based on 4000K nominal system data. Individual fixture performance may vary. Specifications subject to change without notice.

Attachment: PUD ATBS_pdf (Pine Street Lighting Discussion)



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City Council Agenda Item

Meeting Date: 08/15/2017

Sunset Ave Parking (20 min.)

Staff Lead: Phil Williams

Department: Engineering

Preparer: Katie MacCaul

Background/History

On August 8, 2017, staff presented a plan to add more parallel parking on Sunset Avenue and reviewed the scope and cost of the proposed Decision Package.

September 20, 2016. Council approved an Ordinance amending the parking provisions on Sunset Avenue. Council directed staff to prepare an ordinance change reducing the 4-hour on-street parking limit on Sunset Avenue to 2-hour parking.

August 23, 2016. Council directed the removal of the angled parking on Sunset, the increase of parallel parking, where achievable, installation of a modified C-curb along the walkway, design of an interim solution for the 2nd Avenue/Caspers wrong-way driving issue, and the beginning of design for the pocket park at the south end of Sunset Avenue, the intersection table at Edmonds Street, and the design of Caspers Street from Sunset to 3rd Avenue N.

Staff Recommendation

Approve the parking changes and the Decision Package.

Narrative

Parking. In June, 2017 City operations crews removed all angled parking on Sunset Avenue, and restriped the west end of the road for parallel parking. This resulted in the net loss of nine parking stalls. In an effort to make up the loss, staff looked at placing additional parking north of the former angled slots; to accomplish this, a turning study was conducted to determine where additional parking could be placed without compromising access to adjacent driveways. Possible parallel parking space placement has been determined by design realities only.

To better understand the field impact of these parking placements, staff lightly painted where the initial parking could go. Subsequent conversations with adjacent residents indicated many felt the comfortable access to their homes would be impacted by the proposed parking. Many of the adjacent driveway approaches have substandard curb aprons at the road, which if improved, would in several cases allow both additional parking and easier ingress/egress of the residents. This solution is a portion of a forthcoming decision package.

Decision Package. The August 23, 2016 Council meeting included a planning level estimate for: moving the angled parking westward; wrong-way driving reductions; a pedestrian separator, and miscellaneous striping and signage. Since moving the angled parking is no longer an question, it is proposed to shift that identified funding need to the task of improving driveway access. The estimate for the above directed improvements currently stands at \$133,600. Staff has re-considered the timing of placing a

pedestrian separator (modified C-curb) as an interim solution. Sunset Avenue will be impacted by a series of utility construction projects over the next several years. That utility work should be coordinated so the final Sunset Avenue surface improvements will not be affected. This utility work includes a new water main and individual service lines, a new gravity sewer main, demolition of the existing sewer pumping station, all new storm drainage infrastructure, and the new Waterfront Connector project. It is recommended that the cost of the pedestrian separator (est. \$61,900) should be postponed until the final Sunset improvements are made. This would bring the cost for interim improvements down to \$75,900.

Attachments:
Sunset Ave Walkway

City of Edmonds

Sunset Avenue Walkway



Parallel Parking Report

August 8th, 2017

PPP Committee

City of Edmonds

Sunset Avenue Walkway



Task:

--Determine where additional parallel parking may be located along the roadway.

Challenge:

--Additional Parking may impact adjacent driveways

Method:

--Conduct turning study at each driveway where additional parking is proposed.

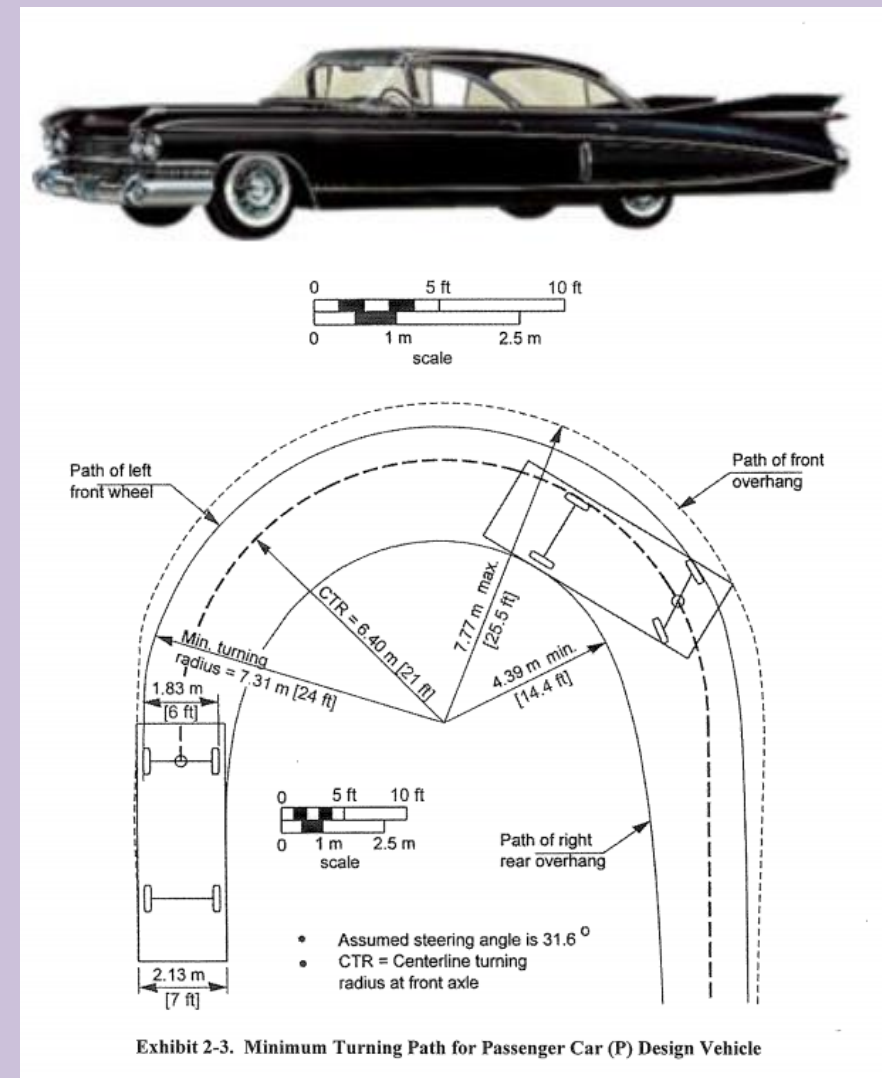
City of Edmonds

Sunset Avenue Walkway

Example:



Turning Radius Templates

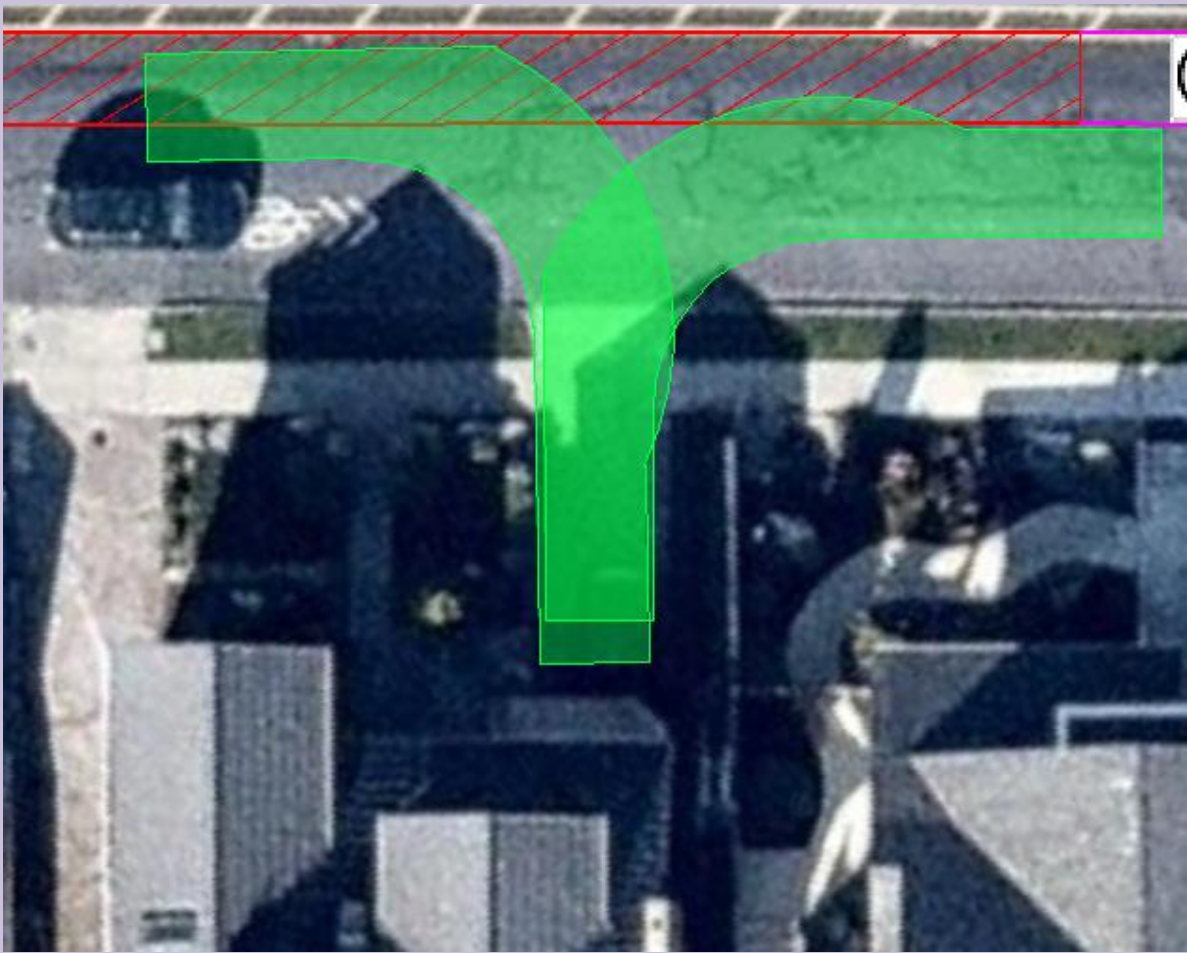
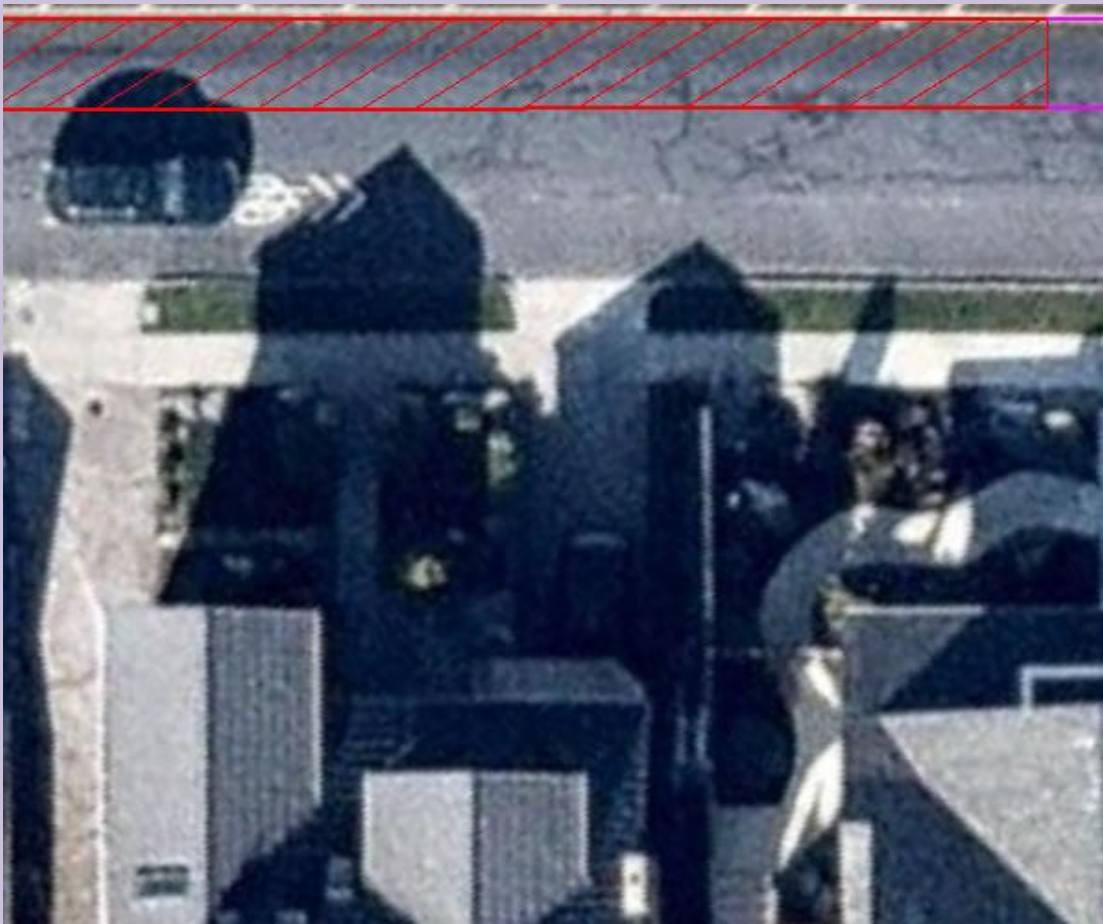


...based on large passenger vehicle

City of Edmonds Sunset Avenue Walkway

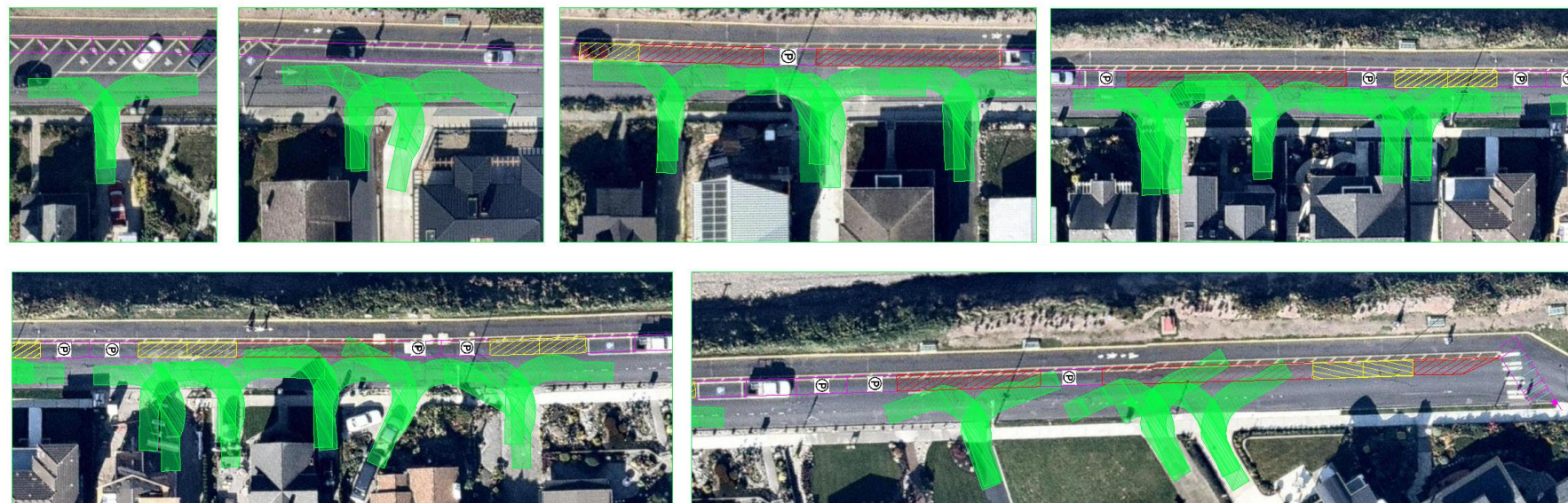
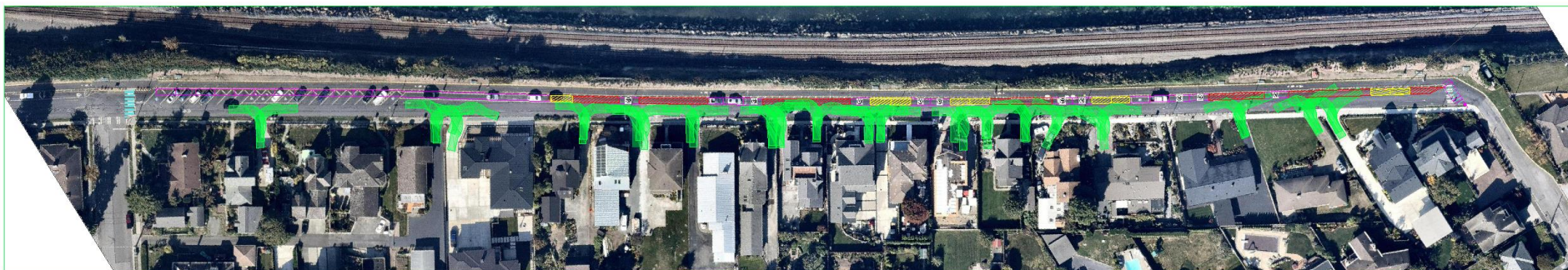


Example:






Applied to each driveway

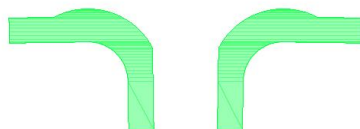
City of Edmonds Sunset Avenue Walkway



CITY OF EDMONDS
PARALLEL PARKING STUDY

LEGEND

-  Potential Parking Laid Out
-  Possible Parking--Needs Closer Study
-  No Parking Possible Without Driveway Apron Alterations

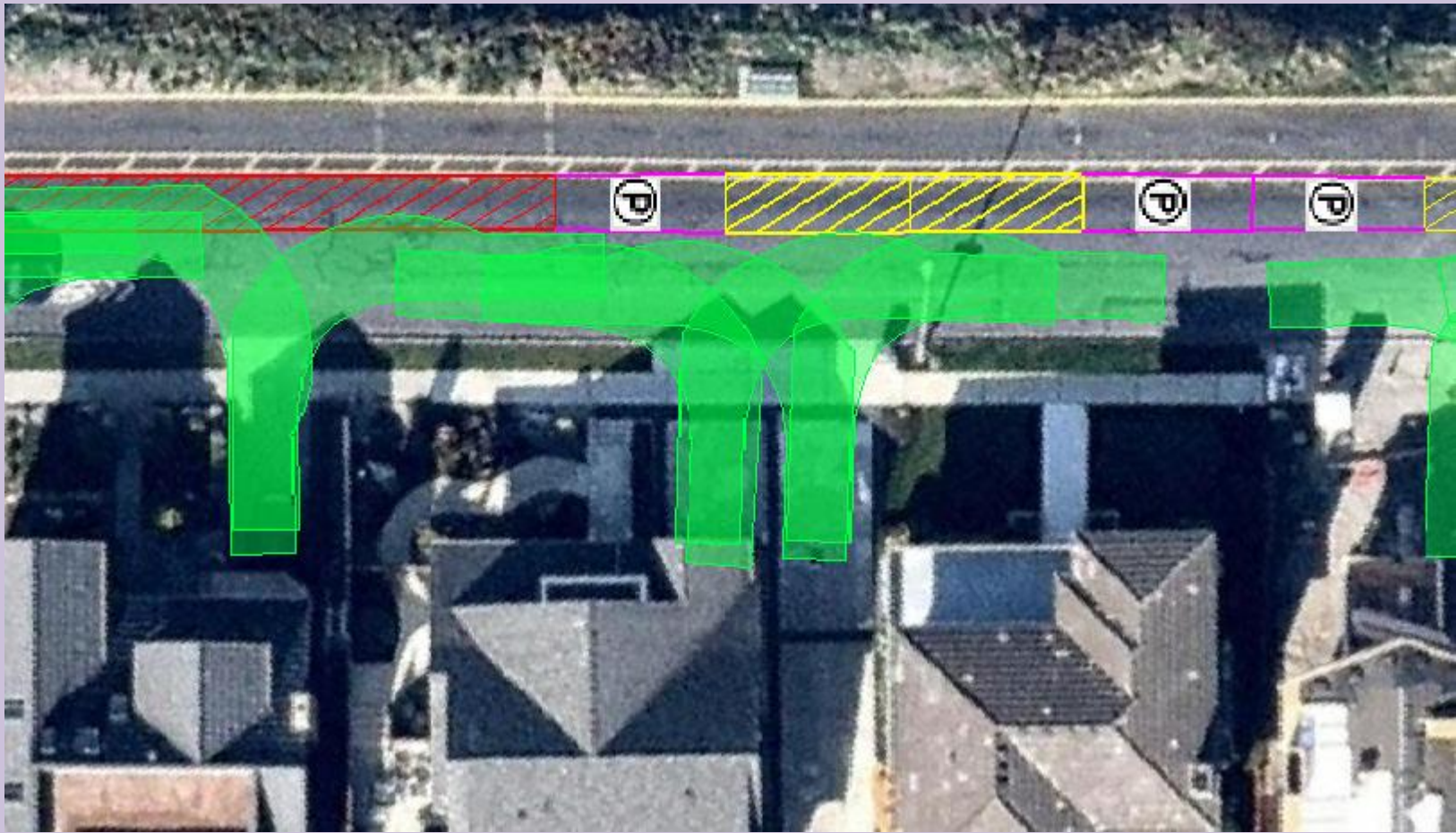


Turning Radius Templates

...*Site-wide.*

City of Edmonds

Sunset Avenue Walkway



Identified potential parking

Attachment: Sunset Ave Walkway (Sunset Ave Parking)

City of Edmonds

Sunset Avenue Walkway



Ingress / Egress Issues

Causes:

- 1. Substandard Driveway Widths*
- 2. Substandard Driveway Aprons*
- 3. Driver Expectations and Performance*
- 4. Large Vehicles*

Solutions:

- 1. Upgrade Certain Driveway Aprons to better allow access*
- 2. Limit Additional Parallel Parking to 7-9 new spaces*

City of Edmonds

Sunset Avenue Walkway



1. Interim Solutions – Planning Level Cost Estimate

1.	Improve Driveway Access	\$ 46,000
2.	Add Parallel Parking/misc. re-striping	\$ 1,100
3.	Wrong-Way Driving Reduction	\$ 16,600
4.	Walkway Physical Separator	\$ 61,900
5.	Miscellaneous Signage	<u>\$ 8,000</u>
6.	Estimate:	Decision Package being Prepared for \$71,700

Attachment: Sunset Ave Walkway (Sunset Ave Parking)

Questions

City Council Agenda Item

Meeting Date: 08/15/2017

Council Committee Reports (10 min.)

Staff Lead: Council

Department: City Clerk's Office

Preparer: Scott Passey

Background/History

N/A

Staff Recommendation

N/A

Narrative

This is an opportunity for the Council to report on items discussed in their committee meetings. The committee meeting minutes are attached.

Attachments:

FC080817

PPP080817

PSP080817

Minutes

FINANCE COMMITTEE MEETING August 8, 2017

Elected Officials Present

Councilmember Diane Buckshnis (Chair)
Councilmember Dave Teitzel

Staff Present

Scott James, Finance Director
Dave Turley, Assistant Finance Director
Jeannie Dines, Recorder

The meeting was called to order at 7:06 p.m. in Council Chambers. Committee members and staff briefly discussed action items at the last meeting. The Policy Matrix was added to the agenda.

1. June Quarterly Financial Report

Mr. Turley highlighted the following on the June Quarterly Report:

- The economy continues to do well
- Fund 112 revenues through June are at 14%, staff is behind on billing grants
- Fund 112 expenditures are only at 28%
- Building maintenance expenditures only at 24%
- REET 1 expenditures only at 1%
- REET 2 expenditures only at 16%
- Water Fund expenditures only at 25% (investigate with Public Works staff)

Mr. James and Mr. Turley responded to questions and discussion followed regarding establishing a procedure related to billing grants, hiring an additional staff person in Finance and including the position in the 2018 budget, Fund 112 revenues versus expenditures, EMS Levy, increase in Woodway law protection revenue, fire/emergency services to Woodway, traffic infraction penalties and NC traffic infraction revenue, effect the decrease in infractions has on revenue, increase in investment interest, building maintenance expenditures, capital budgeting, and Councilmembers attending budget training, and hiring staff versus contracting. Committee members commended staff on increased investment interest.

Staff will provide job description for new Finance position at September 12 Finance Committee meeting.

Action: Schedule on Full Council

2. Amendments to Chapter 3.75, Business Improvement District

Mr. Doherty reviewed proposed amendments. He and Mr. James responded to questions regarding what category real estate offices are in, ability to appeal the business classification, maintenance projects the BID proposes via their budget, and payment collection schedule.

Action: Schedule for short presentation to full Council on August 15
--

3. Amendments to Downtown Business Improvement District By-Laws

Mr. Doherty reviewed proposed amendments and responded to questions regarding the number of board members and the number of BID members in collections for delinquent payments.

Action: Schedule on Consent Agenda

4. NEW ITEM: Policy Matrix

Mr. James distributed and reviewed a Finance Policy Matrix as of August 2017 and policy amendment and development schedule. The following Finance Committee review schedule was established:

August 22	Contracting and Purchasing Policy
September 12	Capital Asset Policy
September 26	Facility Condition Report
October 10	Street Condition Report
October 10	Financial Reporting Policy and rescind Ordinance 3789
2018	Reserve Policy

Action: See above schedule

4. NEW ITEM: Admiral Way Sidewalk

Councilmember Teitzel provided a visual of the proposed location of a crossing on Admiral Way. The next Port Commission meeting will include this as an agenda item. The Port has expressed willingness to split the cost with the City. A brief discussion followed regarding the location and design of the crossing.

Action: Finance Committee agreed with this being a Council decision package

The meeting was adjourned at 8:30 p.m.

Minutes

PARKS, PLANNING AND PUBLIC WORKS COMMITTEE MEETING August 8, 2017

Elected Officials Present

Councilmember Neil Tibbott (Chair)
Councilmember Kristiana Johnson
Council President Tom Mesaros

Staff Present

Phil Williams, Public Works Director
Rob English, City Engineer
Patrick Doherty, Econ. Dev./Comm. Serv. Dir.
Jerrie Bevington, Recorder

The meeting was called to order at 7:10 p.m. in the Jury Meeting Room. Radar Feedback Signs was added as agenda item 5. Councilmember Johnson requested a future agenda (as soon as information is available and before the budget process begins) include 1) a presentation on changes the legislature made this year that allow REET to be used for housing, and 2) amount of REET revenue expected this year and how it is being spent.

2. Resolution to Designate Highway 99 Subarea as Residential Targeted Area

Mr. Doherty reported on July 18 the Council passed a resolution of intent to designate the Hwy 99 subarea as a residential targeted area and setting a public hearing, the first step to implementing the Multi-Family Tax Exemption (MFTE) program in that area. Following the public hearing, the Council can adopt the resolution actually designating Hwy 99 subarea as a residential target area. Discussion followed regarding proceeding with a decision following the public hearing provided there are no substantial public comments at the hearing, previous designation of Westgate as a residential targeted area and updates to the code that will be addressed in the following agenda item.

Action: Public hearing and potential action at the August 15 Council meeting, action potentially postponed if there is public comment the Council needs to consider.

1. Amendments to Multi-Family Tax Exemption Code Provisions

Mr. Doherty explained ECC Chapter 3.38 was developed and approved in late 2016 creating the MFTE program for the Westgate Mixed-Use Zone. He reviewed proposed amendments. Mr. Doherty suggested if the Council delays the designation of the Hwy 99 subarea as a residential target area, this item will also be delayed so they can be approved at the same time.

Discussion followed regarding unit size for low- and moderate-income households, developer interest in Hwy 99, MFTE providing incentive for developers in Edmonds, availability of addition housing assisting with housing affordability, low and moderate area median income (AMI) for Snohomish County, designations below low income, efforts of the Mayor's Housing Strategy Task Force to address providing housing for lower income levels, opportunity on Hwy 99 to potentially provide housing for lower income levels and the State Housing Tax Credit Program for housing at 60% AMI. Committee members requested:

- Next week's packet include the table regarding AMI (entire spectrum) and housing costs for those income levels.
- Staff research whether the City could require MFTE program provide housing for 60% AMI and whether that would create a disincentive for developers

Action: Schedule on full Council agenda for potentially approval following the public hearing on the Resolution to Designate Highway 99 Subarea as Residential Targeted Area

3. Pine Street Lighting Discussion

Mr. Williams provided background: in 2014/2015 Pt. Edwards' residents expressed safety concerns with lack of lighting, November 2016 Council authorized \$20,000 for street lighting on lower Pine Street west of SR104, late 2016/early 2017 City staff worked with SnoPUD on proposal to install lights, May 2017 Public Works staff begins installation of lights, May 2017 Council puts project on hold pending further discussion of options. He displayed a photograph of the area of the demo garden affected by installation of the conduit. He reviewed the two options (SnoPUD and Sternberg) researched by staff during last 60 days.

Mr. Williams relayed following his and Ms. Hite's meeting with the Pilchuck Audubon Society president, she submitted a letter questioning the necessity for lights but expressing a preference for the SnoPUD option due to the shields and asking for more time. Mr. Williams provided the following summary/recommendation:

- Initial cost is similar, affordable
- Choice between City-owned and operated/maintained system versus a system where SnoPUD is responsible for O&M and replacement
- Concern with IES footprint of the Sternberg Type IV fixture without an additional shield
- Staff recommends SnoPUD package

Discussion followed regarding the color of the LED lights, distance between the proposed lighting and St. Edward's lighting, cost of maintaining/replacing the Sternberg lights, illumination pattern of SnoPUD lights, PUD's recommendation for the equivalent of a 200w bulb, moving the pole closest to demo garden further east, usual process for installing streetlights in the City, extending the sidewalk in that area, maintenance of streetlights on Main Street, and funding source for the lights.

Councilmember Johnson displayed photographs of a third option, pedestrian level lights. Discussion followed regarding concern bollard lighting would not light the street, Pt. Edwards residents' concern with security due to darkness, the City's memorandum of understanding with the Audubon Society regarding the demo garden, providing safety in a way that minimizes secondary problems, installing motion activated lighting in the hatchery, potentially partnering with Pilchuck Audubon Society on pedestrian lighting, further investigation of the third option and seeking input from the Pt. Edwards community, bollard lighting does not meet engineering standards for street and sidewalk illumination, goal to install a solution by the end of daylight savings time (November 5) versus the Audubon Society's request for additional time, the Pt. Edwards HOA's disinterest in providing operation/maintenance of the lights and their agreement with installing standard streetlights in this area, and spacing of streetlights on Walnut.

Action: Staff investigate pedestrian/bollard lighting including estimated cost. (Committee members disagreed whether this item should return to committee or proceed to full Council.)

4. Sunset Ave Parking

Mr. Williams relayed as a result of replacing angle parking with parallel parking, spaces were reduced from 21 to 9. Staff's task was to determine where additional parallel parking can be located along the roadway without impacting adjacent driveways. Turning studies using turning radius templates were conducted at each driveway where additional parking is proposed based on a large passenger vehicle which identified 7-9 additional spaces with existing driveway configuration. Reasons for ingress/egress issues include substandard driveway widths and aprons, driver expectations and performance, and large vehicles. Solutions include 1) upgrading certain driveway aprons to allow better access, or 2) limit additional parallel parking to 7-9 new spaces. He provided planning level cost estimates for interim solutions:

1. Improve driveway access \$46,000

2. Add parallel parking/misc restriping	\$ 1,100
3. Wrong-way driving reduction	\$16,600
4. Misc signage	\$ 8,000
Total (2018 decision package being prepared)	\$71,700

Discussion followed regarding where upgrades to driveway aprons would occur, concern residents on 2nd Avenue would object to allowing drivers to turn left onto 2nd Avenue, safety concerns with wrong-way drivers on Sunset Avenue, installing temporary blocks in no-parking areas, enforcement of 2-hour parking, and resident input after staff marked potential additional spaces.

Action: Schedule for full Council discussion. (Committee members disagreed with scheduling for possible action.)

5. **NEW ITEM: Radar Feedback Signs (RFS)**

Councilmember Tibbott asked if additional RFS would be a Council or staff decision package. Discussion followed regarding the location of speed trailers and RFS, cost of the speed trailers, potential locations for additional RFS, funding of the Traffic Calming Program, speed studies staff conducts as a result of citizen requests to determine eligibility for traffic calming, being proactive related to traffic calming, pedestrian education being done by the Pedestrian Safety Committee, plans for an RFS on Olympic View Drive, and speed enforcement. Mr. Williams offered to provide additional information regarding RFS.

Action: Information only.

The meeting was adjourned at 9:05 p.m.

Minutes

PUBLIC SAFETY & PERSONNEL COMMITTEE MEETING August 8, 2017

Elected Officials Present

Councilmember Mike Nelson (Chair)
Councilmember Adrienne Fraley-Monillas
Council President Tom Mesaros

Staff Present

Al Compaan, Police Chief
Andrew Pierce, Legislative/Council Assistant

The meeting was called to order at 7:06 p.m. in the Police Training Room.

1. Proposed Resolution Supporting Consolidation of SNOCOM and SNOPAC

Council President Mesaros proposed consideration of a resolution supporting the consolidation of SNOCOM and SNOPAC. He clarified the resolution does not approve the merger only support for the merger; future steps will include approval of the interlocal agreement which may be available for Council consideration by October.

Chief Compaan reviewed the background of SNOPAC, SNOCOM and SERS; map of service areas; service deficiency in a jointly served area covered by FD1/SNOCOM and SCSO/SNOPAC; call transfer issue and delay it causes; formation of a joint task force to consider consolidation; primary decision points (service levels, costs, resiliency/redundancy, and governance); evaluation of multiple solutions; status of primary decision points; estimated one-time transition costs; governance; supermajority vote items; and rate smoothing. He reviewed next steps in the process:

- Summer 2017: work on new interlocal agreement, transition budget, timeline
- September 2017: Joint Board review and advisory position on new ILA
- October-December 2017: Depending on joint Board action, individual agencies re-briefed and decision whether to sign onto new ILA
- January 2018: New agency could start work under consolidated governance
- January 2019: Operations begin in physically consolidated center

Chief Compaan commented on the effect of Next Gen 911, transfer of calls following the Mukilteo shooting, options considered and selection of Option 4B (consolidated agency and maintaining SNOCOM facility as a “warm backup”), composition of the new board, (15 members - 10 Police and 5 Fire, combination of elected and operational staff), hypothetical assessments and savings, and savings that would be applied to rate smoothing in the first year.

Discussion followed regarding project timeline for consolidation, name of the new agency, larger consolidated center reducing potential for callers to receive a busy signal, how the governance model was determined, importance of a backup facility, reconfiguration of fire services provider seats on the board with approval of FD1/Lynnwood RFA, and the operational timeline.

Action: Presentation to full Council August 15 and consideration of resolution supporting consolidation of SNOCOM and SNOPAC

2. NEW ITEM: Opioid Crisis

Councilmember Fraley-Monillas distributed information regarding a survey that revealed 37 heroin overdoses during a 7-day period in Snohomish County including 3 deaths, 1 overdose in Edmonds, and 24 lives saved by the use of Naloxone. She and other elected officials plan to attend the National

Overdose Awareness Candle Vigil at Thornton A. Sullivan Park in Silver Lake on August 31. Discussion followed regarding future surveys, difficulty collecting information, the new social worker position providing periodic reports to the PSP Committee and an annual report to the City Council, and increasing homelessness observed in downtown Edmonds.

Action: Information only.

The meeting was adjourned at 7:46 p.m.